



AGENDA

Ordinary Meeting

TUESDAY, 28 OCTOBER 2025

7.00PM

Burnie City Council, 80 Wilson Street, Burnie

GENERAL MANAGER'S CERTIFICATION

Notice of Meeting

In accordance with Regulation 9(1) of the *Local Government (Meeting Procedures) Regulations 2025* NOTICE is hereby given that the next Ordinary Meeting of the Burnie City Council will be held at the City Offices, 80 Wilson Street, Burnie on Tuesday, 28 October 2025, with the Business of the meeting to be in accordance with the following agenda paper.

Qualified Persons Advice - Section 65 Local Government Act 1993

PURSUANT to Section 65 of the *Local Government Act 1993* I hereby certify, with respect to the advice, information and/or recommendation provided for the guidance of Council in this Agenda, that:

1. such advice, information and/or recommendation has been given by a person who has the qualifications or experience necessary to give such advice; and
2. where any advice is given by a person who does not have the required qualifications or experience, that person has obtained and taken into account the advice from an appropriately qualified or experienced person; and
3. a copy of that advice or, if the advice was given orally, a written transcript or summary of that advice has been provided to the council.



Shane Crawford
GENERAL MANAGER

PUBLIC PARTICIPATION

All Ordinary and Special Meetings of Council are opened to the public

Public Questions

Anyone may submit a question to be read out at Public Question Time during a Council Meeting. Questions asked during Public Question Time may be asked [on notice](#) or [without notice](#). Further information is provided under the [Public Questions on Notice](#) and [Public Questions without Notice](#) reports in this Agenda.

Invitations to Address Meeting

Members of the public, with the written permission of the Mayor, are able to be part of a deputation to address Council with an address, statement or report. Application forms are available by contacting the City Offices.

Livestreaming of Council Meetings

The open session of the Council Meeting will be video recorded and live streamed. Participation in person at a Council Meeting is considered that attendees have given their consent to Council to broadcast images and speech to be video recorded and live streamed.

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HOUR: 6.00pm

TIME OCCUPIED:

PRESENT:

APOLOGIES:

‘CLOSED SESSION’: COUNCIL

The General Manager advised that in his opinion, the agenda items listed below are prescribed items in accordance with Clause 17 of the *Local Government (Meeting Procedures) Regulations 2025* (confidential matters), and therefore Council may by absolute majority determine to close the meeting to the general public.

	Meeting Regulations Reference
AC096-25 COUNCILLORS DECLARATIONS OF INTEREST	17(2)(h)
AC097-25 CONFIRMATION OF MINUTES OF THE 'CLOSED SESSION' MEETING OF COUNCIL HELD ON 23 SEPTEMBER 2025	17(2)(h)
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AC104-25 GOVERNANCE SUB-RISK REGISTER - QUARTERLY REVIEW - SEPTEMBER 2025	17(2)(b)
AC105-25 NON AGENDA ITEMS	17(2)(h)
AC106-25 MATTERS CONSIDERED IN CLOSED SESSION	17(2)(h)
AC107-25 AUTHORISATION TO DISCLOSE CONFIDENTIAL INFORMATION	17(2)(h)
AC108-25 COMPLETION OF CLOSED SESSION / MEETING ADJOURNMENT	17(2)(h)

Reg Confidential Reason

- 17(2)(a) Personnel matters, including complaints against an employee of the council
- 17(2)(b) Industrial relations matters
- 17(2)(c) Information that, if disclosed, is likely to confer a commercial advantage or impose a commercial disadvantage on a person with whom the council is conducting, or proposes to conduct, business
- 17(2)(d) Commercial information of a confidential nature that, if disclosed, is likely to (i) prejudice the commercial position of the person who supplied it; or (ii) confer a commercial advantage on a competitor of the council; or (iii) reveal a trade secret
- 17(2)(e) Contracts, and tenders, for the supply of goods and services and their terms, conditions, approval and renewal
- 17(2)(f) The security of (i) the council, councillors and council staff; or (ii) the property of the council
- 17(2)(g) Proposals for the council to acquire land or an interest in land or for the disposal of land
- 17(2)(h) Information that is (i) of a personal and confidential nature; or (ii) provided to the council on the condition that it be kept confidential
- 17(2)(i) Requests by councillors for leave of absence
- 17(2)(j) Notifications by councillors of leave of absence for parental leave
- 17(2)(k) Matters relating to actual or possible litigation taken, or to be taken, by or involving the council or an employee of the council
- 17(2)(l) The personal hardship of any person who is resident in, or is a ratepayer in, the relevant municipal area

RECOMMENDATION

“THAT Council resolve by absolute majority that the meeting be closed to the public to enable Council to consider agenda items AC096-25 to AC108-25 which are confidential matters as prescribed in Clause 17 of the Local Government (Meeting Procedures) Regulations 2025.”

RESUMPTION**HOUR:** 7.00pm**TIME OCCUPIED:****PRESENT:****APOLOGIES:****ACKNOWLEDGEMENT OF COUNTRY**

The Chairperson will commence the Open Session with the Acknowledgement of Country.

Burnie City Council acknowledges that every part of Australia is, always was and always will be, Aboriginal land. Burnie City Council pays respect to the pakana / palawa – original owners and cultural custodians – of all the lands and waters across trouwunna / lutruwita / Tasmania upon which pataway / Burnie is situated and where our Council meets.

VIDEO RECORDING AND LIVE STREAMING

It is noted that the open session of the Council Meeting is video recorded and live streamed.

The video recording will be made available to the public in accordance with Regulation 33 of the *Local Government (Meeting Procedures) Regulations 2015*. The live stream is accessible from <https://webcast.burnie.tas.gov.au/>

Participation in person at a Council Meeting is considered that attendees have given their consent to Council to broadcast images and speech to be video recorded and live streamed.

OPENING STATEMENT

In accordance with Regulation 8 of the *Local Government (Meeting Procedures) Regulations 2025*, all persons attending the meeting are to be respectful of, and considerate towards, other persons attending the meeting.

Language and conduct at the meeting that could be perceived as offensive, defamatory or threatening to a person attending the meeting, or listening to the recording, is not acceptable.

PRAYER

The meeting will be opened with prayer by Pastor Jeff Weston of the Combined Churches of Burnie.

AO168-25 COUNCILLORS DECLARATIONS OF INTEREST

The Chairperson requested Councillors to declare any interest that they or a close associate may have in respect of any matter appearing on the agenda.

**AO169-25 CONFIRMATION OF MINUTES OF THE 'OPEN SESSION' MEETING
OF COUNCIL HELD ON 23 SEPTEMBER 2025****RECOMMENDATION:**

“THAT the minutes of the ‘Open Session’ of the Burnie City Council, held at the City Office on 23 September 2025, be confirmed as true and correct.”

PUBLIC QUESTION TIME**AO170-25 PUBLIC QUESTIONS ON NOTICE****FILE NO: 15/5/5**

In accordance with Regulation 36 of the *Local Government (Meeting Procedures) Regulations 2025* Council conduct a Public Question Time.

A limit of two questions received in writing by 5pm Tuesday, one week prior to the Council meeting, are treated as Public Questions on Notice – Reg 38.

Questions must relate to the activities of Council (Reg 38(2)), and should be clear and concise with minimal pre-amble. Statements will not be accepted – Reg 33(2).

Questions and answers cannot be debated – Reg 36(2).

You do not need to be present at the meeting to ask your question.

Your question and answer will be published in the Agenda and Minutes of the Council

The following questions were submitted to the Council on 17 September 2025 by David McLean, of Burnie, and have been answered by the General Manager.

1 After a recent encounter with a lost tourist at the Burnie Skate Park looking for the Penguin Centre and investigating the reason.

Could the Council please investigate adding an arrow on the existing Penguin Centre signs from both the east and west on the Bass Highway so tourists know to turn at the West Park lights.

Whilst on signs will the Council be purchasing signs so visitors can navigate the maze of CBD streets to gain access to the Burnie Arts Information Centre from the Bass Highway whenever it finally opens?

Officers will follow up and investigate your request regarding the Penguin Interpretation directional signage.

In relation to visitor services, clearly any information service needs to be identifiable and locatable. Council is awaiting the outcome of the State Government's Spirit Preparedness Grant process, where Council has applied for establishment costs for a new centre. This request includes provision for wayfinding signage.

- 2 I have been previously advised by a council executive and councillor that one of the reasons that the original Burnie Visitor Information Centre closed was due to it not making a profit.**

Whilst Burnie Arts does provide art and cultural activities for the region could the Council advise if Burnie Arts is expected to run at a profit and not at the continual loss that it currently is at the sole expense of Burnie ratepayers?

Councils need to make investment decisions and allocate funds on community infrastructure as they determine. Many Council owned and operated services and assets do not make a profit but have an important community role, including recreational facilities, parks and reserves.

Burnie Arts does not run at a profit and is unlikely to ever reach a break even point. That said, Council will continue to explore options to reduce the ongoing costs of running the facility.

Investments in facilities such as Burnie Arts provide social, economic and cultural benefits to a community and an ongoing development and activity in this area will be vitally important for Burnie's ongoing community well-being.

The following questions were submitted to the Council on 18 September 2025 by Samuel Bacon, of Burnie, and have been answered by the General Manager.

1 Is the inner city planned to be massively remodelled/revamped? If so what's planned?

Council officially adopted the Burnie City Centre Urban Plan at its February 2025 meeting – all of the details, including a series of videos and the document itself, are available on Council's website. Council want to revitalise the city centre as a thriving place for people to live, work, play and do business and this plan is the guiding document for that to occur.

2 What is going to happen about the tourism issue? is anything under consideration about this and how are you going to resolve these issues?

I am unclear as to what issues you may be referring to in your question.

If your question relates to visitor services, or the provision of visitor information, Council's current plan is to provide these services from the new Burnie Arts facility. Council has an action in its Annual Plan regarding this point.

The following questions were submitted to the Council on 19 September 2025 by Wayne Bramich, of Burnie, and have been answered by the General Manager.

1 What are the reasons for council to consider selling Cooee Point and wouldn't it be a better long term outcome to retain and develop this site ourselves?

The reasons for a potential sale of Cooee Point are outlined in the report on the agenda (Item AO155-25, Council Meeting held on 23 September 2025) including demand for residential land, enabled by the current zoning of the land, and providing opportunity for development.

You have an opportunity to participate in the public submission process to raise any concerns, objections or alternatives before a report is brought back to Council to make a final decision on disposal.

2 We get snippets of information on new developments and how they will create jobs and boost the economic fortunes of Burnie, However they are short on detail and the adverse effects they may also have for us. For instance the new hydrogen plant is supposedly taking the recycled water from the sewerage plant... For instance, just one example, there is no mention if this water will be enough or if not how much extra water will be needed and where will it come from? Does anyone have a complete resource requirement and the value attributed to this?

I understand that your question relates the proposed HIF e-fuel facility on the former pulp mill site in South Burnie. It is planned to produce e-methanol at the new plant.

In the information flyer that HIF provided to neighbouring properties they indicated that *"Tasmania's renewable energy powers electrolyzers to separate hydrogen from water. The water needed for the process can potentially be recycled from the Round Hill Sewage Treatment Plant – this water would otherwise be discharged into the ocean"*

HIF are planning on holding information sessions in coming months for interested community members to enable you to find out more, ask questions and provide opinion.

This, likely many other projects, is a private development and details regarding the nature and operations of the development are not for Council to disclose outside the statutory process. It is up to property developers to engage with the community as they determine.

The following questions were submitted to the Council on 25 September 2025 by Shayne Andrews, of Burnie, and have been answered by the General Manager.

- 1 With the impending removal of the sorry looking Norfolk pines along the foreshore is there really a need to replace them with something similar?. As councillor Aitken pointed out there are a row of perfectly fine trees already separating the foreshore from the carpark. My guess is you would gain at least 4 parking bays once the pines are removed thus improving parking options and receiving ongoing revenue?**

Council has not yet made a decision regarding the future use of the foreshore space following the removal of the Norfolk Pines. In the short term, the trees and stumps will be removed, and the area will be left in a safe and tidy condition for the public.

Long-term planning for the space will be considered as part of the Foreshore Urban Plan, which is currently underway. Community input will play an important role in shaping the final outcome.

- 2 With internal works almost complete at Burnie Arts once all the constructors buildings are removed will the outside improvements opposite the museum commence immediately?**

Yes, the works within the Civic Plaza are funded by Council as part of this year's Capital Program and are scheduled for completion by 30 June 2026. Progress has been delayed due to the need to remove existing site sheds currently located within the plaza space. These are now being relocated, allowing the first stage of the plaza works to commence.

The following question was submitted to the Council on 1 October 2025 by Gordon Russell, of Burnie, and has been answered by the General Manager.

- 1 Is council going to put extra staff on this year so the roadside slashing can be done on time or are some residents going to have to wait til April again. This does not help the bushfire management plan of many rural people when council only slashing roadsides after the fire season. Has been an issue for last several years now.**

Roadside slashing is scheduled to commence between November and December this year. The program typically takes around four months to complete.

At this stage, Council does not plan to add additional staff to the program, as doing so would also require significant investment in additional plant and equipment.

To ensure fairness, Council rotates the slashing routes each year so that areas completed later one year are prioritised earlier the next.

In addition to the slashing program, Council officers also undertake other bushfire mitigation activities to support community safety throughout the fire season.

The following question was submitted to the Council on 15 October 2025 by Sharyn Bartlett, of Burnie, and has been answered by the General Manager.

- 1 I submitted a question in January 2024. The letter of response I received stated my suggestion was a good idea and would be looked into. I note that nothing has happened re dog feces disposal bins or even an extra 'human' bin at Romaine. Nor has anything happened down at South Burnie. The response letter stated I would be contacted but as I haven't, I am basically asking the same question: Can more dog feces disposal bins be placed at Romaine Reserve, including a human bin at the picnic spot half way around, and down along the foreshore at South Burnie. Thank you**

The bins are being considered as part of this year's capital program. Officers are currently assessing suitable locations as part of our broader waste review. It is expected that the selected locations and any additional bins will be in place before the end of the financial year.

The following question was submitted to the Council on 21 October 2025 by David McLean, of Burnie, and has been answered by the General Manager.

- 1 At the September council meeting Councillor Lynch mentioned that if residents were concerned about council business they should review the Auditors report that is contained within the agenda. Following his advice, I reviewed and found not much to read as the majority of information is within attachments not included within the agenda. Will Burnie Council include this information in future agendas so council provides the transparency to its operations as stated many times in the past and as noted in their Community Engagement Framework Strategy. Could I also be sent a copy of these attachments.**

All councils are required to have an Audit Panel which includes experienced independent members, for the purpose of reviewing council's performance across a number of matters prescribed under legislation.

The minutes of the Audit Panel Meeting are generally tabled to the open session of the Council Meeting, which allows the public to see the scope of matters discussed and reviewed. Attachments of content detail are not included in those minutes which are tabled to council.

Some of the reports are duplicates of recent council reports from open sessions of council, which can be found in full in the open council minutes online – such as all policy reviews, quarterly operational reports, financial reports, strategies and plans, and annual plan and budget estimates, adopted by council.

Some reports are duplicates of recent council reports from closed sessions of council, and therefore are not permitted to be published, by virtue of being classified closed in accordance with prescribed reasons set out in the *Local Government (Meeting Procedures) Regulations 2025*.

Lastly, some reports are one-off reports direct to the Audit Panel only which also contain confidential data classified under the prescribed reasons from the above Regulations, and therefore are not able to be provided in the minutes for council's open session.

Those prescribed reasons for confidentiality can be found in Regulation 17 of the *Local Government (Meeting Procedures) Regulations 2025*.

The inclusion of the minutes, despite not including the attachments, does allow the public to see the scope of all topics discussed. The minutes include a section titled Audit Panel Discussion for each item, which summarises the discussion on the matter.

Having said this, in addition to the minutes of Audit Panel, the independent Chairperson also provides a detailed summary of the meeting with a description of the items discussed, any issues or concerns raised in the discussion and responses given. Unless a specific reason prevents it, this chairperson report is included in open session, alongside the minutes.

PUBLIC QUESTION TIME**AO171-25 PUBLIC QUESTIONS WITHOUT NOTICE****FILE NO: 15/5/5**

In accordance with Regulation 36 of the *Local Government (Meeting Procedures) Regulations 2025* Council conduct a Public Question Time.

A limit of two questions received in writing after 5pm Tuesday, but before 5pm Friday, prior to the Council Meeting, are treated as a Public Questions without Notice – Reg 37(1). Questions received after this time will be deferred to the next scheduled Council Meeting and be treated as a Public Question on Notice.

Questions must relate to the activities of Council (Reg 37(2)), and should be clear and concise with minimal pre-amble. Statements will not be accepted – Reg 33(2).

You need to be present at the Council Meeting to ask your question.

If your question is accepted, it will be either answered at the Council Meeting or Taken on Notice and tabled in the Minutes of the following meeting.

If you are absent at the Meeting, the question will not be read or answered. Your question will be tabled at the next meeting as a Public Question on Notice.

If a question is refused, the Minutes will record your name and the reason for refusal – Reg 33(4).

COUNCIL MEETING AS A PLANNING AUTHORITY

The Chairperson advised that for items AO172-25 and AO173-25 Council is acting as a Planning Authority under the provisions of the *Land Use Planning and Approvals Act 1993*.

The Chairperson advised that the following provisions apply with respect to motions relating to recommendations on a land use planning process:

- (a) a councillor moving a motion contrary to the recommendation is to:-
 - (i) provide the motion in writing; and
 - (ii) provide in writing supporting reasons for approval or refusal;
- (b) the motion and supporting reasons for approval or refusal are to be provided to the general manager at least 24 hours prior to the meeting to allow for circulation and consideration by all members of the planning authority;
- (c) the general manager is to ensure that the supporting reasons provided under paragraph (a)(ii) are recorded in the minutes, in accordance with Regulation 29 of the *Local Government (Meeting Procedures) Regulations 2025*.

PLANNING AUTHORITY**AO172-25 TASMANIAN PLANNING SCHEME
PERMIT APPLICATION DA 2025/38 - 1 & 3 ATKINS DRIVE,
ROMAINE - MULTIPLE DWELLINGS X 18 AND SUBDIVISION AND
CONSOLIDATION**

FILE NO: DD001.2025.00000038.001
PREVIOUS MIN:

1.0 RECOMMENDATION:

“THAT Council in its role as a planning authority under the Land Use Planning and Approvals Act 1993 determine in accordance with section 51(2)(c) and section 57(6) of the Land Use Planning and Approvals Act 1993, State Planning Provision 6.8.1 and Section 81 of the Local Government (Building and Miscellaneous Provisions) Act 1993 to GRANT a permit to application DA 2025/38 on land described in CT 161069/2 and CT 162223/0 identified as 3 & 1 Atkins Drive, ROMAINE for the following:-

- a) To establish a Multiple Dwelling Development consisting of 18 dwellings;*
- b) Subdivision to reduce the area contained within CT 161069/2 from 4058m2 to 3940m2; and*
- c) A new sewer connection via the services/drainage easement on the adjoining property at 1 Atkins Drive identified as CT 162223/0*

Nature of Approval

- 1) That the development is to proceed and be maintained generally in accordance with the descriptions, commitments and requirements contained in the following documents - copies of which are attached and endorsed to form part of this Permit:-*
 - a) Further information response, as prepared by ireneinc planning & urban design, dated 12 August 2025;*
 - b) Supporting Planning Report, as prepared by ireneinc & smith street studio, dated August 2025;*
 - c) Location Plan, as prepared by Jaws Architects, project no. P24042, sheet DA-01, revision A, dated 13 June 2025;*
 - d) Existing Site Plan, as prepared by Jaws Architects, project no. P24042, sheet DA-02, revision A, dated 13 June 2025;*

- e) *Proposed Site Plan, as prepared by Jaws Architects, project no. P24042, sheet DA-03, revision B, dated 7 August 2025;*
- f) *Open Space Diagrams, as prepared by Jaws Architects, project no. P24042, sheet DA-04, revision A, dated 13 June 2025;*
- g) *Site Elevations, as prepared by Jaws Architects, project no. P24042, sheet DA-05, revision B, dated 7 August 2025;*
- h) *Site Sections – Proposed, as prepared by Jaws Architects, project no. P24042, sheet DA-06, revision A, dated 13 June 2025;*
- i) *Typical Unit Plans, as prepared by Jaws Architects, project no. P24042, sheet DA-07, revision B, dated 7 August 2025;*
- j) *Overshadow Diagrams, as prepared by Jaws Architects, project no. P24042, sheet DA-08, revision A, dated 13 June 2025;*
- k) *3D Perspective View – Heights, as prepared by Jaws Architects, project no. P24042, sheet DA-09, revision B, dated 7 August 2025;*
- l) *Proposed Landscape Plan, as prepared by Jaws Architects, project no. P24042, sheet DA-10, revision B, dated 7 August 2025;*
- m) *Proposed Driveway Plan, as prepared by Jaws Architects, project no. P24042, sheet DA-11, revision A, dated 13 June 2025;*
- n) *Open Space Diagrams- Individual Lot Area, as prepared by Jaws Architects, project no. P24042, sheet DA-12, revision B, dated 7 August 2025;*
- o) *Overshadow Diagrams – Unit 12-18, as prepared by Jaws Architects, project no. P24042, sheet DA-13, revision B, dated 7 August 2025;*
- p) *Proposed Site Plan – Upper Floor, as prepared by Jaws Architects, project no. P24042, sheet DA-14, revision B, dated 7 August 2025;*
- q) *Traffic Impact Assessment, as prepared by Hubble Traffic, dated August 2025;*
- r) *Response to information request, as prepare by ADG, dated 14 August 2025;*
- s) *Preliminary Civil Services Concept Sketch, as prepared by ADG, project no. 28911, drawing no. DA02, revision. 02, dated 14 August 2025;*
- t) *Detail Survey, as prepared by Arthur Moehrke Surveys Pty Ltd, project no. P24042, drawing no. 01, dated 28 August 2024;*

- u) Detail Survey, as prepared by Arthur Moehrke Surveys Pty Ltd, project no. P24042, sheet 1 of 1, revision 1, dated 28 August 2024;*
- v) Council Advice to Developer, dated 2 September 2025, as prepared by Burnie City Council; and*
- w) TasWater Development Permit Conditions/Submission dated 20 August 2025 which the Regulated Entity has required the Planning Authority to include in this Permit pursuant to Section 56W of the Water and Sewerage Industry Act 2008.*

Operation and Management Constraints

Nil

Notes:

- (1) It is the responsibility of the Permit holder to:-**
- (a) Identify the correct boundaries of the land and to ensure the structures will be located where approved;*
 - (b) Cover all costs associated with the provision and construction of access to the site and with the connection of utilities;*
 - (c) Take all reasonable measures to locate and protect any public utility installation within the land or in the vicinity of works, and to make good any damage which may occur;*
 - (d) In accordance with section 35 and 43 Local Government (Highways) Act 1982 any damage caused to the Council Road (Local Highway) including any public utility installation shall be repaired to Councils satisfaction prior to the completion of the development.*
 - (e) In accordance with Burnie City Council Highways By-Law No.3 of 2013 -
 - I. No use is to be made of the Council Road for the loading, unloading, storage or handling of goods and materials prior to the issue of a Permit under this By-Law by Burnie City Council.*
 - II. The permit holder must ensure reasonable measures are in place to avoid the tracking of mud and debris from the site onto the public road; and to immediately remove and clean up any mud or debris which may inadvertently be carried onto the road.**
 - (f) Have regard to the peaceful enjoyment and protection of occupation and use and development on adjacent land, and to program the nature and timing of work to minimise potential for disturbance, disruption or harm; and*
 - (g) Identify the need for and obtain all other permits or approvals which may be required by the law of Tasmania in relation to the conduct of the use and the carrying out of development on the land.*

- (h) ***Necessary easements for power, sewerage, water, drainage, access and the like must be depicted on the final survey plan lodged for sealing; and***
- (i) ***Final Plan of Subdivision will not be sealed in accordance with section 89 Local Government (Building and Miscellaneous Provisions) Act 1993 unless all of the information required under section 88(1) of the Act has been lodged to the satisfaction of the Council and Section 81 of the Local Government (Building and Miscellaneous Provisions) Act 1993.***

2.0 EXECUTIVE SUMMARY

Purpose and Background

Application has been made to Council seeking grant of a permit under the *Land Use Planning and Approvals Act 1993* for the following:-

- A Multiple Dwelling Development consisting of 18 dwellings proposed to be located on 3 Atkins Drive identified as CT 161069/2;
- Subdivision to remove a portion of the public footpath and roadway from the title to 3 Atkins Drive identified as CT 161069/2; and
- A new sewer connection via the services/drainage easement on the adjoining property at 1 Atkins Drive identified as CT 162223/0

This report provides an assessment of the proposed development against the relevant requirements of the *Tasmanian Planning Scheme* taking into consideration the matters contained within 5 representations (one of which contains 5 additional signatories) received during the public exhibition period under section 57 of the Act.

Key Issues

The proposal relies on the following Performance Criteria for grant of a permit:-

- Clause 8.4.1 – Residential Density for Multiple Dwellings (P1) in relation to the land size and the number of dwellings proposed;
- Clause 8.4.2 – Setbacks and building envelope for all dwellings (P1 & P3) – due to units 12 & 13 having a setback of 3.3m and 3m to the site's frontage following the proposed subdivision and units 1 to 11 being partially located outside of the building envelope;
- Clause 8.4.7 – Frontage fences for all dwellings (P1) due to the proposed 1.7m fence adjacent to units 12, 13, 14 & 15;
- Clause 8.4.8 – Waste Storage for Multiple Dwellings (P1) due to proposed waste storage being located within 4.5m of the sites frontage; and
- Clause C2.5.1 – Car parking Numbers (P1.1)

3.0 SUPPORTING INFORMATION

Strategic Alignment

This report relates to the Council Plan Goal 2: Making liveable local communities; by ensuring Council remains compliant with its statutory and regulatory obligations and contributes to the regulatory environment which affects our community.

Legal

The Council is established as a planning authority by definition under section 3(1) of the Act and must enforce the planning scheme under section 48. The purpose of this report is to enable the planning authority to determine application DA 2025/38.

In accordance with section 57 of the Act and the Scheme, this proposal is an application for a discretionary permit. Council, as the planning authority, may approve or refuse discretionary permit applications after considering both Council's planning scheme and the public representations received.

The provisions of the Act require a planning authority to take all reasonable steps to ensure compliance with the planning scheme.

Council's assessment of this proposal should also consider the issues raised in any representations received, the outcomes of the State Policies and the objectives of Schedule 1 of the Act.

This report details the reasons for the officer recommendation. The planning authority must consider this report but is not bound to adopt the recommendation. Broadly, the planning authority can either:

- 1) adopt the recommendation, or
- 2) vary the recommendation by adding, modifying, or removing recommended reasons and conditions or replacing an approval with a refusal (or vice versa).

Any alternative decision requires a full statement of reasons to comply with the *Judicial Review Act 2000* and the *Local Government (Meeting Procedures) Regulations 2015*.

Finance

There are no financial impacts resulting from this report.

There is likelihood if a decision on a permit application is appealed, that the planning authority will be responsible for the costs of representation and appearance before the Tasmanian Civil and Administrative Tribunal.

Relation to Council Policy

There are no Council policies relating to this report.

Discussion

Refer to Part 4 and Attachments for further information.

Health and Safety

There are no health and safety impacts resulting from this report.

Risk

There is risk -

- a) The decision of a planning authority may be appealed if the applicant, or any person who has made a relevant representation during the exhibition period, is dissatisfied with the decision;
- b) The applicant or a third party may allege breach of procedural fairness in relation to the execution of one or more of the statutory processes applicable to assessment and determination of a permit application;
- c) The applicant will bring an action before the Tasmanian Civil and Administrative Tribunal (TASCAT) if the planning authority fails to make a decision within the relevant statutory timeframe, in which event the planning authority will be liable for the costs of all parties;
- d) A person may allege the planning authority has failed to exercise, or improperly exercised, a statutory power relating to assessment and determination of a permit application; or
- e) A person may allege a planning authority, or a person appointed or employed by a planning authority has engaged in improper conduct relating to assessment and determination of a permit application.

A planning authority may minimise risk by –

- a) Determining a permit application by reference only to the information provided with the permit application or in any representation received during the exhibition period;
- b) Determining compliance by reference only to the regulatory provisions and processes which are relevant to the use or development described in the permit application;
- c) By remaining impartial, observe all relevant processes, and not indicating any position on the application until the matter is presented for decision;

- d) Not accept or invite any inducement relating to a decision on a permit application.

Environmental Sustainability

There are no environmental sustainability impacts to be considered in this report apart from those specifically outlined in any relevant provisions of the Scheme.

Consultation

Relevant Council Officers have been consulted in the preparation of this report.

Conflict Of Interest

No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

4.0 DETAIL

The sites are identified as CT 161069/2, known as 3 Atkins Drive and CT 162223/0 (common property for strata scheme), known as 1 Atkins Drive.

CT 162223/0 known as 1 Atkins Drive only forms part of the proposal as far as it relates to connection to the sewer main. This site contains an existing Multiple Dwelling development containing 16 dwellings and a new sewer connection/drainage easement is proposed through the property.

CT 161069/2 known as 3 Atkins Drive is vacant and contains an area of 4085m².

Both sites are zoned General Residential under the *Tasmanian Planning Scheme*.

The sites do not contain any applicable overlays within the *Tasmanian Planning Scheme*.

CT 161069/2 adjoins Crown Land to the east which contains Parklands Highschool and is zoned Community Purposes. Opposite the site to the south is residential development.

The site has a crossfall of approximately 9.0m from southeast to northwest.

The site has frontage to Atkins Drive. There is no existing vehicular access.

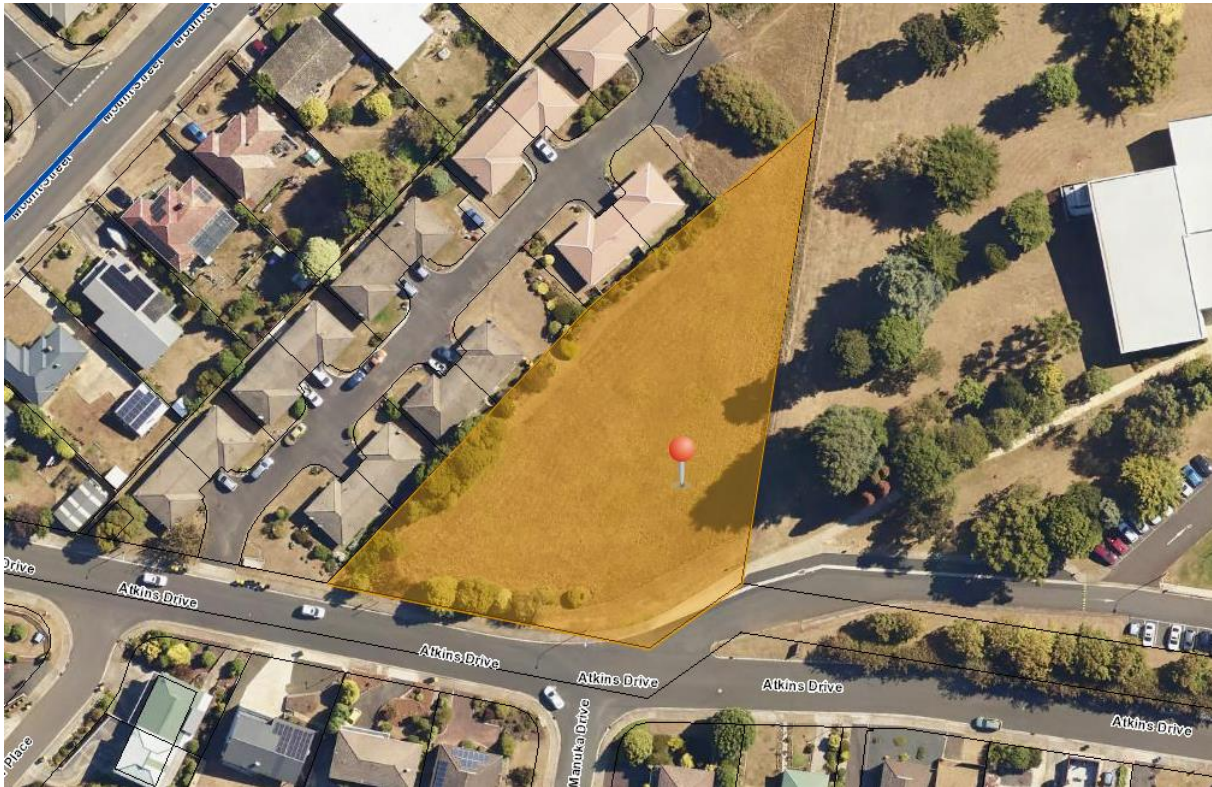


Figure 1 – Aerial photograph from ListMaps (<https://maps.thelist.tas.gov.au/listmap/app/list/map>) showing the subject site at 3 Atkins Drive identified as CT 161069/2

The permit application includes the following:-

- A Multiple Dwelling Development consisting of 18 dwellings proposed to be located on 3 Atkins Drive identified as CT 161069/2;
- Subdivision to remove a portion of the public footpath and roadway from the title to 3 Atkins Drive identified as CT 161069/2. This will reduce the size of the title from 4058m² to 3940m² (approx. 118m²); and
- A new sewer connection via the services/drainage easement on the adjoining property at 1 Atkins Drive identified as CT 162223/0

The multiple dwelling component of the application proposes 11 dwelling units on the lower northwestern portion of the site and 7 dwelling units on the higher southeastern side of the site.

Each dwelling is proposed to contain two bedrooms and to be of two-story split-level construction to accommodate the slope of the site.

The ground floor of each unit will contain a kitchen and living room with a deck. The first floor will contain two bedrooms, bathroom and balcony.

Units 1 to 11 will present as single level when viewed from the internal driveway whilst units

12 to 18 will present as two story when viewed from the internal driveway – see images below in figures 2 and 3.

Retaining walls are proposed to the rear of units 12 to 18

Dwellings are proposed to be clad in shiplap timber mimic finish (weatherboard look alike) in white with masonry block work walls also finished in white and longline profile wall and roof cladding in a white powder coat finish.

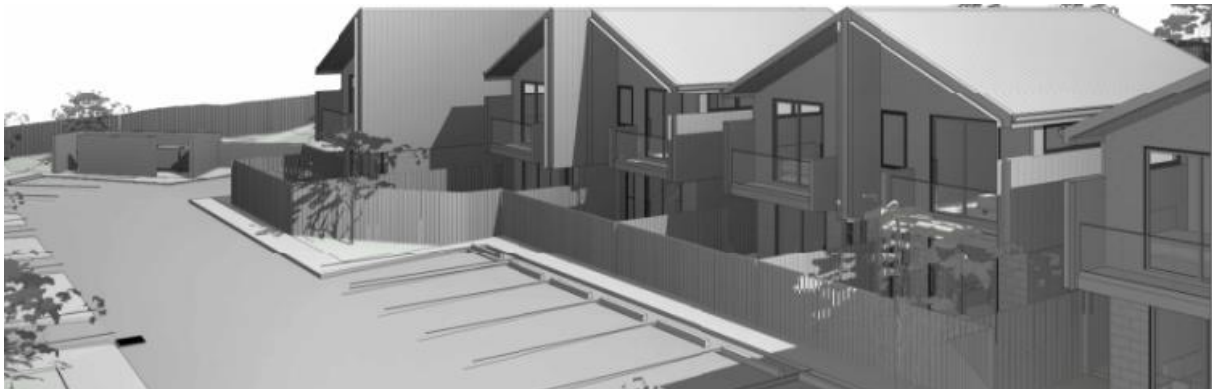


Figure 2 – Shows the internal driveway and proposed dwelling units on the south eastern (higher) side of the site – image extract from cover sheet as prepared by Jaws Architects

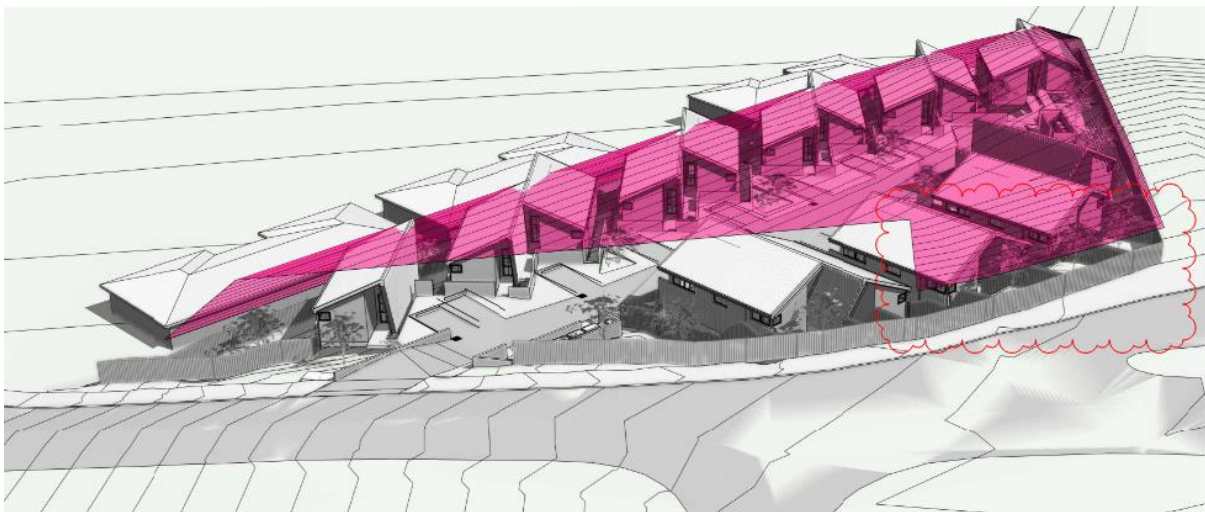


Figure 3 – Shows the proposed dwelling units 1 to 11 on the north-western portion of the site as single storey when viewed from the internal driveway – image extract from 3D Perspective View – Heights, as prepared by Jaw's Architects, sheet no. DA-09

Two of the proposed dwellings, dwellings 1 and 18 are proposed to stand independently with the remainder of the dwellings co-joined.

A total of 18 car parking spaces are proposed along with a new vehicular access from Atkins Drive.

Pursuant to clause 6.2.6 of the *Tasmanian Planning Scheme* the proposed development is categorised into 'Residential' use class as listed in Table 6.2 of the Scheme. A residential use (if not listed as No Permit Required) within the General Residential zone is 'permitted' provided it can meet all the applicable Acceptable Solutions.

In this instance, the application is discretionary as it cannot meet all the applicable Acceptable Solutions and relies on the following Performance Criteria for grant of a permit:-

- Clause 8.4.1 – Residential Density for Multiple Dwellings (P1) in relation to the land size and the number of dwellings proposed;
- Clause 8.4.2 – Setbacks and building envelope for all dwellings (P1 & P3) – due to units 12 & 13 having a setback of 3.3m and 3m to the site's frontage following the proposed subdivision and units 1 to 11 being partially located outside of the building envelope;
- Clause 8.4.7 – Frontage fences for all dwellings (P1) due to the proposed 1.7m fence adjacent to units 12, 13, 14 & 15;
- Clause 8.4.8 – Waste Storage for Multiple Dwellings (P1) due to proposed waste storage being located within 4.5m of the sites frontage; and
- Clause C2.5.1 – Car parking Numbers (P1.1)

Assessment against these provisions is provided in the following tables.

Table 1 – Assessment of Clause 8.4.1 (P1) of the Scheme

8.4.1 Residential density for multiple dwellings	
Objective:	That the density of multiple dwellings: (a) makes efficient use of land for housing; and (b) optimises the use of infrastructure and community services.
Acceptable Solutions	Performance Criteria
A1 Multiple dwellings must have a site area per dwelling of not less than 325m ²	P1 Multiple dwellings must only have a site area per dwelling that is less than 325m ² , if the development will not exceed the capacity of infrastructure services and: (a) is compatible with the density of existing development on established properties in the area; or (b) provides for a significant social or community benefit and is: i) wholly or partly within 400m walking distance of a public transport stop; or ii) wholly or partly within 400m walking distance of an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone, Central Business Zone or Commercial Zone.

Assessment:

P1 –

The site has an area of 4058m². This will reduce to 3940m² (approx. 118m²) following subdivision to remove the portion of public footpath and roadway from the title to 3 Atkins Drive.

The Acceptable Solution requires that multiple dwelling must have a site area per dwelling of not less than 325m².

The proposal is for 18 dwellings on the site requiring a total area of 5850m² (18 x 325m² = 5850m²).

In this regard the proposal provides a total of 218.8m² (total site area = 3940m²/18) per dwelling and relies on assessment against the corresponding Performance Criteria in P1 above which states:-

Multiple dwellings must only have a site area per dwelling that is less than 325m², if the development will not exceed the capacity of infrastructure services and:

- (a) is compatible with the density of existing development on established properties in the area; or*
- (b) provides for a significant social or community benefit and is:*
 - iii) wholly or partly within 400m walking distance of a public transport stop; or*
 - iv) wholly or partly within 400m walking distance of an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone, Central Business Zone or Commercial Zone.*

The applicant has provided justification as follows:-

An initial review of dwelling density in the surrounding area indicates the proposed density is compatible, and capable of satisfying P1(a). However, as the proposal is for social/affordable housing provided by Homes Tasmania, P1(b) is most applicable.

The test under P1(b) was designed to provide special consideration of residential housing projects (such as social housing) that require higher densities to address homelessness and disadvantaged communities. These projects deliver significant social and community benefits and generally satisfy the test under P1(b), on the proviso that they are delivered by a registered housing provider and are located wholly or partly within 400m of a public transport stop, or areas where appropriate services and facilities can be accessed.

There are five (5) public bus stops within 220m of the site. One is located along Manuka Drive to the south, two are located further west along Three Mile Line Road and a further two stops are located to the north-west along Mount Street.

Planner Response:-

In terms of the capacity of infrastructure services, the site can connect to Tas Water's water and sewerage network (Tas Water have issued conditions in relation to this dated 20 August 2025) and connection to Council's stormwater network can also be achieved subject to specific conditions as per Council's Advice to Developer correspondence dated 2 September 2025.

It is considered that social and affordable housing does provide a significant social and community benefit in terms of providing affordable housing options to the community.

Below shows a screenshot taken from 'List Maps' – figure 4. This shows the subject site at 3 Atkins

Drive, the general access bus routes in green and the general access bus stops in green and yellow.

The second screenshot below from List Maps' – figure 5 shows the subject site outlined in blue with an approximate radius of 400m from the centre of the site showing that there are numerous public bus stops within 400m of the site including a bus stop directly in front of the site meeting P1 (b) (iii).



Figure 4 – Shows the subject site in relation to the general access bus routes shown in green (source – List Maps - <https://maps.thelist.tas.gov.au/listmap/app/list/map>)



Figure 5 – Shows the subject site in relation to the general access stops shown in green and yellow within a

400m radius of the site (source – List Maps - <https://maps.thelist.tas.gov.au/listmap/app/list/map>)

It is concluded that the proposal can meet the requirements of the Performance Criteria (P1 (b) (i)) by making efficient use of land for housing and optimising the use of infrastructure.

Table 2 – Assessment of Clause 8.4.2 (P1 & P3) of the Scheme

8.4.2 Setbacks and building envelope for all dwellings	
Objective:	<p>The siting and scale of dwellings:</p> <ul style="list-style-type: none"> (a) provides reasonably consistent separation between dwellings and their frontage within a street; (b) provides consistency in the apparent scale, bulk, massing and proportion of dwellings; (c) provides separation between dwellings on adjoining properties to allow reasonable opportunity for daylight and sunlight to enter habitable rooms and private open space; and (d) provides reasonable access to sunlight for existing solar energy installations.
Acceptable Solutions	Performance Criteria
<p>A1</p> <p>Unless within a building area on a sealed plan, a dwelling, excluding garages, carports and protrusions that extend not more than 0.9m into the frontage setback, must have a setback from a frontage that is:</p> <ul style="list-style-type: none"> (a) if the frontage is a primary frontage, not less than 4.5m, or, if the setback from the primary frontage is less than 4.5m, not less than the setback, from the primary frontage, of any existing dwelling on the site; (b) if the frontage is not a primary frontage, not less than 3m, or, if the setback from the frontage is less than 3m, not less than the setback, from a frontage that is not a primary frontage, of any existing dwelling on the site; (c) if for a vacant site and there are existing dwellings on adjoining properties on the same street, not more than the greater, or less than the lesser, setback for the equivalent frontage of the dwellings on the adjoining sites on the same street; or (d) if located above a non-residential use at ground floor level, not less than the setback from the frontage of the ground floor level. 	<p>P1</p> <p>A dwelling must have a setback from a frontage that is compatible with the streetscape, having regard to any topographical constraints.</p>

Assessment:

P1 –

The application relies on Performance Criteria as dwellings 12 and 13 will be setback 3.3m and 3.0m from the sites frontage following the proposed subdivision and therefore cannot meet the Acceptable Solution in A1 (c) which applies to a vacant site. In this case dwellings 12 and 13 will have a setback less than the lesser setback of unit 1/1 Atkins Drive on the adjoining property.

The Performance Criteria states:-

A dwelling must have a setback from a frontage that is compatible with the streetscape, having regard to any topographical constraints.

The applicant has provided justification against the Performance Criteria as follows:-

The site has a steep downward slope to the west/north-west. The proposed units have been sited in response to this, to take advantage of the north-westerly orientation whilst providing appropriate gradients for vehicle access and private open space, whilst minimising cut and fill.

Whilst the topographical constraints and general siting requirements contribute to the discretionary setback for Units 12 and 13, the permitted setback would otherwise be achieved if not for the required boundary adjustment. Notwithstanding, the site effectively represents the last residential lot along the northern side of Atkins Drive, with the adjoining Parklands High School occupying the remaining frontage. Unit 1 provides a consistent and compatible frontage setback, with the discretionary variation for Units 11 and 12 limited to the south-eastern portion of the frontage.

Given these factors, the setback of Units 11 and 12 is compatible with the prevailing frontage setbacks to Atkins Drive.

Planner Response:-

The site will be amended by way of subdivision to remove the portion of roadway and footpath from the title. The new proposed title boundary will follow the curve of the existing roadway and entry into Parklands Highschool as can be seen in figure 6 below.

The majority of dwelling 12 and 13 will be within 4.5m setback from the frontage to Atkins Drive with the only sections of the dwellings within 3.0m and 3.3m of the frontage being when the frontage starts to curve around the corner.

Given the positioning of the dwellings, the topography and shape of the site following subdivision which follows the edge of the roadway it is considered that the setback of the dwellings from the site's frontage will be compatible with the streetscape.



Figure 6 – Showing the proposed new title boundary in relation to the proposed dwelling units 12 and 13 – Extract from Open Space Diagrams as prepared by Jaws Architects, sheet no. DA-04

A3

A dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions that extend not more than 0.9m horizontally beyond the building envelope, must:

- (a) be contained within a building envelope (refer to Figures 8.1, 8.2 and 8.3) determined by:
 - i) a distance equal to the frontage setback or, for an internal lot, a distance of 4.5m from the rear boundary of a property with an adjoining frontage; and
 - ii) projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above existing ground level at the side and rear boundaries to a building height of not more than 8.5m above existing ground level; and
- (b) only have a setback of less than 1.5m from a side or rear boundary if the dwelling:
 - i) does not extend beyond an existing building built on or within 0.2m of the boundary of the adjoining property; or
 - ii) does not exceed a total length of 9m or one third the length of the side boundary (whichever is the lesser).

P3

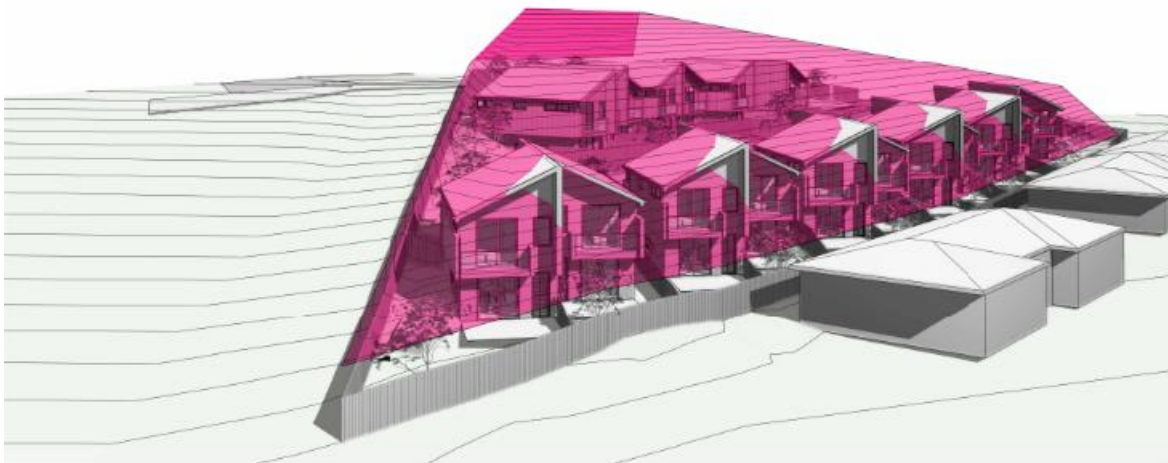
The siting and scale of a dwelling must:

- (a) not cause an unreasonable loss of amenity to adjoining properties, having regard to:
 - i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;
 - ii) overshadowing the private open space of a dwelling on an adjoining property;
 - iii) overshadowing of an adjoining vacant property; and
 - iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property;
- (b) provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area; and
- (c) not cause an unreasonable reduction in sunlight to an existing solar energy installation on:
 - i) an adjoining property; or
 - ii) another dwelling on the same site.

Assessment:

P3 –

The proposal relies on assessment against the Performance Criteria as part of the roof in relation to dwellings 1 to 11 along the north western boundary is to be located outside the building envelope – see figure 7 below showing the extent of the proposed dwellings located outside of the building envelope on the northern perspective facing the adjoining property at 1 Atkins Drive.



BUILDING ENVELOPE NORTH PERSPECTIVE

Figure 7 – Building envelope north perspective as prepared by Jaws Architects (extract from 3D Perspective View - Heights, as prepared by Jaws Architects, sheet no DA-09)

The Performance Criteria tests are as follows:-

The siting and scale of a dwelling must:

- (a) not cause an unreasonable loss of amenity to adjoining properties, having regard to:*
 - (i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;*
 - (ii) overshadowing the private open space of a dwelling on an adjoining property;*
 - (iii) overshadowing of an adjoining vacant property; and*
 - (iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property;*
- (b) provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area; and*
- (c) not cause an unreasonable reduction in sunlight to an existing solar energy installation on:*
 - (i) an adjoining property; or*
 - (ii) another dwelling on the same site.*

Justification has been provided by the applicant within the supporting planning report submitted with the application as follows:-

The following provides a response to each criteria.

P3 (a) Reduction in sunlight to a habitable room

The site has a favorable north-westerly orientation, with a variable gradient that rises to the south-east. The site constitutes the eastern most residential property along Atkins Drive, with the Parklands Highschool adjoining the site to the east.

To the west, the site adjoins an existing residential unit development at 1 Atkins Drive.

The location of the subject site to west of 1 Atkins Drive, combined with the rising gradient, confirms the proposal will not overshadow 1 Atkins Drive at any point on June 21st (when the sun is at its lowest point) – as shown in the accompanying shadow diagrams.

Overshadowing of private open space

As outlined above, the location, orientation and gradient across the site ensures the proposed units will not overshadow the private open space of adjoining units at 1 Atkins Drive at any point, on June 21st.

Visual Impacts, caused by apparent scale, bulk or proportions

Due to the split-level design and staggered siting pattern to retain consistent levels, orientation and overall design in response to the topography, parts of the south-west facing elevations of Units 3, 5, 7, 9 and 11 extend beyond the building envelope. These sections present wall heights of up to 8.1m.

To mitigate potential visual impacts, Unit 1, 2, 4, 6, 8, 10 and 11 have a minimum setback of 3m between the westernmost edge of the units and the shared boundary. Unit 3, 5, 7 and 9 extend slightly closer, with a minimum setback of no less than 2m. However, due to the staggered and offset siting of the north-western façades, the north-eastern corner of each unit and associated south-west facing wall of the adjoining unit increase to well over 4m.

The staggered siting and offset unit orientation, along with the alternating roof forms and gradients greatly reduce the perceived height and bulk of the units. Together with highlight windows across the upper level of each unit, and use of light-colored external materials/finishes (white brickwork and shiplock timber cladding), the design provides a level of façade articulation and variation which significantly reduces bulk and proportions.

When combined, these design responses effectively break down the scale, bulk and proportions of the units to a degree that sufficiently mitigates the potential for visual impact, to an extent that is not considered unreasonable.

P3 (b) – Separation between adjoining dwellings

The proposal must also demonstrate that separation between the proposed units and adjoining dwellings is consistent with that found on established properties in the area.

Units 1 to 11 adopt minimum setbacks ranging from 2m to 3m at the shortest point from the shared boundary. This increases to well over 4m on the north-eastern edge of the units, due to the north-west façade orientation. Whilst an assessment under the performance criteria cannot have regard to a provision under the corresponding acceptable solution, it is noted that a setback of 1.5m to a side boundary is deemed acceptable under A3(b).

The existing units on the adjoining site at 1 Atkins Drive incorporate a consistent setback of approximately 2.1m. Combined, this achieves a minimum separation of approximately 4.1m.

A review of separation distances between adjacent dwellings is provided below.

No. of Dwellings	Street Name / No.	Minimum Separation Distance
Atkins Drive		
1	2 Atkins Dr / 4 Atkins Dr	6.6m
2	4 Atkins Dr / 6 Atkins Dr	5.4m
3	6 Atkins Dr / 8 Atkins Dr	4.1m
4	10 Atkins Dr / 12 Atkins Dr	6.8m
5	12 Atkins Dr / 14 Atkins Dr (Unit 2)	4.4m
6	18 Atkins Dr / 20 Atkins Dr	6m
7	20 Atkins Dr / 22 Atkins Dr	5.6m
8	22 Atkins Dr / 24 Atkins Dr	4.9m
Cedar Place		
9	2 Cedar Pl / 4 Cedar Pl	2.4m
Banksia Court		
10	1 Banksia Cr / 3 Banksia Cr	6.5m
11	3 Banksia Ct / 5 Banksia Ct	3.9m
12	5 Banksia Cr / 7 Banksia Ct	4m

The table above demonstrates a relatively high degree of variation in dwelling separation across established properties in the immediate area. This is influenced by variations in lot dimensions, frontage orientation and siting of dwellings.

Compared to the proposed development, the minimum separation of 4.1m between Units 1 to 9 and the adjoining units at 1 Atkins Drive is broadly consistent with that found between established properties in the area and complies with P3 (b).

P3 (c) – overshadowing of solar energy installations on adjoining properties

The proposed development will not overshadow any adjoining residential properties. Whilst no solar panels are currently proposed.

The proposal complies with P3.

Planner Response:-

- (a) not cause an unreasonable loss of amenity to adjoining properties having regard to:-
- i. reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on and adjoining property

Comment:- The shadow diagrams provided show shading on the 21 June at 9am, 10am, 12pm, 1pm, 2.30pm and 3pm from two different angles – see drawing sheet no's DA08 and DA-13. Shadow diagrams confirm that there will be no overshadowing to the adjoining property at 1 Atkins Drive.

- (ii) overshadowing the private open space of a dwelling on an adjoining property;

Comment:- Shadow diagrams show that there will be no overshadowing of private open space associated with adjoining properties.

(iii) overshadowing of an adjoining vacant property; and

Comment:- There are no adjoining vacant properties.

(iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property;

Comment:- The main part of the development concerned with visual impact are the proposed dwellings on the northwestern part of the site which comprise of dwellings 1 to 11 as the apex of the pitched roofs sit outside the building envelope.

These dwelling are situated adjacent to the sites side boundary to 1 Atkins Drive which contains an existing Multiple Dwelling development.

There are six dwellings adjacent to the proposed development which are contained within three buildings each containing two units.

In order to ascertain whether the visual impacts caused by the apparent scale, bulk or proportions of the proposed development when viewed form the adjoining property at 1 Atkins Drive are responsible or unreasonable, the existing amenity experienced by the dwelling units on the adjoining property and the level of amenity following the proposed development needs to be established.



Figure 8 – Aerial image of the subject site showing the adjoining dwelling units and their numbers at 1 Atkins Drive (Source List Maps <https://maps.thelist.tas.gov.au/listmap/app/list/map>)

In relation to the existing dwelling units on the adjoining property at 1 Atkins Drive, the front of

each unit faces onto the internal roadway which forms part of the existing multiple dwelling development.

Private open space is located at the rear of each dwelling with dwellings 22, 19, 18 and 17 to a certain extent also having area of private open space to the side of the dwellings.

The list property reports indicate four of the units, units 19 and 20 and units 21 & 22 were constructed in 1985 and units 17 & 18 constructed in 1992.

A floor plan obtained of unit 18 indicates as shown in figure 9 shows that each unit contains two bedrooms with the main living space including kitchen/dining and living room located at the front of the dwellings (fronting onto the shared driveway).

The rear of the dwelling units adjoining the subject site contains utility areas being bathroom and laundry along with a bedroom window and is unlikely to experience any significant visual impact from the proposed development.

It appears from the size and external design of each of the six units that each would have a similar floor plan.

The view from each of the adjoining dwelling units (17 to 22) looks out across the internal shared driveway to the adjoining units and out over the residential development within the Romaine and Upper Burnie area.

It is considered that the primary outlook for the adjoining dwelling units is from the front (being the northern western elevation) which contains the living areas of the dwellings of which the proposed development would not impact.

Each unit contains an area of private open space as can be seen in figures 10 & 11 below in relation to unit 18 and 19 which may contain clothes drying facilities and outdoor recreation (ie: seating and landscaped garden area).

A timber log retaining wall structure appears to sit on or near the title boundary between the private open space and the subject site and existing vegetation is located along this boundary of the subject site.

The existing/demolition site plan as submitted with the proposal, drawing no. DA-02 shows that the existing vegetation on the southwestern corner will be removed.

Access is provided to the rear space of each dwelling on the adjoining site by a door in the laundry.

The standard requires that the siting and scale of the proposed dwellings do not cause an unreasonable loss of amenity to adjoining properties through visual impacts caused by the apparent scale, bulk and proportions of the dwelling when viewed from an adjoining property.

Amenity is defined in the Scheme as “in relation to a locality, place or building, any quality, condition or factor that makes or contributes to making the locality, place or building harmonious, pleasant or enjoyable.”

Visual impact is not defined in the Scheme. In *McCullugh v Glamorgan Spring Bay Council [2019] TASRMPAT 30 at [22]- [23]*, the Tribunal accepts that the visual impact of a development is generally a combination of visual effects and visual sensitivity. The visual effects relate to the

nature of the Proposal and whether it is consistent or harmonious with the surroundings”

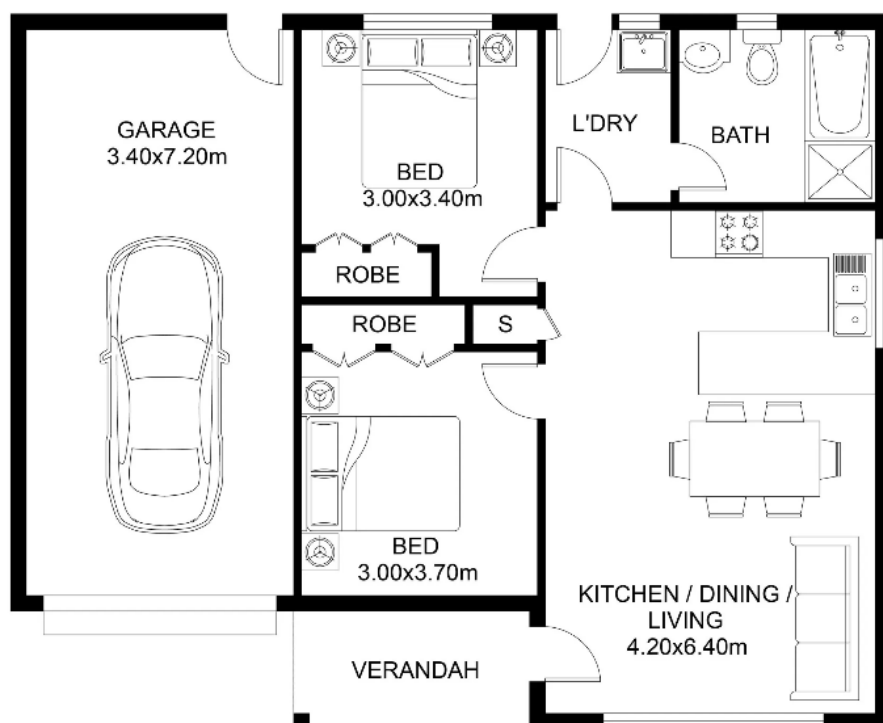


Figure 9 – Image extracted from a Harcourts Real Estate Advertisement for 18/1 Atkins Drive, dated May 2024 and accessed 8 October 2025 - <https://www.domain.com.au/property-profile/18-1-atkins-drive-romaine-tas-7320>



Figure 10 – Image extracted from a Harcourts Real Estate Advertisement for 18/1 Atkins Drive, dated May 2024 and accessed 8 October 2025 - <https://www.domain.com.au/property-profile/18-1-atkins-drive-romaine-tas-7320>



Figure 11 – Unit 19/1 Atkins Drive showing the rear of the dwelling located in the second building within the complex adjacent to the subject site – image from [domain.com.au](https://www.domain.com.au/property-profile/19-1-atkins-drive-romaine-tas-7320), taken November 2022 - <https://www.domain.com.au/property-profile/19-1-atkins-drive-romaine-tas-7320>

The proposed dwellings will present as two storeys when viewed from the site at 1 Atkins Drive.

The dwellings will have a typical setback from the northwestern boundary of 3.0m – see figure 12 below. On the proposed site plan the darker grey area labelled deck is the deck accessible from the ground floor. This deck is a ground level deck and is not considered in the building setback. The first-floor balcony protrudes over 0.9m of this deck.

The dwellings will have a total height of approximately 8.3m at their highest point see figure 13 below.

The proposed dwellings will be angled on the site to face north therefore not directly towards the adjoining dwelling at 1 Atkins Drive.

The application includes a landscaping plan, drawing no. DA-10. The plan shows that a number of new 'Banksia Marginata' otherwise known as Silver Banksia will be planted. This species can occur as a shrub or tree ranging in height from 2 to 5m with a spread of 1.5 to 4m <https://sequoiavalleyfarms.com.au/silver-banksia>. The tree is said to be hardy and fast growing, long living species.

The Landscape Plan also shows the treatment of external property boundaries will be treated as timber fencing finished in a white colour.

Given the topography of the site, any development placed on the site at 3 Atkins Drive will be higher in elevation than the existing development at 1 Atkins Drive.

The planning scheme allows for two story development, however, due to the topography of the site and when the building envelope follows these contours it creates an uneven building envelope.

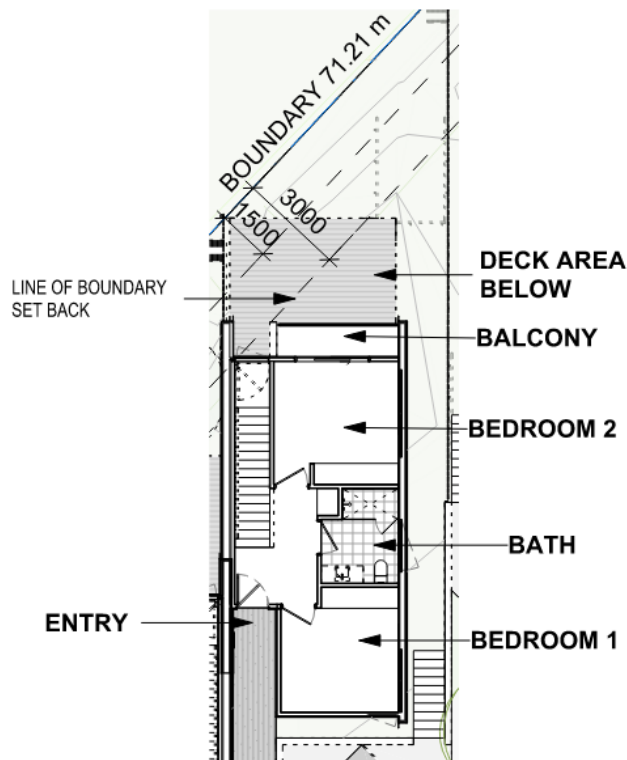
The word to consider within the Performance Criteria is 'unreasonable'. Are any visual impacts caused by the apparent scale, bulk or proportions of the dwellings unreasonable when viewed from

an adjoining property?

Unreasonable is not defined within the planning scheme. The general meaning within the Macquarie dictionary is '*exceeding the bounds of reason; immoderate; exorbitant*' meaning not excessive.

The external cladding of the proposed dwelling and the appearance of the dwellings lessen the bulk by providing articulation and three-dimensional building surfaces.

The walls and roof protrude approximately 0.9m out from the bulk of the building (acting as an eve) and screen to the side of the balcony. The dwellings incorporate a variety of cladding in light colours including shiplap timber mimic cladding, masonry block walls in white paint finish, longline profile wall and roof cladding with aluminium windows and glass. The variety of cladding and light-coloured materials assist to break up the dwelling bulk along with the profile and protrusion of the walls and roof from the main building which assists with breaking up the bulk. Shadows cast by the protrusion rather than a solid surface with dark cladding or little articulation contributes to breaking up the bulk of the building.

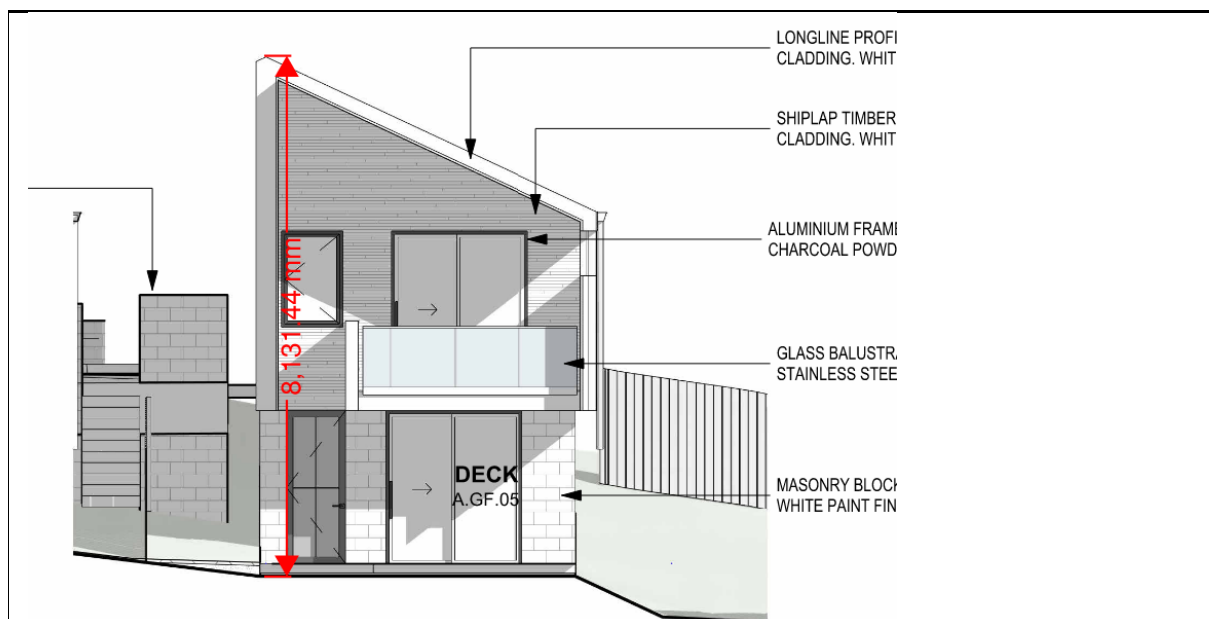


2

PROPOSED TYP. BALCONY SETBACK

REFER: DA-03
SCALE: 1 : 200

Figure 12 – Extract from Proposed Site Plan, as prepared by Jaws Architects, drawing no. DA-03 showing typical setback of the proposed dwelling units from the side boundary to 1 Atkins Drive



6

TYPICAL NORTHERN ELEVATION

Figure 13 – Showing total height of the proposed dwelling units (extract taken from Typical Unit Plans, as prepared by Jaws Architects, sheet no. DA-07 – Height measured in BlueBeam



Figure 14 – North Perspective – Lower Dwelling – showing the elevation of the proposed dwelling units adjacent to 1 Atkins Drive (Extract from Site Elevations, as prepared by Jaws Architects, sheet no. DA-05)

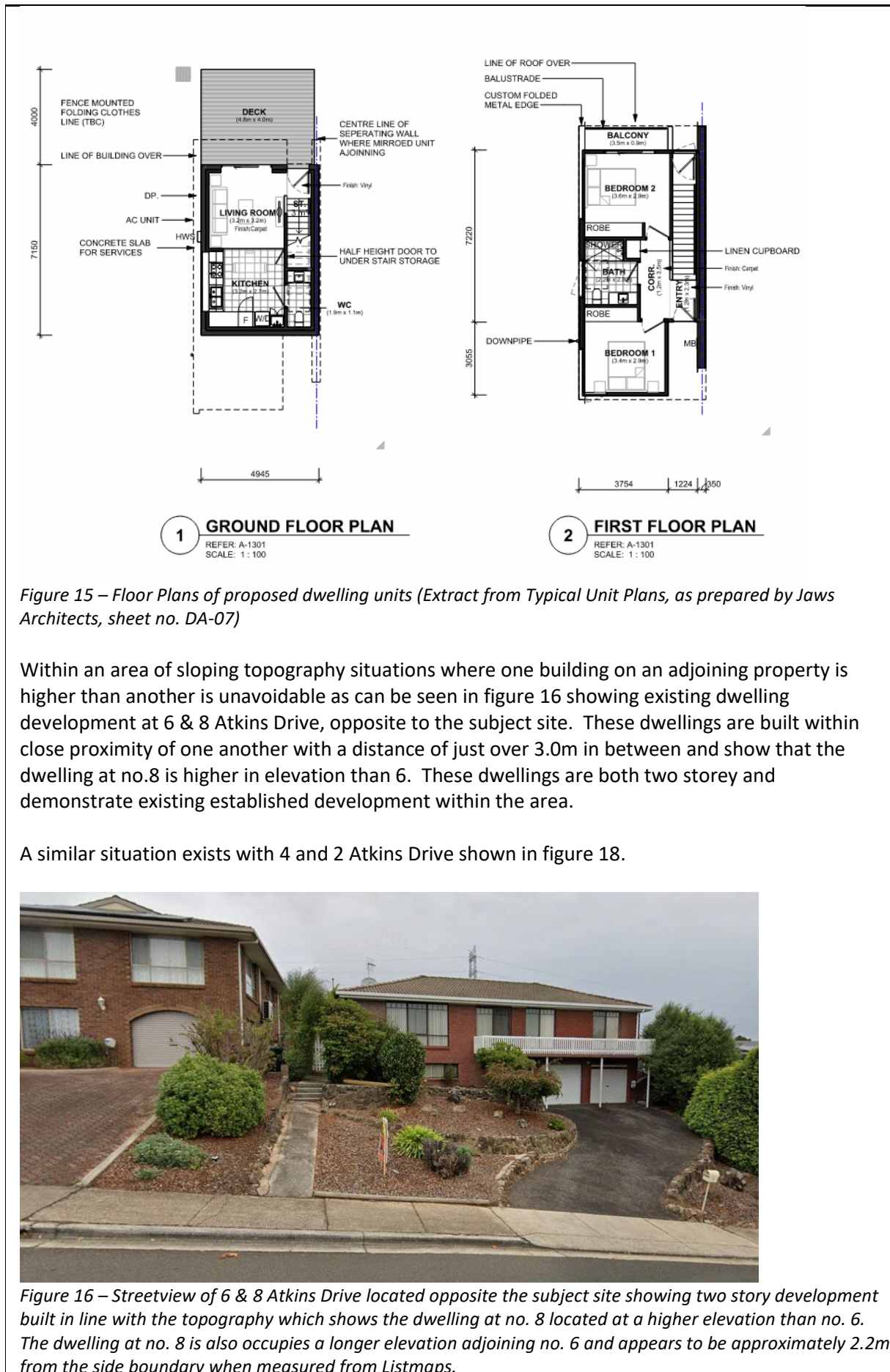




Figure 17 – Image from List Maps showing 6 & 8 Atkins Drive and the separation distance between dwellings - <https://maps.thelist.tas.gov.au/listmap/app/list/map>



Figure 18 – Shows 4 & 2 Atkins Drive with 4 Atkins Drive being two story and located above the dwelling at 1 Atkins Drive Source: GoogleMaps, accessed 9 October 2025.

In this case, the primary outlook in relation to the dwelling units on the adjoining property is to the northwest away from the subject site. There is no doubt that the proposed dwelling units will cause a visual impact when viewed from the adjoining property at 1 Atkins Drive however, the impact of visual bulk and scale when the proposed dwelling units are viewed from the private open space at the rear of the existing dwelling units will be lessened by the distance of the proposed dwellings from the title boundary being 3m, the use of a variety of light coloured cladding materials, the provision of new vegetation and the ability for additional vegetation to be planted.

In this case, it is considered that due to the above, the visual impact is not unreasonable in the context of the topography of the area .

- b) provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area.*

Planner Comment: The application has submitted an examination of existing separation distance within the area, and it is concluded that the proposal has demonstrated compliance with this part of the Performance Criteria.

- c) not cause an unreasonable reduction in sunlight to an existing solar energy installation on an adjoining property or another dwelling on the same site.*

Planner Comment: There are no existing solar energy installations.

Table 3 – Assessment of Clause 8.4.7 (P1) of the scheme

8.4.7 Frontage fences for all dwellings	
Objective:	<p>The height and transparency of frontage fences:</p> <ul style="list-style-type: none"> (a) provides adequate privacy and security for residents; (b) allows the potential for mutual passive surveillance between the road and the dwelling; and (c) is reasonably consistent with that on adjoining properties.
Acceptable Solutions	Performance Criteria
<p>A1 No Acceptable Solution.</p> <p><i>An exemption applies for fences in this zone – see Table 4.6</i></p>	<p>P1 A fence (including a free-standing wall) for a dwelling within 4.5m of a frontage must:</p> <ul style="list-style-type: none"> (a) provide for security and privacy while allowing for passive surveillance of the road; and (b) be compatible with the height and transparency of fences in the street, having regard to: <ul style="list-style-type: none"> i) the topography of the site; and ii) traffic volumes on the adjoining road.
<p>Assessment:</p> <p>P1 –</p> <p>A front fence is proposed to extend onto the new title boundary frontage adjacent to units 12, 13, 14 & 15 – see figure 19 below. The fence proposed is a solid timber paling fence to a height of 1.7m and white in colour.</p>	

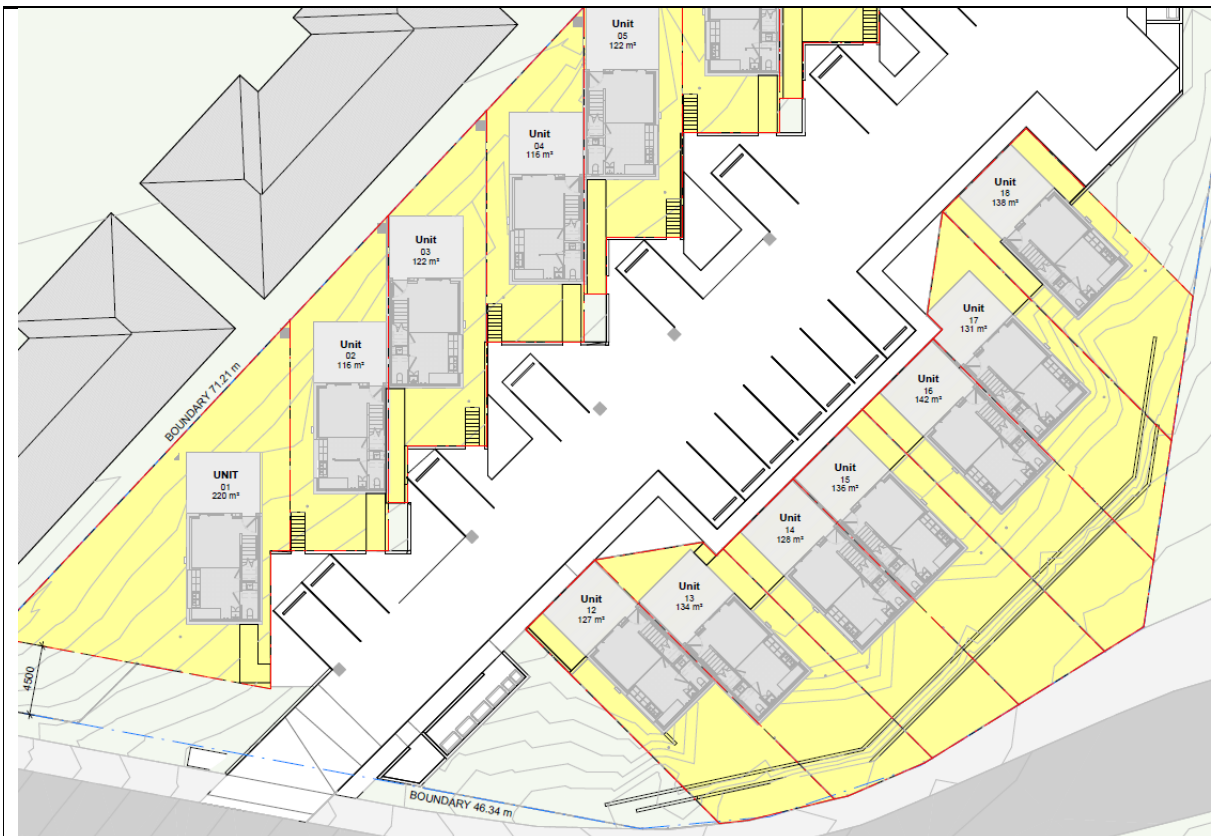


Figure 19 – Extract from Open Space Diagrams – Individual Lot Area, as prepared by Jaws Architects drawing no. DA-04. The red lines showing proposed fencing starting towards the rear of proposed unit 12 and 4.5m from the sites frontage adjacent to unit 01.

The supporting planning report prepared by ireneinc & smithstreet studio submitted with the application states:

No fencing is proposed within 4.5m of the frontage between Unit 1 and Unit 12, except for a low freestanding wall forming part of the waste bin and letter box enclosure. The wall varies between 950mm to 1.1m high within 4.5m of the frontage, satisfying the exemption above.

However, due to the route of the adjoining road reservation, the frontage technically continues beyond Unit 12, terminating at the rear of Unit 15. A 1.7m high timber fence is proposed along this section. Therefore, a response to the performance criteria is required.

As the fence is solid it cannot meet the exemption under table 4.6 of the planning scheme which exempts a front fence within 4.5m of a frontage if it is not higher than 1.8m and has a transparency of at least 30% above 1.2m high. Therefore, the proposed fence must be assessed against the Performance Criteria. It is noted that the fence proposed in front of dwelling unit 1 is exempt under table 4.6 as it is 4.5m back from the sites frontage.

The following justification against the Performance Criteria is provided in the supporting planning report.

The 1.7m high timber fence will provide privacy and security to Units 12 to 18, which sit approximately 2.4-2.7m below the existing footpath level. Due to this, several terraces are proposed to the rear of each unit. The fence will act as a side boundary fence, preventing people from climbing over the fence from the adjoining school grounds.

Whilst this portion of the fence could be provided with openings for passive surveillance, its purpose is essentially a boundary fence which fronts a portion of road and footpath that essentially serves as

access to the adjoining Parklands High School. The potential streetscape impact of fence along this section would appear limited given the above.

The fencing would have no impact on sight-lines, as it is well clear of the proposed site access.

The side boundary fence along the north-western elevation, extending within 4.5m of the frontage, can be reduced in height to 1.2m if required.

Planner Comment:

The proposed fencing arrangement allows for a visually open entrance into the proposed development with a wide driveway entry with low level retaining walls.

The fencing proposed adjacent to units 12, 13, 14 and 15 will allow for security between the proposed dwellings and Parklands High School. Whilst technically a frontage, this part of the title curves around the corner of Atkins Drive and the private roadway that leads into Parklands High School therefore lessening any impact to streetscape as the site curves away from Atkins Drive.

Due to the topography of the site and the curve of the frontage away from Atkins Drive the fencing will seem less intrusive to the landscape.

The majority of residential properties within the area do not have front fences presumably due to covenants put on at the time of subdivision that did not allow for front fences within 4.5m of a frontage. There are no covenants on the subject title.

The dwelling on the corner of Atkins Drive and Mount Street has solid fencing extending up the northern side of Atkins Drive whilst the dwelling on the corner of Atkins Drive and Cedar Place also has fencing on its Atkins Drive frontage extending up the southern side of Atkins Drive, however, from this point into Atkins Drive there is no front fencing and houses on the southern side sit up considerably higher than the road pavement. Retaining walls are common and feature within properties and on property boundaries.



Figure 20 – Image from Google Street view showing the entry to Atkins Drive – accessed 15 October 2025

The properties directly adjoining the site are 1 Atkins Drive which does not have frontage fencing and Parklands High School.

Given that the subject site at 3 Atkins Drive adjoins Parklands High School and the southeastern portion of the site curves away from Atkins Drive and forms a frontage with the private roadway to Parklands High School it is reasonable to expect fencing to be provided along this boundary for safety and security. Passive surveillance into the subject site is still readily available from Atkins

Drive.

With regards to consistency of the proposed fencing arrangement with adjoining properties, the subject site, whilst the southeastern corner is technically a frontage, still acts in a similar fashion to a corner lot or side boundary and it is reasonable to expect that fencing is required for privacy and to enclose private open space.

The site is on the northern side of Atkins Drive and adjoins Parklands High School. The site to the west of the site at 1 Atkins Drive has no fencing whilst the site adjoining 1 Atkins Drive has solid timber paling fencing on its Atkins Drive elevation. Therefore, the northern side of Atkins Drive has little consistency in fencing arrangements.

It is concluded that the proposal can comply with the Performance Criteria.

Table 4 – Assessment of Clause 8.4.8 (P1) of the scheme

8.4.8 Waste storage for multiple dwellings	
Objective:	To provide for the storage of waste and recycling bins for multiple dwellings.
Acceptable Solutions	Performance Criteria
<p>A1 A multiple dwelling must have a storage area, for waste and recycling bins, that is not less than 1.5m² per dwelling and is within one of the following locations:</p> <ul style="list-style-type: none"> (a) an area for the exclusive use of each dwelling, excluding the area in front of the dwelling; or (b) a common storage area with an impervious surface that: <ul style="list-style-type: none"> i) has a setback of not less than 4.5m from a frontage; ii) is not less than 5.5m from any dwelling; and iii) is screened from the frontage and any dwelling by a wall to a height not less than 1.2m above the finished surface level of the storage area. 	<p>P1 A multiple dwelling must have storage for waste and recycling bins that is:</p> <ul style="list-style-type: none"> (a) capable of storing the number of bins required for the site; (b) screened from the frontage and any dwellings; and (c) if the storage area is a common storage area, separated from any dwellings to minimise impacts caused by odours and noise.
<p>Assessment:</p> <p>P1 -</p> <p>Two common bin storage areas are proposed within the site, one storage area at the front of the site and one storage area towards the rear of the site.</p> <p>The storage area at the front of the site is partially within 4.5m of the frontage and is therefore reliant on assessment against the Performance Criteria.</p> <p>The supporting planning report submitted with the application provides the following justification:- <i>The communal waste storage areas accommodate bins sufficient in size and number to support the proposed development. Each area is provided with screening and appropriate separation from units on the same site, and the frontage to Atkins Drive.</i></p>	

Planner Comment

The bin area is enclosed with masonry block walls with a white paint finish. As can be seen in the three-dimensional shadow diagram below, the bin storage area will be lower than the road. A combination of the fencing and brick masonry walls will cancel the bin area from view from Atkins Drive.

The proposed bin storage areas are capable of storing the number of bins required for the site, are screened from the frontage and dwellings and are separated from bins to minimise impacts caused by odours or noise.

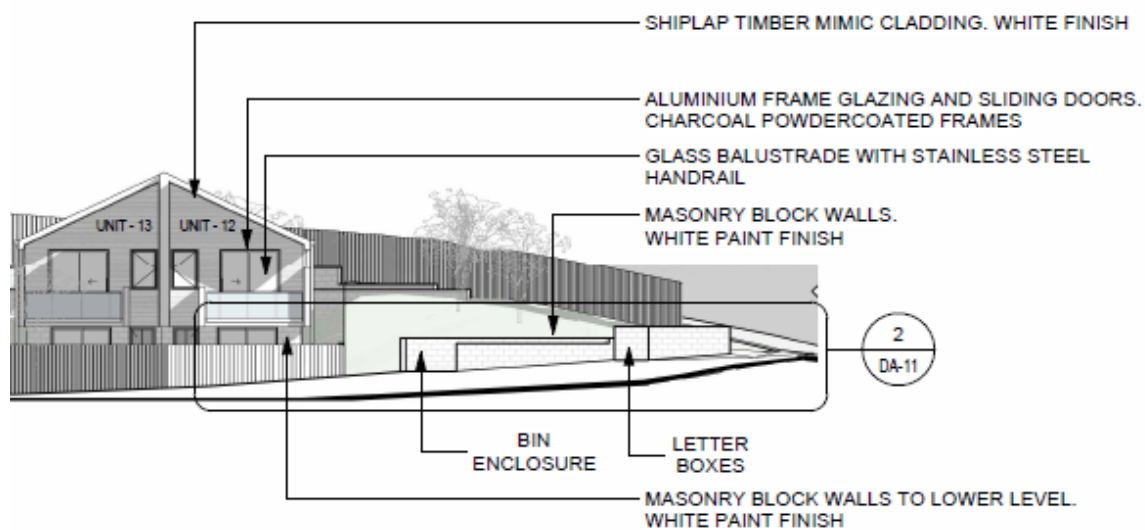


Figure 21 – Extract from Site Elevations, as prepared by Jaws Architects, drawing no. DA-05 showing the bin enclosure area

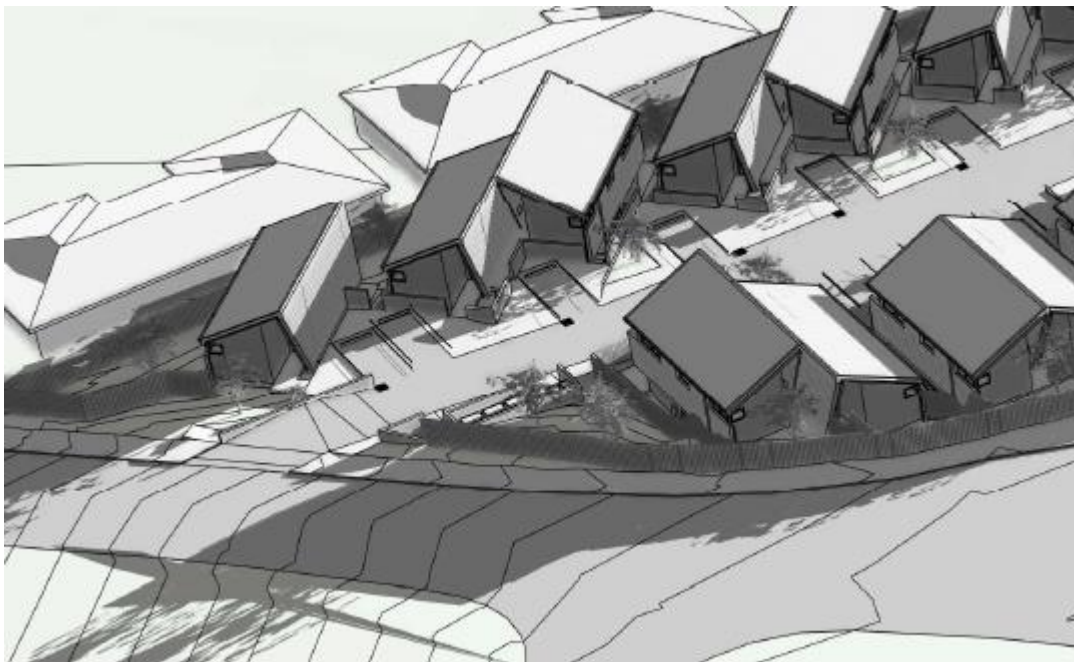


Figure 22 – Extract from Shadow Diagrams, as prepared by Jaws Architects, drawing no. DA-08, showing bin enclosure area adjacent to dwelling unit no. 12

Table 5 – Assessment of Clause C2.5.1 (P1.2) of the scheme

C2.0 Parking & Sustainable Transport Code

C2.5.1 Car parking numbers	
Objective:	That an appropriate level of car parking spaces are provided to meet the needs of the use.
Acceptable Solutions	Performance Criteria
<p>A1 The number of on-site car parking spaces must be no less than the number specified in Table C2.1, less the number of car parking spaces that cannot be provided due to the site including container refund scheme space, excluding if:</p> <ul style="list-style-type: none"> (a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan; (b) the site is contained within a parking precinct plan and subject to Clause C2.7; (c) the site is subject to Clause C2.5.5; or (d) it relates to an intensification of an existing use or development or a change of use where: <ul style="list-style-type: none"> i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows: <p style="text-align: center;">$N = A + (C - B)$</p> <p>N = Number of on-site car parking spaces required</p>	<p>P1.1 The number of on-site car parking spaces for uses, excluding dwellings, must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the availability of off-street public car parking spaces within reasonable walking distance of the site; (b) the ability of multiple users to share spaces because of: <ul style="list-style-type: none"> i) variations in car parking demand over time; or ii) efficiencies gained by consolidation of car parking spaces; (c) the availability and frequency of public transport within reasonable walking distance of the site; (d) the availability and frequency of other transport alternatives; (e) any site constraints such as existing buildings, slope, drainage, vegetation and landscaping; (f) the availability, accessibility and safety of on-street parking, having regard to the nature of the roads, traffic management and other uses in the vicinity; (g) the effect on streetscape; and (h) any assessment by a suitably qualified person of the actual car parking demand determined having regard to the scale and nature of the use and development. <p>P1.2 The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:</p> <ul style="list-style-type: none"> (a) the nature and intensity of the use and car parking required; (b) the size of the dwelling and the number of bedrooms; and (c) the pattern of parking in the surrounding area.

<p>A = Number of existing on site car parking spaces</p> <p>B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1</p> <p>C= Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1.</p>	
<p>Assessment:</p> <p>P1.2 –</p> <p>The proposal is for 18 multiple dwellings. For multiple dwellings the scheme requires for a 2 bedroom dwelling, 2 spaces per dwelling and for visitor parking for multiple dwellings in the General Residential zone, 1 dedicated space per 4 dwellings (rounded up to the nearest whole number).</p> <p>Therefore, to be compliant with the Acceptable Solution the proposal requires a total of 36 spaces along with 5 additional spaces for visitors making 41 spaces required in total.</p> <p>The proposal seeks to provide a total of 18 individually accessible spaces, one space dedicated to each dwelling. Therefore, the proposal relies on assessment against the Performance Criteria under P1.2 which states that:-</p> <p><i>The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:</i></p> <ul style="list-style-type: none"> <i>(a) the nature and intensity of the use and car parking required;</i> <i>(b) the size of the dwelling and the number of bedrooms; and</i> <i>(c) the pattern of parking in the surrounding area.</i> <p>A Traffic Impact Assessment has been submitted with the proposal prepared by Hubble Traffic and dated June 2025.</p> <p>The TIA provides data relating to car ownership for people living in social and affordable housing. The data states that car parking demand of Tasmanian social and affordable housing sites varying between 0.47 to 0.72 cars per dwelling.</p> <p>The TIA states that the planning scheme does not account for low car ownership among social and affordable housing tenancy and the accessibility to a frequent public transport route.</p> <p>The supporting planning report submitted with the application provides the following justification:-</p> <p><i>The proposal is for social/affordable housing, typically catering for lower-income, disadvantaged people and families who generally have reduced car ownership. Therefore, the anticipated demand for on-site parking is expected to be lower than may otherwise the case. The provision of one-space per unit is considered sufficient given the nature of the use to meet the reasonable needs of future residents.</i></p>	

The site is also within proximity to several existing public transport stops, with up to five located within 220m of the site. This includes one located along Manuka Drive to the south, two located further west along Three Mile Line Road and a further two stops to the north-west along Mount Street.

With respect to visitor parking, significant on-street public parking is available along Atkins Drive, Manuka Drive and Banksia Court, which is considered appropriate to cater for visitor demand.

A more detailed response demonstrating compliance with P1 is provided in the accompanying traffic impact assessment.

In relation to reduced visitor parking numbers the TIA states:-

In optimising the number of social and affordable apartments, only 18 on-street parking spaces can be achieved. If these spaces are occupied by tenants, visitors will need to utilise the available on-street parking. Lower car ownership is typically associated with low-income households, which may result in reduced visitor parking demands compared to private housing. The planning scheme specifies a visitor parking rate of one space per four multiple dwellings for private housing; however, it would be reasonable to reduce this rate to one space per eight social or affordable apartments. Consequently, the development may generate a lower visitor parking demand of two to three vehicles.

A recent site inspection found that the surrounding residential properties generate a low on-street parking demand, and the nearby school has on-site parking spaces. The development site has road frontage exceeding 30 metres, suitable for accommodating on-street parking for six vehicles. This suggests that visitor parking is unlikely to adversely impact the surrounding residential properties, and on-street parking can be accommodated without causing impact to traffic flow.

On page 29 the TIA provides the following justification:-

Performance criteria	Assessment
The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:	
a) the nature and intensity of the use and car parking required;	The development is for residential units, which are situated within close proximity to public bus stops, providing tenants with an alternative mode of transport. This assessment has demonstrated that one parking space per each apartment is considered acceptable, minimising parking overflow.
b) the size of the dwelling and the number of bedrooms; and	The development consists of 18 two-bedroom units, which will be allocated to social and affordable housing tenants. Each unit will be less than 80 square metres in size.
c) the pattern of parking in the surrounding area.	The surrounding residential properties and Parklands High School have off-street parking facilities, generating a low demand for on-street parking. Atkins Drive has sufficient width to accommodate on-street parking, while maintaining efficient traffic flow for the road function. The development site has at least 30 metres of road frontage to accommodate on-street parking without adversely impacting surrounding properties.

The TIA sights Queensland Government published design standards for construction of social housing which recognises that social housing in accessible areas requires a lower number of on-site parking spaces along with a Victorian GTA report which undertook research for car parking rates associated with social housing development at 67 housing sites comprising of 802 dwellings. For a middle metro site, the car parking rate is 0.74 spaces per two bedroom apartment.

It also provides data in a Tasmanian context from Centacare Evolve Housing, Homes Tasmania and data from Queens Walk Complex located in Hobart indicating a rate of car ownership to dwellings between 0.47 to 0.72 per dwelling.

The site provides access to bus stops which are located within the near vicinity.

The objective of the standard is that an appropriate level of car parking spaces are provided to meet the needs of the use.

It is concluded that the proposal has provided adequate justification against the Performance Criteria and that the proposed number of car parking spaces can meet the needs of the proposed use.

Representations

Clause 6.10.1 of the Scheme requires a decision on a permit application must take into consideration all applicable standards and requirements of the planning scheme and any representations made during the public exhibition period, but only insofar as each such matter is relevant to the particular discretion being exercised.

In this instance, the application is discretionary as it relies on Performance Criteria for grant of a permit including:-

- Clause 8.4.1 – Residential Density for Multiple Dwellings (P1) in relation to the land size and the number of dwellings proposed;
- Clause 8.4.2 – Setbacks and building envelope for all dwellings (P1 & P3) – due to units 12 & 13 having a setback of 3.3m and 3m to the site's frontage following the proposed subdivision and units 1 to 11 being partially located outside of the building envelope;
- Clause 8.4.7 – Frontage fences for all dwellings (P1) due to the proposed 1.7m fence adjacent to units 12, 13, 14 & 15;
- Clause 8.4.8 – Waste Storage for Multiple Dwellings (P1) due to proposed waste storage being located within 4.5m of the sites frontage; and
- Clause C2.5.1 – Car parking Numbers (P1.1)

There were 5 (one including 5 signatories) representations received during the public exhibition period which occurred from 6 until 22 September 2025.



It is noted that two additional representations were received outside of the public exhibition period on the 6 October 2025, 14 days past the closing date for representations. Council is unable to accept the representations given it is 14 days post the exhibition period however note that the matters raised within the submissions included traffic and carparking. Similar matters have been raised by others and discussed below.

The representations and planning responses to the issues raised are provided below. While every effort has been made to include all issued raised, this summary should be read in conjunction with the representations which are included as an enclosure to this report.

Matter Raised	Comment
<p>Impact on real estate values</p> <p>Property devaluation and income loss</p> <p>The proposed development will significantly devalue surrounding properties, including my own investment units.</p> <p>The loss of privacy, increased noise, and diminished neighbourhood appeal will make these properties less attractive to tenants, resulting in reduced rental income and long-term financial harm to owners.</p>	<p>Real estate values is not a matter considered under the <i>Tasmanian Planning Scheme</i>.</p> <p>See discussion below and within the body of report in relation to privacy.</p> <p>Noise is not a factor assessment under the Tasmanian Planning Scheme in its own right, see discussion relating to amenity within Clause 8.4.2.</p>
<p>Low income high density project</p>	<p>The application states that the purpose of the development is to provide social and affordable housing. The planning scheme does not distinguish the purpose of residential development. All residential developments are assessed under the same provisions.</p> <p>Please see density discussion below and within Clause 8.4.1 above.</p>
<p>Density and overdevelopment</p> <p>The scheme requires 325m² of land per dwelling. This proposal provides only 225m² per dwelling. This represents clear overdevelopment and is inconsistent with the area's character.</p>	<p>Density for multiple dwellings is assessed under Clause 8.4.1 of the <i>Tasmanian Planning Scheme</i>.</p> <p>The objective of the standard is that the density of multiple dwellings makes efficient use of land for housing and optimizes the use of infrastructure and community services.</p> <p>The standard provides two pathways to compliance with the objective of the standard, the Acceptable Solution pathway or alternatively the Performance Criteria pathway.</p> <p>The proposal relies on assessment against the Performance Criteria as it cannot meet the site area required under the Acceptable Solution of 325m² per dwelling.</p>

	<p>The Performance Criteria requires that proposed development will not exceed the capacity of existing infrastructure services and that the proposal can meet either (a) or (b) of the Performance Criteria.</p> <p>It is concluded that the proposal can meet the requirements under P1 (b) the Performance Criteria.</p>
<p>Overlooking</p> <p>Direct overlooking adjoining multiple dwelling complex – elevated development site</p>	<p>See discussion under Clause 8.4.2 (P3) within body of report.</p>
<p>Privacy/Noise</p> <p>Small floor area means residents will seek to use their balconies more. Balconies will directly look down into adjoining complex destroying privacy. Also raises noise concerns from balcony use. 11 balconies directly overlooking unit 18 on the adjoining land.</p> <p>Appears to be no setback from the boundary line.</p> <p>An argument could be made that the left corner of these balconies infringe on our airspace – anything dropped from them would fall directly into our backyards.</p>	<p>Additional information was requested from the applicant in relation to the proposed setback of balconies from the sites side boundary adjoining 1 Atkins Drive.</p> <p>Updated architectural drawings were provided on sheets DA-03 and DA-14 which show that balconies associated with the proposed units will be 3.0m from the northwestern side boundary adjoining 1 Atkins Drive.</p> <p>In this respect, the proposal meets the part of the Acceptable Solution requiring a balcony with a finished floor level 1m or more above existing ground level to be setback not less than 3m from a side boundary unless it is screened.</p> <p>Nevertheless, the application is reliant on the Performance Criteria pathway as the balcony cannot provide a setback of not less than 6m from a dwelling on the same site.</p> <p>See discussion under Clause 8.4.6 – Privacy for all dwellings (P1).</p>
<p>Traffic and Safety Concerns</p> <p>The development will dramatically increase traffic volume on Atkins Drive, a narrow road ill equipped for high density usage. Risk to road safety particularly pedestrians and children. The lack of adequate traffic</p>	<p>A Traffic Impact Assessment, dated August 2025 has been prepared for the proposed development by Hubble Traffic by a suitably qualified person.</p> <p>The Traffic Impact Assessment addresses the relevant Clauses within the Tasmanian</p>

<p>management measures is unacceptable.</p> <p>Parking and traffic flow</p> <p>Provision for excess parking, that is visitors or a household with more than one vehicle is virtually non-existent.</p> <p>This in our opinion will impact negatively on the flow of traffic in this area, because people are going to park in Atkins Drive as they already do quite frequently, and park half on the footpath. It is quite a narrow road for School Buses and school traffic to have to contend with.</p> <p>Traffic, Parking and Safety</p> <p>No visitor parking is proposed despite 34 dwellings across 1 & 3 Atkins combined. This will cause overflow parking and congestion on Atkins Drive. The site is located beside Parklands High School's drop-off/pick-up zone. This area already experiences heavy traffic and pedestrian movement at peak times. Adding further vehicles here poses increased safety risks for students and residents.</p> <p>Queries Traffic Impact Assessment</p> <p>A proper traffic impact assessment should be required.</p>	<p>Planning Scheme.</p> <p>The estimates that the development site could generate 90 daily vehicle Trips.</p> <p><i>It states that 'Atkins Drive is a local residential street, built to an urban standard, with sufficient road width to accommodate on-street parking, while maintaining efficient traffic flow. The proposed new vehicular access onto Atkins Drive will have sufficient available sight distance for a residential property, enabling safe vehicular movements to and from the development site.</i></p> <p><i>Atkins Drive operates under a 50 km/h urban speed limit, with a 40 km/h school zone during drop-off and pick-up times. The school generates increased traffic flow as students arrive and leave, with 345 two-way vehicles observed during the morning peak. Outside of school periods, Atkins Drive is lightly trafficked, with the evening peak generating 125 two-way vehicles. These flows ensure both school and residential users are provided with an acceptable level of service. Traffic modelling indicates that the Mount Street roundabout provides motorists with a high level of traffic efficiency during both peak periods, allowing vehicles to enter and leave Atkins Drive in a safe and efficient manner. The additional traffic generated by the development is not expected to cause any deterioration in traffic performance or adversely impact traffic flow on the surrounding road network. Traffic modelling demonstrated there is spare capacity for incremental growth in the local area for the next decade, without adversely impacting motorists using the roundabout'.</i></p> <p>The assessment found no reason for the development not to proceed on the basis of traffic concerns.</p>
<p>Sewage Pump Station</p>	<p>There has been no suggestion of a sewer pump station.</p>

<p>The need for a sewage pump station may be a cause for concern depending on location and any resulting noise factor</p>	
<p>Environmental Impact</p> <p>Site includes mature vegetation and borders sensitive ecological zones. The removal of trees and disruption of natural habitats will irreversibly damage local biodiversity, threatening native bird species and small mammals. This destruction is both short-sighted and irresponsible.</p>	<p>The existing site plan submitted as part of the application shows that existing vegetation around the southwestern portion of the site is proposed to be removed.</p> <p>The planning scheme does not have any protection mechanisms for this existing vegetation.</p>  <p><i>Figure ? – Aerial photograph showing existing vegetation on the site (Source: ListMaps)</i></p>  <p><i>Figure ? – Extract from drawing sheet no. DA-02 showing the existing vegetation proposed to be removed in red.</i></p>
<p>Neighbourhood character</p> <p>Romaine is a peaceful, low-density residential area with a unique character. The scale and design of the proposed building are completely out of step with the existing streetscape and will erode property values and community cohesion. This development is incompatible with the spirit of our neighbourhood.</p>	<p>See discussion under clause 8.4.1 – Residential density for Multiple Dwellings.</p>
<p>Privacy and amenity/Impact of double</p>	<p>See discussions under Clause 8.4.2 –</p>

<p>story units</p> <p>The height and positioning of the proposed structure will directly overlook neighbouring properties, including mine, resulting in a significant loss of privacy and natural light. This will promote mould and negatively affect the health and wellbeing of long-term tenants who cherish the tranquil setting.</p> <p>Double story units that will impact negatively on the existing units seems totally unfair and inappropriate for this area.</p>	<p>Setbacks and building envelope for all dwellings and Clause 8.4.6 – Privacy for all dwellings within the body of report.</p> <p>Shadow diagrams provided by the applicant show shading on the 21 June at 9am, 10am, 12pm, 1pm, 2.30pm and 3pm from two different angles – see drawing sheet no's DA08 and DA-13. Shadow diagrams confirm that there will be no overshadowing to the adjoining property at 1 Atkins Drive.</p>
<p>Infrastructure Strain</p> <p>The development will overwhelm water supply, sewage systems, and waste management services, which are not designed to support increased demand. This strain will degrade the quality of life for existing residents.</p>	<p>The proposed development has been assessed both by Tas Water regarding water and sewage and Council acting as the Stormwater Service Provider under the <i>Urban Drainage Act 2013</i>.</p> <p>Tas Water have provided a Subdivision to Planning Authority Notice with conditions indicating that the proposed development is capable of connecting to Tas Water's water and sewerage network – see Submission to Planning Authority Notice, dated 20 August 2025.</p> <p>Council have indicated that the site currently has a connection to the stormwater network located near the north corner of the site and the development may utilised this connection subject to conditions – see Council's Advice to Developer Letter dated 2 September 2025.</p>
<p>On two previous occasions planning approval for single story units was denied on the basis that there would be too many units in the one area</p>	<p>Council has no record of any previous planning applications for Multiple Dwellings for the subject site at 3 Atkins Drive.</p>
<p>Retaining wall & Land stability</p> <p>Our property lies directly below the retaining wall that supports existing dwellings at 1 Atkins Drive. This wall already shows visible cracking. Adding 18 more dwellings at 3 Atkins will increase stormwater and soil pressure</p>	<p>This is a matter separate to those under the <i>Tasmanian Planning Scheme</i>.</p> <p>Considerations regarding the existing retaining wall within the site at 1 Atkins Drive are a matter between property owners.</p>

<p>through infrastructure at 1 Atkins. This creates a serious and foreseeable safety risk. An independent geotechnical/structural assessment should be required before approval.</p>	<p>All stormwater will be directed to Council's public stormwater network.</p> <p>The site is not within a known landslip area.</p>
<p>Stormwater Management</p> <p>The development relies on existing drainage and easements at 1 Atkins.</p> <p>There is no evidence these systems can handle the additional load. The planning scheme requires stormwater disposal not to adversely affect adjoining land.</p> <p>In our view, this standard is not met.</p>	<p>See Council's Advice to Developer, dated 2 September 2025 – page 3 Stormwater Service Provider Information.</p> <p>Council has indicated that the new development may utilise the existing connection located on the north of the subject site subject to a number of conditions which require the permissible site discharge rate to be calculated for the development site and where the design site discharge is greater than the permissible site discharge the increase in site stormwater is to be accommodated by providing on-site detention to limit stormwater runoff to the satisfaction of Council.</p> <p>Onsite detention must provide for temporary storage, discharge control and a maintenance scheme.</p>
<p>Building envelope and bulk</p> <p>Units 1–11 extend up to 0.9m outside the permitted building envelope. This breach increases building bulk along the boundary. It unreasonably impacts the visual amenity of adjoining properties.</p>	<p>See discussion within the body of this report under Clause 8.4.2 – Setbacks and building envelope for all dwellings (P3).</p>
<p>Process and documentation</p> <p>The application bundles 1 and 3 Atkins together in a confusing way. It is misleading for adjoining owners. It fails to properly assess the cumulative impacts of both sites, particularly in relation to drainage, traffic, and amenity.</p>	<p>The application includes the property at 1 Atkins Drive due to a proposed new sewer connection via the services/drainage easement on 1 Atkins Drive. Whilst it is acknowledged this could present as confusing, the application clearly outlines the proposed development.</p> <p>See responses in relation to traffic, drainage and amenity throughout this report.</p>

<p>Sewer Line and easement alignment</p> <p>Concern relates to the existing sewer line that traverses my property at 1 Atkins Drive. It appears the pipes were laid incorrectly many years ago or the easements were not properly aligned. This misplacement has created an ongoing conflict between the asset and the development potential of the land.</p> <p>From my perspective, the most equitable outcome would be to have the pipe relocated back into the designated easement. Ideally, this would be in a pragmatic, cost-effective way that does not place an undue burden on either myself or the current applicant.</p>	<p>During the assessment process, the proposed development was referred to Tas Water. Tas Water requested further information from the applicant including providing an amended concept plan showing an indicative location of sewer main extensions as one plan received showed utilising existing infrastructure and another showed a sewer main extension outside of the existing drainage easement.</p> <p>Further information was provided by the applicant in response to Tas Water's request and reviewed by Tas Water who have provided permit conditions.</p> <p>The matter of the sewer main location is a matter for discussion with Tas Water as Council do not own or maintain the sewer mains.</p>
---	--

ATTACHMENTS

1. [Complete Application Documentation](#)
2. [Tas Water Conditions Submission](#)
3. [Statement of Compliance](#)
4. [Combined Representations - Redacted](#)

PLEASE QUOTE

Your Ref:

Our Ref: DA 2025/38

Enquiries: Planning Department

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320ABN: 29 846 979 690
Phone: (03) 6430 5700
Email: burnie@burnie.tas.gov.au
Web: www.burnie.tas.gov.auWe value your feedback on our service.
Tell us about it at www.burnie.tas.gov.au/feedback**NOTICE OF APPLICATION FOR LAND USE PERMIT**

(Section 57(3) Land Use Planning and Approvals Act 1993)

Advice to Adjoining Land Owner or Occupier

Application No: - DA 2025/38
Development Site: - 1 & 3 Atkins Drive ROMAINE
CT: 161069/2, 162223/0
Proposal: - Dwelling – Multiple and subdivision and consolidation

Notice of the above application is served on you as an adjoining land owner or occupier.

The application may be viewed at -

**Burnie City Council Customer Services Counter
Ground Floor, City Offices,
80 Wilson Street, Burnie**

Between the hours of 8.45 am – 4.45 pm Monday to Friday inclusive (excluding public holidays) or on Council's website at www.burnie.tas.gov.au/permits

You are entitled to make representation in writing on any aspect of the proposal addressed to: -

**General Manager,
Burnie City Council,
PO Box 973, Burnie 7320**

or burnie@burnie.tas.gov.au by no later than 5.00 pm on **22 September 2025**. Council must have regard to any written representation received during the exhibition period when considering its decision on the application.

All persons who make representation will be notified within seven (7) days of the Council's decision. Any persons who made representation and is not satisfied with the Council decision may, under Section 61(5) of the *Land Use Planning and Approvals Act 1993*, lodge an appeal against that decision within fourteen (14) days of the date of that notice to: -

**The Tasmanian Civil and Administrative Tribunal,
GPO Box 1311,
HOBART TAS 7001.**

Should you have any enquiries regarding this development proposal, please do not hesitate to contact the Planning Department on (03) 6430 5700.

Troy McCarthy
PRINCIPAL PLANNER
Date of Notice: - **6 September 2025**

BURNIE CITY COUNCIL
PO Box 973, BURNIE, TASMANIA 7320.
Ph : (03) 6430 5700
Email : burnie@burnie.tas.gov.au

**Land Use Planning and Approvals Act 1993****Tasmanian Planning Scheme****PERMIT APPLICATION***Office use only*

Application No _____

Date Received _____

Permit Pathway - *Permitted/Discretionary***Use or Development Site:****Street Address**

3 Atkins Drive, Romaine - TAS 7320
1 Atkins Drive, Romaine

**Certificate of
Title Reference**

CT 161069/2
CT 162223/0 - Common Folio

Applicant**First Name**

Ireneinc Planning & Urban Design

**Second
Name****Surname****Postal Address:**

49 Tasma Street
North Hobart TAS 7001

Phone No:

6234-9281

Mobile:**Email Address:**

phil@ireneinc.com.au

I/we consent for all giving of information and the serving of notices in relation to this application to
be delivered electronically to the above email address?

YES ☒ NO ☐**Applicants Signature:***p.gatrell***Owner** (note – if more than one owner, all names must be indicated)**First Name**

Richard Gilmour (Homes Tas - 3 Atkins Dr)

Second Name**Surname**


Atkins Drive Pty Ltd (Owner - 1 Atkins Drive)

Postal Address:

Homes Tas - GPO Box 65,
Hobart, TAS, 7001


Phone No:

Value of use and/or development

Notification of Landowner/s	
If land is not in applicant's ownership	
I, Phil Gartrell, declare that the owner/each of the owners of the land has been notified of the intention to make this permit application.	
Signature of Applicant 	Date 18/06/2025

If the permit application involves land owned or administered by the BURNIE CITY COUNCIL	
Burnie City Council consents to the making of this permit application.	
General Manager (Signature)	Date

If the permit application involves land owned or administered by the CROWN	
I, the Minister responsible for the land, consent to the making of this permit application.	
Minister (Signature)	Date

Applicant Declaration	
I, Phil Gartrell declare that the information I have given in this permit application to be true and correct to the best of my knowledge.	
Signature of Applicant 	Date 18/06/2025

Office use only

Permit Information	(NB If insufficient space, please attach separate document)
Proposed Use:	
Use Class	8 - General Residential
Documents included with the permit application to describe the Use	
- 3 Atkins Drive Planning Report	
Proposed Development	
Use class to which the development applies 8 - General Residential	
Documents included with the permit application to describe the Development	
- 3 Atkins Drive, Architectural Documentation - 3 Atkins Drive, Civil Report - 3 Atkins Drive, Traffic Impact Report	
Provisions and Standards relied upon for grant of a Permit	
Please refer to accompanying documentation.	

ireneinc

PLANNING & URBAN DESIGN



12 August 2025

Burnie City Council

PO Box 973

Burnie TAS 7320

FURTHER INFORMATION RESPONSE - 3 ATKINS DRIVE

The following has been prepared in response to the request for further information received on the 7th of July 2025, regarding the application on the abovementioned site (DA 2025/38).

This response is accompanied by an updated Planning Report and architectural set.

1. Application Form

Please sign the notification to receive correspondence electronically section on the first page of the application form (attached) and return.

Please refer to the updated planning application form.

2. Please adjust unit numbering on the Open Space Diagrams, sheet no. DA-04.

Please refer to the updated architectural plans.

3. Clause 8.4.2 – Setbacks and building envelope for all dwellings

- a) P1 - The planning report states that the application will rely on Performance Criteria in relation to dwellings 12 & 13 following the Subdivision (boundary adjustment). It is assumed that the planning report is referring to units 12 & 13 rather than 11 & 12 in the last two paragraphs (page 12 of report). Can you please clarify and if necessary, amend this section in the report.

This is correct. Please refer to the updated planning report.

- b) Additionally, can the drawing 'proposed site plan' sheet no. DA-03 provide an annotation identifying the proposed new lot boundary following subdivision and plot the offsets of units 12 and 13 to the new boundary in accordance with the dimensions as referenced in the planning report (approx. 3.3m & 3.0m).

Please refer to the updated architectural plans.

- c) P3 - The shadow diagram seems to show for 21 June at 3pm that shadow enters the adjoining site at 1 Atkins Drive. It is not known if this shadow impacts any habitable room windows on the southeastern elevation (rear portion) of the dwellings on the adjoining property at 1 Atkins Drive. For example, are there any habitable rooms windows on the southeastern elevations of the adjoining dwellings and if so will the shadow created by the proposed dwellings reach or extend onto the windows? Is there any private open space

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ABN 78 114 905 074

associated with the units on the adjoining property that may be impacted by shadowing at 3pm?

At 3pm, the shadowing shown across the rear of 1 Atkins Drive is cast from the units on that site, given the angle of the sun, which actually begin to overshadow the proposed development.

Due to the orientation of the subject site and its location to the east/south-east of 1 Atkins Drive, the shadow diagrams demonstrate there is no overshadowing of 1 Atkins Drive at any point on June 21st.

4. Clause 8.4.4 – Sunlight to private open space of multiple dwellings

The sheet titled overshadow diagrams, sheet DA-08 shows shadow diagrams at 9am, 12pm and 3pm for 21 June. Further demonstration is required on the architectural plans to corroborate the statement in the planning report. The planning report states that units 12 to 18 will receive full sunlight to their primary north-west facing private open space from approximately 10.00am until 2.30pm with private open space for each unit along the northwestern boundary receiving full sunlight from approximately 10.30am to 1.30pm. Given that the shadow diagrams don't demonstrate the shading at these times, further demonstration is required.

Sheet DA-13 provides additional shadow diagrams at 10am, 1pm and 2.30pm on June 21st, which show the extent of sunlight received along the north-western elevation of Units 12 to 18 and associated private open space.

Whilst the fences around the private open space for each unit does create some shading, each façade and ground level private open space will receive ample sunlight given the north-westerly orientation and the siting of the units on the higher topographical contour.

Please refer to the updated planning report.

5. Clause 8.4.6 – Privacy for all dwellings

- a) *The planning report states that the proposal complies with A1. It still appears that the glass balustrade component of the balcony still partially faces the side boundary within 3m for dwelling 3, 5 and potentially dwellings 7 and 9, albeit a small amount. Could you please provide further visual demonstration regarding compliance with the Acceptable Solution or alternatively adjust the planning report to reflect reliance on Performance Criteria.*

Please refer to the updated planning report and architectural drawings (sheet DA-03 and DA-14).

- b) *A2 - Please plot the 1.7m sill height on the 'typical western elevation' or the 'Typical Section A-A' on sheet title 'Typical Unit Plans, Sheet DA-07' for clarity. This will enable for easy demonstration and understanding during public notification.*
- c) *A3 - Please plot the sill height being 1.7m above floor level of the bedroom window on the southern elevation facing the shared driveway and parking spaces on drawing 'typical unit plans' sheet no. DA07 on 'typical southern elevation'. This will provide clarity and add to demonstrating compliance with the Acceptable Solution.*

Please refer to the updated architectural set which now includes the required dimensions.

6. Clause 8.4.8 – Waste Storage for multiple dwellings

The bin storage area at the front of the site is located within the 4.5m frontage setback. Please plot the setback of the storage area from the site's frontage on the

proposed site plan.

Please refer to the updated architectural set, specifically DA-11 which includes the required setback dimensions to the waste storage area.

7. C3.5.1 – Traffic generation at a vehicle crossing, level crossing or new junction – A1.2

Written consent for the proposed vehicle crossing is required to be issued from the Road Authority.

In this regard, please make an application for a Statement of compliance. This can be done through Council's website at www.burnie.tas.gov.au by typing in Statement of Compliance in the search function. This will then bring up a Statement of Compliance Request form that can be completed online.

The statement of compliance form has been submitted to Council.

If you have any further questions in relation to the above, please do not hesitate to contact me on 6234 9281.

Yours sincerely,

Phil Gartrell

Senior Planner

IRENEINC PLANNING & URBAN DESIGN

3 ATKINS DRIVE, ROMAINE



ireneinc & smithstreetstudio
PLANNING & URBAN DESIGN

PLANNING TAS PTY LTD TRADING AS IRENEINC PLANNING & SMITH STREET STUDIO PLANNING & URBAN DESIGN ABN 78 114 905 074

3 ATKINS DRIVE, ROMAINE

Planning application for eighteen (18) social and affordable housing units
Prepared on behalf of Homes Tasmania

Last Updated - August 2025

Author - Phil Gartrell

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1. INTRODUCTION

Ireneinc Planning and Urban Design has been engaged by JAWS Architects, on behalf of Homes Tasmania, to prepare a planning report to accompany an application for a social and affordable housing development at 3 Atkins Drive, Romaine. This report provides an assessment of the proposal against the provisions of the *Tasmanian Planning Scheme – Burnie*.

1.1 SITE DETAILS

1.1.1 3 ATKINS DRIVE

3 Atkins Drive is currently vacant with an area of approximately 4,058m², and is subject to a consistent downward gradient to the north-west, with the lowest point occurring along the boundary with 1 Atkins Drive. The site directly adjoins an existing multiple dwelling development to the north-west and the existing Parklands Primary School and High School to the east.

A portion of an existing footpath and roadway extend into the south-eastern corner of the site, which was previously subject to a right-of-way on the title, which has been removed. However, the footpath and road portion remain. This irregularity is to be corrected via a boundary adjustment/subdivision, as outlined in section 1.2.2 of this report.

1.1.2 1 ATKINS DRIVE

1 Atkins Drive (CT 162223/0) supports sixteen (16) existing multiple dwellings under a strata arrangement. A portion of the site to the north is identified as Unit 11-16, 1 Atkins Drive and is currently vacant, however we understand the owner is preparing to lodge a planning application to establish additional dwellings. This land is required to form part of this application, to utilise the existing drainage/service easement for stormwater reticulation and a proposed sewer connection.

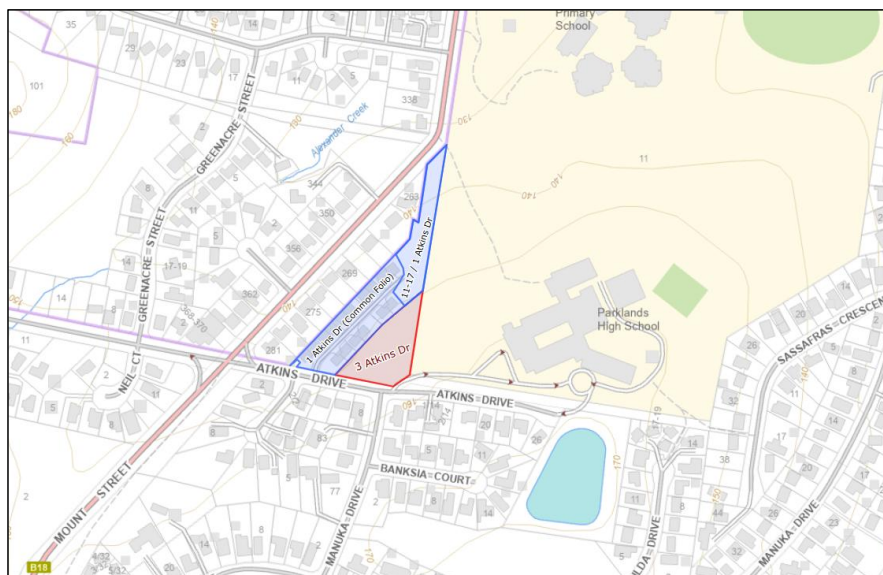


Figure 1: Topographic map (source: www.thelist.tas.gov.au © State of Tasmania)

1.1.3 EASEMENTS

The title was previously subject to a right-of-way easement, benefitting the Parklands High School, over the south-eastern corner of the site. Whilst this easement has been removed, a portion of public footpath and roadway remain within the site. For liability reasons, it is proposed that this area will be subtracted from the title and amalgamated with the adjoining road reservation. This will be achieved via a boundary adjustment, as outlined in section 1.2 of this report.

In addition, the adjoining site to the north-west at 1 Atkins Drive is subject to a drainage easement, extending from Mount Street to the north, to the northern edge of the subject site, as illustrated below.

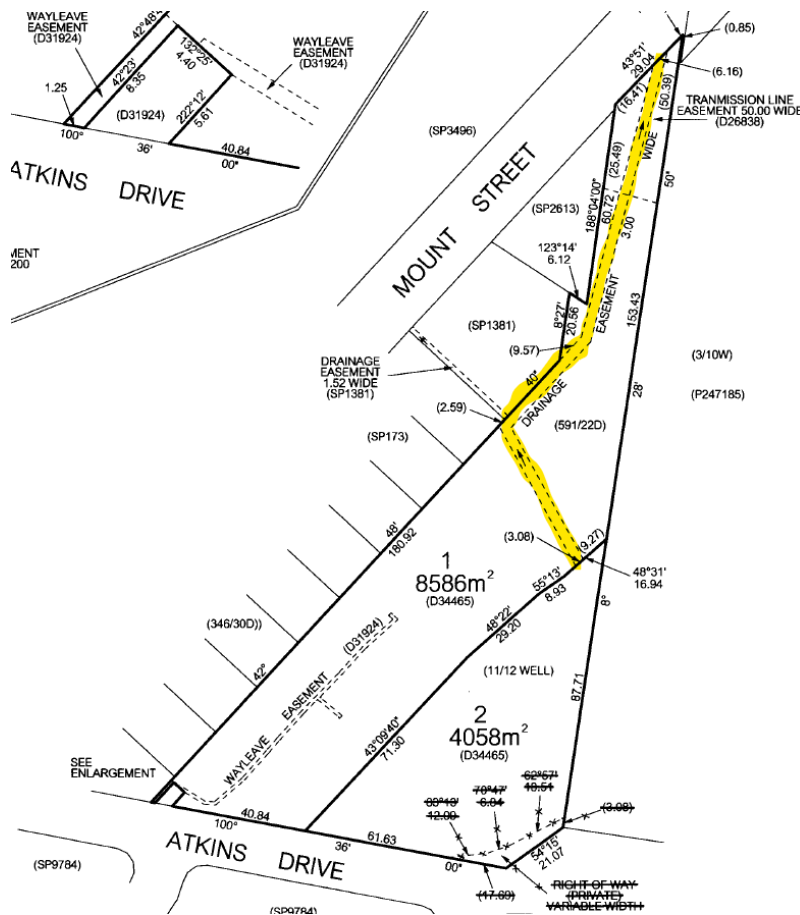


Figure 2: Extent of drainage easement across adjoining title (shown yellow)

1.2 PROPOSAL

The application seeks approval for the construction of eighteen (18), two-storey residential units for social and affordable housing, to be delivered by Homes Tasmania. Each unit will have a height varying between 7.8m to 8.1m.

The split-level design is a direct response to the site's topography, which also assists to maximise floor area per unit and provide a high quality of living.

Each unit will accommodate two-bedrooms and associated living/dining and kitchen areas. A new driveway and crossover will be provided from Atkins Drive.

A portion of the site currently extends into the road reservation along Atkins Drive, including a portion of the of the public footpath. To rectify this, the application also includes subdivision to adjust a portion of the south-eastern boundary.

The following provides a more detailed description of the proposal.

1.2.1 RESIDENTIAL UNITS

Units 1 to 11

Units 1 to 11 are sited along the north-western boundary and have been angled to the north, to take full advantage of the site's orientation. Due to the topography of the site, the units are two-storey with the living, dining and kitchen areas provided on the lower level. The upper floor provides two bedrooms and a bathroom, along with level access from the internal driveway and parking spaces.

Each unit is provided with private open space, which includes no less than 40m² directly accessible from living/dining areas, with ample room for gardens and landscaping.

Units 12 to 18

Units 12 to 18 are sited along the higher south/south-eastern boundary of the site. These units adopt the same design and general siting, with kitchen/dining and living areas provided on the lower level. The upper floors provide two bedrooms and a bathroom, along with access to a secondary area of private open space sited to the south-east of each unit.

The primary areas of private open space for Units 12 to 18 range in size from a minimum of 25m² up to 33m² and are provided along the north-west elevation, adjoining the internal driveway. Additional private open space is provided to the rear, along the south-eastern elevation, providing an additional 25m² to 29m², up to 33m² of private open space.

1.2.2 BOUNDARY ADJUSTMENT

As indicated in section 1.1, the south-eastern corner of the site extends out into the road reservation, incorporating a portion of the public footpath. This irregularity was originally addressed by a right-of-way, which has since been removed.

To address potential liability issues, a boundary adjustment is proposed to realign the front boundary to ensure the footpath falls entirely within the public road reservation. This will reduce the overall size of the lot by approximately 118m², with a revised area of 3,940m², down from 4,058m².

Whilst this adjustment would ordinarily be exempt under General Provision 7.3, the lot boundary abuts a zone boundary and does not satisfy the general exemption test. Therefore, the adjustment must be assessed as a subdivision.

1.1.4 INFRASTRUCTURE WORKS

Stormwater

Stormwater management is not explicitly regulated under the State Planning Provisions. However, a concept servicing plan and report has been prepared, demonstrating that stormwater will be captured and directed to an existing DN225 Stormpro pipe outlet in the northern corner of the site.

The pipe continues over the adjoining land at 1 Atkins Drive, via a 3-meter-wide service/drainage easement, to which 1 Atkins Drive has legal right of drainage.

The stormwater system includes an on-site detention and treatment tank, to manage post-development flows and meet the State Stormwater Quality and Quantity targets. The system has also been designed to accommodate the required 10% AEP event.

Please refer to the accompanying documents prepared by ADG.

Sewer & Water

A new property water connection is proposed from Atkins Drive.

TasWater asset information indicates the site currently has no sewer property connection. Due to the topography, the following options were explored to provide a sewer connection:

Option 1 – Provide a new connection from Main Street, utilising an existing drainage/services easement over the adjoining property identified at 11-16, 1 Atkins Drive.

Option 2 – Provide a new connection from the existing sewer main in Atkins Drive.

- Due to the change in grades, this would require an on-site pump station.

Given the above, the preference is to provide a new connection from Main Street, via the services/drainage easement on the adjoining property at 11-16, 1 Atkins Drive, which forms part of the application.

Works will also be required in Main Street, to connect the new sewer pipe into the existing manhole (Asset ID: A213099). Further information is provided in the accompanying civil documentation.



Figure 3: Indicative route of proposed sewer connection (shown red), over the existing 3m-wide drainage/service easement over Unit 11-16 / 1 Atkins Drive (source: www.thelist.tas.gov.au © State of Tasmania)

2. PLANNING ASSESSMENT

The following is an assessment of the proposal in response to the provisions of *Tasmanian Planning Scheme – Burnie*.

2.1 GENERAL RESIDENTIAL ZONE

The site falls within the General Residential Zone as seen in the figure below. The adjoining lots are similarly zoned, with the exception of the adjoining land to the east, which is zoned Community Purpose and supports the existing Parklands High School and Romaine Primary School.



Figure 4: Site zoning (source: www.thelist.tas.gov.au © State of Tasmania)

2.1.1 ZONE PURPOSE

The zone purpose statements for the General Residential Zone are as follows:

- 8.1.1 To provide for residential use or development that accommodates a range of dwelling types where full infrastructure services are available or can be provided.
- 8.1.2 To provide for the efficient utilisation of available social, transport and other service infrastructure.
- 8.1.3 To provide for non-residential use that:
 - (a) primarily serves the local community; and

(b) does not cause an unreasonable loss of amenity through scale, intensity, noise, activity outside of business hours, traffic generation and movement, or other off site impacts.

8.1.4 To provide for Visitor Accommodation that is compatible with residential character.

The site is within an existing and well-established residential area and the multiple dwellings directly align with the purpose of the zone.

2.1.2 USE CLASSIFICATION & STANDARDS

Multiple Dwellings fall within the ‘Residential’ use class, which is permitted in the Zone. There are no applicable use standards for residential use.

2.1.3 DEVELOPMENT STANDARDS

8.4.1 - Residential Density for Multiple Dwellings

Objective: That the density of multiple dwellings:

- a) Makes efficient use of the land for housing; and
- b) Optimises the use of infrastructure and community services

SCHEME REQUIREMENTS

A1

Multiple dwellings must have a site area per dwelling of not less than 325m2.

P1

Multiple dwellings must only have a site area per dwelling that is less than 325m2, if the development will not exceed the capacity of infrastructure services and:

- (a) is compatible with the density of existing development on established properties in the area; or
- (b) provides for a significant social or community benefit and is:
 - (i) wholly or partly within 400m walking distance of a public transport stop; or
 - (ii) wholly or partly within 400m walking distance of an Inner Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone, Central Business Zone or Commercial Zone.

RESPONSE

A1

The proposal is for eighteen (18) residential units to be provided for social/affordable housing in 2 x bedroom configurations.

The proposed density equates to 225m2 per unit, which exceeds that required under the acceptable solution. A response to the performance criteria has been provided.

P1

An initial review of dwelling density in the surrounding area indicates the proposed density is compatible, and capable of satisfying P1(a). However, as the proposal is for social/affordable housing provided by Homes Tasmania, P1(b) is most applicable.

The test under P1(b) was designed to provide special consideration of residential housing projects (such as social housing) that require higher densities to address homelessness and disadvantaged communities. These projects deliver significant social and community benefits and generally satisfy the test under P1(b), on the proviso that they are delivered by a registered housing provider and are located wholly or partly within 400m of a public transport stop, or areas where appropriate services and facilities can be accessed.

There are five (5) public bus stops within 220m of the site. One is located along Manuka Drive to the south, two are located further west along Three Mile Line Road and a further two stops are located to the north-west along Mount Street.

The proposal complies with P1(b).

8.4.2 - Setback

Objective: *The siting and scale of dwellings:*

- (a) provides reasonably consistent separation between dwellings and their frontage within a street;*
- (b) provides consistency in the apparent scale, bulk, massing and proportion of dwellings;*
- (c) provides separation between dwellings on adjoining properties to allow reasonable opportunity for daylight and sunlight to enter habitable rooms and private open space; and*
- (d) provides reasonable access to sunlight for existing solar energy installations.*

SCHEME REQUIREMENTS

A1 - Unless within a building area on a sealed plan, a dwelling, excluding garages, carports and protrusions that extend not more than 0.9m into the frontage setback, must have a setback from a frontage that is:

- (a) if the frontage is a primary frontage, not less than 4.5m, or, if the setback from the primary frontage is less than 4.5m, not less than the setback, from the primary frontage, of any existing dwelling on the site;*
- (b) if the frontage is not a primary frontage, not less than 3m, or, if the setback from the frontage is less than 3m, not less than the setback, from a frontage that is not a primary frontage, of any existing dwelling on the site;*
- (c) if for a vacant site and there are existing dwellings on adjoining properties on the same street, not more than the greater, or less than the lesser, setback for the equivalent frontage of the dwellings on the adjoining sites on the same street; or*
- (d) if located above a non-residential use at ground floor level, not less than the setback from the frontage of the ground floor level.*

P1 - A dwelling must have a setback from a frontage that is compatible with the streetscape, having regard to any topographical constraints.

RESPONSE

A1

The site has frontage to Atkins Drive, along the southern boundary and extends around the south-eastern corner of the site.

Given the site is currently vacant and adjoins an existing unit development to the west at 1 Atkins Drive and the existing Parklands High School to the east, A1(c) appears to be most applicable.

Two units on the adjoining property at 1 Atkins Drive (Unit 1 and 22) have frontage setbacks varying between 8m (approx.) and 4.3 – 4.5m (approx.). Within the proposed development, Units 1, 12 and 13 all incorporate setbacks capable of compliance with A1(c), based on the current title boundary to Atkins Drive. However, due to the required subdivision (boundary adjustment), the frontage setback for Units 12 and 13 will be reduced to 3.3m and 3m (approx.) respectively. This falls below the minimum setback of the adjoining units at 1 Atkins Drive.

A response to the performance criteria has been provided.

P1

The site has a steep downward slope to the west/north-west. The proposed units have been sited in response to this, to take advantage of the north-westerly orientation whilst providing appropriate gradients for vehicle access and private open space, whilst minimising cut and fill.

Whilst the topographical constraints and general siting requirements contribute to the discretionary setback for Units 12 and 13, the permitted setback would otherwise be achieved if not for the required boundary adjustment. Notwithstanding, the site effectively represents the last residential lot along the northern side of Atkins Drive, with the adjoining Parklands High School occupying the remaining frontage. Unit 1 provides a consistent and compatible frontage setback, with the discretionary variation for Units 11 and 12 limited to the south-eastern portion of the frontage.

Given these factors, the setback of Units 12 and 13 is compatible with the prevailing frontage setbacks to Atkins Drive.

A2 - A garage or carport for a dwelling must have a setback from a primary frontage of not less than:

- a) 5.5m, or alternatively 1m behind the building line;
- b) The same as the building line, if a portion of the dwelling gross floor area is located above the garage or carport; or
- c) 1m, if the existing ground level slopes up or down at a gradient steeper than 1 in 5 for a distance of 10m from the frontage.

...

RESPONSE

A2

No garages or carports are proposed.

A3 - A dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions that extend not more than 0.9m horizontally beyond the building envelope, must:

- a) be contained within a building envelope (refer to Figures 8.1, 8.2 and 8.3) determined by:
 - (i) a distance equal to the frontage setback or, for an internal lot, a distance of 4.5m from the rear boundary of a property with an adjoining frontage; and
-

- (ii) projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above existing ground level at the side and rear boundaries to a building height of not more than 8.5m above existing ground level; and
- b) ...

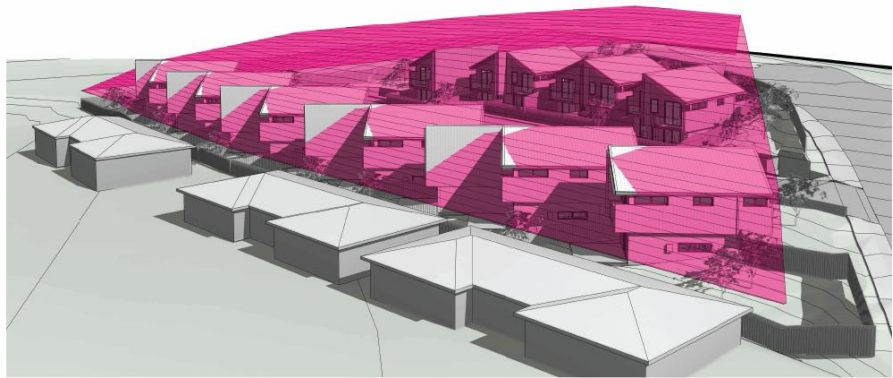
P3 - The siting and scale of a dwelling must:

- (a) not cause an unreasonable loss of amenity to adjoining properties, having regard to:
 - (i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;
 - (ii) overshadowing the private open space of a dwelling on an adjoining property;
 - (iii) overshadowing of an adjoining vacant property; and
 - (iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property;
- (b) provide separation between dwellings on adjoining properties that is consistent with that existing on established properties in the area; and
- (c) not cause an unreasonable reduction in sunlight to an existing solar energy installation on:
 - (i) an adjoining property; or
 - (ii) another dwelling on the same site.

RESPONSE

As illustrated in the accompanying architectural set, the proposed development generally sits within the building envelope.

However, sections of the roof form associated with Units 1 to 11 along the north-western boundary extend more than 0.9m beyond the envelope – as illustrated below.



1 BUILDING HEIGHT WEST PERSPECTIVE
SCALE:

A response to the performance criteria is provided below.

P3 (a) – Overshadowing & Visual Impacts

The performance criteria require the following matters to be considered, to determine the level of impact on the residential amenity of adjoining properties:

(a) not cause an unreasonable loss of amenity to adjoining properties, having regard to:

(i) reduction in sunlight to a habitable room (other than a bedroom) of a dwelling on an adjoining property;

(ii) overshadowing the private open space of a dwelling on an adjoining property;

(iii) overshadowing of an adjoining vacant property; and

(iv) visual impacts caused by the apparent scale, bulk or proportions of the dwelling when viewed from an adjoining property;

The following provides a response to each criteria.

Reduction in sunlight to a habitable room

The site has a favorable north-westerly orientation, with a variable gradient that rises to the south-east. The site constitutes the eastern most residential property along Atkins Drive, with the Parklands Highschool adjoining the site to the east.

To the west, the site adjoins an existing residential unit development at 1 Atkins Drive.

The location of the subject site to west of 1 Atkins Drive, combined with the rising gradient, confirms the proposal will not overshadow 1 Atkins Drive at any point on June 21st (when the sun is at its lowest point) – as shown in the accompanying shadow diagrams.

Overshadowing of private open space

As outlined above, the location, orientation and gradient across the site ensures the proposed units will not overshadow the private open space of adjoining units at 1 Atkins Drive at any point, on June 21st.

Visual Impacts, caused by apparent scale, bulk or proportions

Due to the split-level design and staggered siting pattern to retain consistent levels, orientation and overall design in response to the topography, parts of the south-west facing elevations of Units 3, 5, 7, 9 and 11 extend beyond the building envelope. These sections present wall heights of up to 8.1m.

To mitigate potential visual impacts, Unit 1, 2, 4, 6, 8 and 10 achieve a minimum setback of 3m between the westernmost edge of the units and the shared boundary. Unit 3, 5, 7, 9 and 11 extend slightly closer as a result of the external wall and roof, with minimum setbacks of approximately 2m. Due to the staggered and offset siting of the north-western façades, the north-eastern corner of each unit and associated south-west facing walls of each adjoining unit increase to well over 4m.

The staggered siting and offset unit orientation, along with the alternating roof forms and gradients greatly reduce the perceived height and bulk of the units. Together with highlight windows across the upper level of each unit, and use of light-colored external materials/finishes (white brickwork and shiplock timber cladding), the design provides a level of façade articulation and variation which significantly reduces bulk and proportions.

When combined, these design responses effectively break down the scale, bulk and proportions of the units to a degree that sufficiently mitigates the potential for visual impact, to an extent that is not considered unreasonable.

P3 (b) – Separation between adjoining dwellings

The proposal must also demonstrate that separation between the proposed units and adjoining dwellings is consistent with that found on established properties in the area.

Units 1 to 11 adopt minimum setbacks ranging from 2m to 3m at the shortest point from the shared boundary. This increases to well over 4m on the north-eastern edge of the units, due to the north-west façade orientation. Whilst an assessment under the performance criteria cannot have regard to a provision under the corresponding acceptable solution, it is noted that a setback of 1.5m to a side boundary is deemed acceptable under A3(b).

The existing units on the adjoining site at 1 Atkins Drive incorporate a consistent setback of approximately 2.1m. Combined, this achieves a minimum separation of approximately 4.1m.

A review of separation distances between adjacent dwellings is provided below.

No. of Dwellings	Street Name / No.	Minimum Separation Distance
	Atkins Drive	
1	2 Atkins Dr / 4 Atkins Dr	6.6m
2	4 Atkins Dr / 6 Atkins Dr	5.4m
3	6 Atkins Dr / 8 Atkins Dr	4.1m
4	10 Atkins Dr / 12 Atkins Dr	6.8m
5	12 Atkins Dr / 14 Atkins Dr (Unit 2)	4.4m
6	18 Atkins Dr / 20 Atkins Dr	6m
7	20 Atkins Dr / 22 Atkins Dr	5.6m
8	22 Atkins Dr / 24 Atkins Dr	4.9m
	Cedar Place	
9	2 Cedar Pl / 4 Cedar Pl	2.4m
	Banksia Court	
10	1 Banksia Cr / 3 Banksia Cr	6.5m
11	3 Banksia Ct / 5 Banksia Ct	3.9m
12	5 Banksia Cr / 7 Banksia Ct	4m

13	7 Banksia Cr / 9 Banksia Ct	4.2m
14	9 Banksia Cr / 11 Banksia Ct	6.7m
15	11 Banksia Cr / 13 Banksia Ct	5.4m
16	13 Banksia Cr / 15 Banksia Cr	5.5m
17	15 Banksia Cr / 16 Banksia Cr	1.7m
18	16 Banksia Cr / 14 Banksia Cr	5m
19	14 Banksia Cr / 12 Banksia Cr	4.4m
20	12 Banksia Cr / 10 Banksia Cr	8.4m
21	10 Banksia Cr / 8 Banksia Cr	4m
22	8 Banksia Cr / 6 Banksia Cr	5.8m
23	6 Banksia Cr / 4 Banksia Cr	5.3m
24	4 Banksia Cr / 2 Banksia Cr	7.8m
	Minimum Separation = 1.7m	
	Maximum Separation = 8.4m	
	Average separation = total separation (124.8) / No. of dwellings (24) = 5.2m	

The table above demonstrates a relatively high degree of variation in dwelling separation across established properties in the immediate area. This is influenced by variations in lot dimensions, frontage orientation and siting of dwellings.

Compared to the proposed development, the minimum separation of 4.1m between Units 1 to 9 and the adjoining units at 1 Atkins Drive is broadly consistent with that found between established properties in the area and complies with P3 (b).

P3 (c) – overshadowing of solar energy installations on adjoining properties

The proposed development will not overshadow any adjoining residential properties. Whilst no solar panels are currently proposed.

The proposal complies with P3.

8.4.3 - Site Coverage and Private Open Space for all dwellings

Objective: That dwellings are compatible with the amenity and character of the area and provide:

- a) for outdoor recreation and the operational needs of the residents;
- b) opportunities for the planting of gardens and landscaping; and
- c) private open space that is conveniently located and has access to sunlight.

SCHEME REQUIREMENTS

A1 - Dwellings must have:

- (a) a site coverage of not more than 50% (excluding eaves up to 0.6m wide); and
- (b) for multiple dwellings, a total area of private open space of not less than 60m² associated with each dwelling, unless the dwelling has a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer).

P1 - Dwellings must have:

- a) site coverage consistent with that existing on established properties in the area;
- b) private open space that is of a size and with dimensions that are appropriate for the size of the dwelling and is able to accommodate:
 - (i) outdoor recreational space consistent with the projected requirements of the occupants and, for multiple dwellings, take into account any common open space provided for this purpose within the development; and
 - (ii) operational needs, such as clothes drying and storage; and
- c) reasonable space for the planting of gardens and landscaping.

RESPONSE

Complies with A1

(a) The proposal includes 18 units, each with an area of approximately 62m², with a site coverage of approximately 1,116m².

Accounting for the proposed boundary adjustment, the site has an area of approximately 4,058m². Therefore, the proposal results in a site coverage of approximately 28% and complies with A1(a).

(b) Each unit will have a total area of POS exceeding 60m². This is comprised of a primary and secondary area allocated for each unit.

Units 1 to 11

For Units 1 to 11, the primary areas provide no less than 43m² of POS, sited to the north/north-west and directly accessible from living spaces. The secondary areas provide a further 12m² to 15m² of total POS and are provided along the western or eastern sides of each Unit.

Units 12 to 18

For Units 12 to 18, the primary POS provides 25m² and 35m² of space along the northern elevation, directly accessible from living areas. The secondary areas provide a further 25m², up to 44m² of space along the south, south-west and north-eastern elevations.

A2 - A dwelling must have private open space that:

- a) is in one location and is not less than:
 - (i) 24m²; or
 - (ii) 12m², if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer);
- b) has a minimum horizontal dimension of:
 - (i) 4m; or
 - (ii) 2m, if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer);
- c) is located between the dwelling and the frontage only if the frontage is orientated between 30 degrees west of true north and 30 degrees east of true north; and
- d) has a gradient not steeper than 1 in 10.

P2 - A dwelling must have private open space that includes an area capable of serving as an extension of the dwelling for outdoor relaxation, dining, entertaining and children's play and is:

- a) conveniently located in relation to a living area of the dwelling; and
- b) orientated to take advantage of sunlight.

RESPONSE**Complies with A2**

- (a)(i) Units 1 to 11 are each provided with more than 24m² of POS in one location.
This is similarly the case for Units 12 to 18, sited along the eastern boundary and to the frontage with Atkins Street, each with a dedicated area of POS exceeding 24m² in one location.
- (b)(i) Each of the areas identified above have minimum horizontal dimensions greater than 4m.
- (c) The primary areas of private open space identified above are not located between the Units and the frontage.
- (d) All areas of private open space have gradients less than 1 in 10.

8.4.4 - Sunlight to private open space of multiple dwellings

Objective: That the separation between multiple dwellings provides reasonable opportunity for sunlight to enter private open space for dwellings on the same site.

SCHEME REQUIREMENTS

A1 - A multiple dwelling, that is to the north of the private open space of another dwelling on the same site, required to satisfy A2 or P2 of clause 8.4.3, must satisfy (a) or (b), unless excluded by (c):

- a) the multiple dwelling is contained within a line projecting (see Figure 8.4):
 - (i) at a distance of 3m from the northern edge of the private open space; and
 - (ii) vertically to a height of 3m above existing ground level and then at an angle of 45 degrees from the horizontal.
-

- b) *the multiple dwelling does not cause 50% of the private open space to receive less than 3 hours of sunlight within the hours of 9.00am to 3.00pm on 21st June.*
- c) *this Acceptable Solution excludes that part of a multiple dwelling consisting of:*
- (i) *an outbuilding with a building height not more than 2.4m; or*
 - (ii) *protrusions that extend not more than 0.9m horizontally from the multiple dwelling.*

P1 - *A multiple dwelling must be designed and sited to not cause an unreasonable loss of amenity by overshadowing the private open space, of another dwelling on the same site, which is required to satisfy A2 or P2 of clause 8.4.3 of this planning scheme.*

RESPONSE

Complies with P1

Units 1 to 11

Units 1 to 11 are sited to the north of Units 12 to 18 and incorporate a setback of well over 3m, in accordance with A1(a).

However, the private open space for Units 1 to 11 have separation distances less than 3m from the northern edge of the private open space of each corresponding unit in that row. Whilst the units are not directly north of one another, there will be some shadowing that occurs on June 21st.

A response to P1 is provided.

The shadow diagrams demonstrate that the balconies for Units 1 to 11 will begin to receive full sunlight between approximately 10.30am until after 2.30pm on June 21st. The ground level decks will also receive ample sunlight from approximately 10.30am until approximately 2.30pm, when the north-western side boundary fencing begins to cast shadows at ground level.

The orientation of these units ensures they will receive ample sunlight on June 21st.

Units 12 to 18

These units are also not directly north of one another, however there will be some shadowing that occurs on June 21st.

The updated shadow diagrams demonstrate that due to the siting/design and change in topography, Units 12 to 18 will also receive full sunlight to their primary north-west facing areas of private open space, from approximately 10.30am until 2.30pm on June 21st. Whilst the front and side fences between each unit do generate some ground level shading throughout the day, this is not caused by the massing of Units sited to the north.

The additional shadow diagrams and 3D views provided in the architectural set (sheet DA-13) illustrate the above.

The proposal complies with P1.

8.4.5 - Width of openings for garages and carports for all dwellings

Objective: To reduce the potential for garage or carport openings to dominate the primary frontage.

SCHEME REQUIREMENTS

A1 - A garage or carport within 12 m of a primary frontage (whether the garage or carport is free-standing or part of the dwelling) must have a total width of openings facing the primary frontage not exceeding 6m or half the width of the frontage (whichever is the lesser).

P1 - A garage or carport for a dwelling must be designed to minimise the width of its openings that are visible from the street, so as to reduce the potential for the openings of a garage or carport to dominate the primary frontage.

RESPONSE

No garages or carports are proposed.

8.4.6 - Privacy for all dwellings

Objective: To provide a reasonable opportunity for privacy for dwellings.

SCHEME REQUIREMENTS

A1 - A balcony, deck, roof terrace, parking space, or carport for a dwelling (whether freestanding or part of the dwelling), that has a finished surface or floor level more than 1m above existing ground level must have a permanently fixed screen to a height of not less than 1.7m above the finished surface or floor level, with a uniform transparency of not more than 25%, along the sides facing a:

- a) side boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 3m from the side boundary;
- b) rear boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of not less than 4m from the rear boundary; and
- c) dwelling on the same site, unless the balcony, deck, roof terrace, parking space, or carport is not less than 6m:
 - (i) from a window or glazed door, to a habitable room of the other dwelling on the same site; or
 - (ii) from a balcony, deck, roof terrace or the private open space of the other dwelling on the same site.

P1 - A balcony, deck, roof terrace, parking space or carport for a dwelling (whether freestanding or part of the dwelling) that has a finished surface or floor level more than 1m above existing ground level, must be screened, or otherwise designed, to minimise overlooking of:

- a) a dwelling on an adjoining property or its private open space; or
- b) another dwelling on the same site or its private open space.

RESPONSE

Complies with P1

Due to the topography of the site, the balconies associated with each unit will have floor levels more than 1m above ground level.

Units 1 to 11

Sheet DA-03 on the amended architectural set demonstrates that portions of the south-western walls and roof to Units 1 to 11 extend beyond the 3m setback from the north-western boundary. However, the inset provided on DA-03 and DA-14 demonstrate that each balcony achieves the required 3m setback from the north-western side boundary and complies with P1(a).

The balconies are within 6m of the private open space of the corresponding unit within the same row (e.g. the balcony to Unit 2 is within 6m of the private open space to Unit 1, etc). To address this, the south-western side of each balcony (Units 1 to 11) is separated by a solid wall extending to the underside of the roof, or screen to a height of at least 1.7m above the finished floor level, demonstrating compliance with P1(b).

Units 12 to 18

Units 12-18 are sited over 28m from the adjoining units at 1 Atkins Drive, therefore P1(a) is not applicable. With regard to P1(b), the balconies are within 6m of private open space of units within the same row.

The north-western elevation of Unit 17 is significantly offset from Unit 18, such that there is little or no line of sight between each unit's respective areas of private open space. This is also the case with Unit 12 and 13.

To a lesser degree, Unit 16 and Unit 17 are also offset and incorporate a solid wall (forming part of the south-western elevation) which aligns with the outer edge of each balcony, which assists in minimising potential for overlooking. The balcony to Unit 16 also incorporates a 1.7m screen along its south-western side to minimise overlooking of private open space associated with Unit 15.

Whilst Units 14 and 15 do not incorporate any offset, the shared wall between the two units extends out from the first-floor level with the edge of each balcony, which assists to minimise potential for overlooking. The north-western side of the balcony to Unit 15 also incorporates a 1.7m screen, to minimise overlooking of private open space associated with Unit 16.

These arrangements are considered appropriate to minimise potential overlooking to an extent that is not unreasonable.

The proposal complies with P1.

A2 - A window or glazed door, to a habitable room of a dwelling, that has a floor level more than 1 m above the natural ground level, must satisfy (a), unless it satisfies (b):

a) The window or glazed door:

- (i) is to have a setback of not less than 3m from a side boundary;
- (ii) is to have a setback of not less than 4m from a rear boundary;
- (iii) if the dwelling is a multiple dwelling, is to be not less than 6m from a window or glazed door, to a habitable room, of another dwelling on the same site; and
- (iv) if the dwelling is a multiple dwelling, is to be not less than 6m from the private open space of another dwelling on the same site.

b) The window or glazed door:

-
- (i) is to be offset, in the horizontal plane, not less than 1.5m from the edge of a window or glazed door, to a habitable room of another dwelling;
 - (ii) is to have a sill height of not less than 1.7m above the floor level or have fixed obscure glazing extending to a height of at least 1.7m above the floor level; or
 - (iii) is to have a permanently fixed external screen for the full length of the window or glazed door, to a height of not less than 1.7m above floor level, with a uniform transparency of not more than 25%.

P2 - A window or glazed door to a habitable room of a dwelling that has a floor level more than 1m above existing ground level, must be screened, or otherwise located or designed, to minimise direct views to:

- a) a window or glazed door, to a habitable room of another dwelling; and
- b) the private open space of another dwelling.

RESPONSE

Complies with A2

All 18 Units include two bedrooms on the first floor, one of which is consistently located on the north-western side. These bedrooms are more than 1m above ground level and include highlight windows along the south-western and north-eastern elevations (depending on Unit type), along with windows/doors to aforementioned balconies.

For Units 1 to 11 along the north-western boundary, the windows/doors to each balcony are setback no less than 3m from boundary, in accordance with A2(a).

All windows on the first floor along the south-west and north-east elevations are within 6m of the adjoining unit on the same site. However, all incorporate sill heights of at least 1.7m in accordance with A2(b)(ii).

A3 - A shared driveway or parking space (excluding a parking space allocated to that dwelling) must be separated from a window, or glazed door, to a habitable room of a multiple dwelling by a horizontal distance of not less than:

- a) 2.5m; or
- b) 1m if:
 - (i) it is separated by a screen of not less than 1.7m in height; or
 - (ii) the window, or glazed door, to a habitable room has a sill height of not less than 1.7m above the shared driveway or parking space, or has fixed obscure glazing extending to a height of not less than 1.7m above the floor level.

P3 - A shared driveway or parking space (excluding a parking space allocated to that dwelling), must be screened, or otherwise located or designed, to minimise unreasonable impact of vehicle noise or vehicle light intrusion to a habitable room of a multiple dwelling.

RESPONSE

Complies with A3

(a) All habitable room windows at ground floor level for Units 12 to 18 are separated more than 2.5m from the shared driveway and parking spaces. The first floor of these units and associated windows have sill heights well over 1.7m above the shared driveway/parking areas – in compliance with A3.

The parking spaces for Units 3 to 11 along the north-western boundary are exempt, as each space is directly allocated to the unit in which they front and are separated by at least 2.5m from windows to habitable rooms of the adjoining unit to the south. Each parking bay is also separated by a 1.5m high solid block wall, ensuring no impact to ground floor windows below the parking areas.

The parking space for Unit 1 is located approximately 2.5m away from the south-western elevation of Unit 2, which includes a window to a habitable room. Whilst the setback under A3(a) can be met, the window also has a sill height of at least 1.7m above the parking space.

8.4.7 - Frontage fences for all dwellings

Objective: That the height and transparency of frontage fences:

- a) provides adequate privacy and security for residents;
- b) allows the potential for mutual passive surveillance between the road and the dwelling; and
- c) are reasonably consistent with that on adjoining properties.

SCHEME REQUIREMENTS

A1 – No Acceptable Solution.

P1 – A fence (including a free-standing wall) within 4.5m of a frontage for a dwelling must:

- a) provide for security and privacy, while allowing for passive surveillance of the road; and
- b) be compatible with the height and transparency of fences in the street, having regard to:
 - (i) the topography of the site; and
 - (ii) traffic volumes on the adjoining road.

RESPONSE

Complies with P1

Frontage fencing is exempt provided it complies with the following:

4.6.3 - Fences (including free-standing walls) within 4.5m of a frontage, if located in:

(a) the General Residential Zone, Inner Residential Zone, Low Density Residential Zone, Village Zone, Urban Mixed Use Zone, Local Business Zone, General Business Zone, Central Business Zone, Commercial Zone or any particular purpose zone, and if not more than a height of:

(i) 1.2m above existing ground level if the fence is solid; or

(ii) 1.8m above existing ground level, if the fence has openings above the height of 1.2m which provide a uniform transparency of at least 30% (excluding any posts or uprights).

No fencing is proposed within 4.5m of the frontage between Unit 1 and Unit 12, except for a low free-standing wall forming part of the waste bin and letter box enclosure. The wall varies between 950mm to 1.1m high within 4.5m of the frontage, satisfying the exemption above.

However, due to the route of the adjoining road reservation, the frontage technically continues beyond Unit 12, terminating at the rear of Unit 15. A 1.7m high timber fence is proposed along this section. Therefore, a response to the performance criteria is required.

P1

The 1.7m high timber fence will provide privacy and security to Units 12 to 18, which sit approximately 2.4-2.7m below the existing footpath level. Due to this, several terraces are proposed to the rear of each unit. The fence will act as a side boundary fence, preventing people from climbing over the fence from the adjoining school grounds.

Whilst this portion of the fence could be provided with openings for passive surveillance, its purpose is essentially a boundary fence which fronts a portion of road and footpath that essentially serves as access to the adjoining Parklands High School. The potential streetscape impact of fence along this section would appear limited given the above. The fencing would have no impact on sight-lines, as it is well clear of the proposed site access.

The side boundary fence along the north-western elevation, extending within 4.5m of the frontage, can be reduced in height to 1.2m if required.

8.4.8 - Waste Storage for Multiple dwellings

Objective: To provide for the storage of waste and recycling bins for multiple dwellings.

SCHEME REQUIREMENTS

A1 - A multiple dwelling must have a storage area, for waste and recycling bins, that is an area of not less than 1.5m² per dwelling and is within one of the following locations:

- a) in an area for the exclusive use of each dwelling, excluding the area in front of the dwelling; or
- b) in a common storage area with an impervious surface that:
 - (i) has a setback of not less than 4.5m from a frontage;
 - (ii) is not less than 5.5m from any dwelling; and
 - (iii) is screened from the frontage and any dwelling by a wall to a height of not less than 1.2m above the finished surface level of the storage area.

P1 - A multiple dwelling must have storage for waste and recycling bins that is:

- a) capable of storing the number of bins required for the site;
- b) screened from the frontage and any dwellings; and
- c) if the storage area is a common storage area, separated from any dwellings to minimise impacts caused by odours and noise.

RESPONSE

Complies with P1

Two common waste storage areas are provided on-site. One is located at the end of the driveway, well setback from the frontage and over 6m from the nearest unit. Appropriate screening of not less than 1.2m in height will be provided, in accordance with A1. However, the second common waste storage area is located within 4.5m of the frontage to Atkins Drive.

Therefore, a response to the performance criteria is required.

P1

The communal waste storage areas accommodate bins sufficient in size and number to support the proposed development. Each area is provided with screening and appropriate separation from units on the same site, and the frontage to Atkins Drive.

2.1.4 STANDARDS FOR SUBDIVISION

8.6.1 – Lot design

Objective: That each lot:

- (a) has an area and dimensions appropriate for use and development in the zone;
- (b) is provided with appropriate access to a road;
- (c) contains areas which are suitable for development appropriate to the zone purpose, located to avoid natural hazards; and
- (d) is orientated to provide solar access for future dwellings.

SCHEME REQUIREMENTS

A1 - Each lot, or a lot proposed in a plan of subdivision, must:

- (a) have an area of not less than 450m² and:
 - (i) be able to contain a minimum area of 10m x 15m with a gradient not steeper than 1 in 5, clear of:
 - a. all setbacks required by clause 8.4.2 A1, A2 and A3, and 8.5.1 A1 and A2; and
 - b. easements or other title restrictions that limit or restrict development; and
 - (ii) existing buildings are consistent with the setback required by clause 8.4.2 A1, A2 and A3, and 8.5.1 A1 and A2;
- (b) be required for public use by the Crown, a council or a State authority;
- (c) be required for the provision of Utilities; or
- (d) be for the consolidation of a lot with another lot provided each lot is within the same zone.

P1 - Each lot, or a lot proposed in a plan of subdivision, must have sufficient useable area and dimensions suitable for its intended use, having regard to:

- (a) the relevant requirements for development of buildings on the lots;
- (b) the intended location of buildings on the lots;
- (c) the topography of the site;
- (d) the presence of any natural hazards;
- (e) adequate provision of private open space; and
- (f) the pattern of development existing on established properties in the area.

RESPONSE

Complies with A1

The subdivision is for a boundary adjustment. Notwithstanding, the lot has an area exceeding the minimum required under A1(a) and can accommodate a minimum building area with a gradient not steeper than 1 in 5.

There are no substantial easements or other title restrictions and no existing buildings.

A2 - Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must have a frontage not less than 12m.

...

RESPONSE

Complies with A2

The subdivision is for a boundary adjustment. The lot will retain a frontage greater than 12m.

A3 - Each lot, or a lot proposed in a plan of subdivision, must be provided with a vehicular access from the boundary of the lot to a road in accordance with the requirements of the road authority.

...

RESPONSE

Complies with A3

The subdivision is for a boundary adjustment. The lot will retain frontage and vehicle access from Atkins Drive.

A4 - Any lot in a subdivision with a new road, must have the long axis of the lot between 30 degrees west of true north and 30 degrees east of true north.

...

RESPONSE

Not applicable. The subdivision is for a boundary adjustment and does not include any new road.

8.6.2 – Roads

Objective: That the arrangement of new roads within a subdivision provides for:

- (a) safe, convenient and efficient connections to assist accessibility and mobility of the community;
- (b) the adequate accommodation of vehicular, pedestrian, cycling and public transport traffic; and
- (c) the efficient ultimate subdivision of the entirety of the land and of surrounding land

SCHEME REQUIREMENTS

A1 - The subdivision includes no new roads.

...

RESPONSE

Complies with A1

The subdivision is for a boundary adjustment and does not include any new road.

8.6.3 – Services

Objective: *That the subdivision of land provides services for the future use and development of the land.*

SCHEME REQUIREMENTS

A1 - *Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must have a connection to a full water supply service.*

A2 - *Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must have a connection to a reticulated sewerage system.*

A3 - *Each lot, or a lot proposed in a plan of subdivision, excluding for public open space, a riparian or littoral reserve or Utilities, must be capable of connecting to a public stormwater system.*

RESPONSE

Complies with A1, A2 and A3

The subdivision is for a boundary adjustment. Notwithstanding, the lot will be connected to public reticulated water and sewer infrastructure.

Stormwater will be drained via gravity to the public stormwater system.

3. CODES

3.1 PARKING AND SUSTAINABLE TRANSPORT CODE

3.1.1 USE STANDARDS

C2.5.1 Car Parking Spaces

Objective: That an appropriate level of car parking spaces are provided to meet the needs of the use.

SCHEME REQUIREMENTS

A1 - The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:

- a) The site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;
- b) The site is contained within a parking precinct plan and subject to clause C2.7;
- c) The site is subject to clause C2.5.5; or
- d) It relates to an intensification of an existing use or development or a change of use where:
 - (i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or
 - (ii) The number of on-site car parking spaces for the existing use or development specified in table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case onsite car parking must be calculated as follows:

$$N = A + (C - B)$$

N = number of onsite car parking spaces required

A = number of existing on site car parking spaces

B = number of onsite car parking spaces required for the existing use or development specified in table c2.1

C = number of onsite car parking spaces required for the proposed use or development specified in table C2.1

P1.2 - The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:

- a) the nature and intensity of the use and car parking required;
- b) the size of the dwelling and the number of bedrooms;
- c) the pattern of parking in the surrounding area.

RESPONSE

A1

All eighteen units have been designed to accommodate 2 x bedrooms.

For residential use, Table C2.1 requires the following:

- Two spaces per 2+ bedroom dwelling and 1 x visitor space, per four dwellings.

To comply with A1, a total of 36 residential spaces and 5 visitor spaces is required.

A total of 18 x spaces is proposed, one space per apartment. Therefore, a response to the performance criteria is required.

P1.2

The proposal is for social/affordable housing, typically catering for lower-income, disadvantaged people and families who generally have reduced car ownership. Therefore, the anticipated demand for on-site parking is expected to be lower than may otherwise be the case. The provision of one-space per unit is considered sufficient given the nature of the use to meet the reasonable needs of future residents.

The site is also within proximity to several existing public transport stops, with up to five located within 220m of the site. This includes one located along Manuka Drive to the south, two located further west along Three Mile Line Road and a further two stops to the north-west along Mount Street.

With respect to visitor parking, significant on-street public parking is available along Atkins Drive, Manuka Drive and Banksia Court, which is considered appropriate to cater for visitor demand.

A more detailed response demonstrating compliance with P1 is provided in the accompanying traffic impact assessment.

C2.5.2 - Bicycle Parking Spaces

Objective: That an appropriate level of bicycle parking spaces are provided to meet the needs of the use.

SCHEME REQUIREMENTS

A1 - Bicycle parking spaces must:

- Be provided on the site or within 50m of the site; and
- Be no less than the number specified in table C2.1

...

RESPONSE

The scheme does not require bicycle parking spaces for residential uses.

C2.5.3 - Motorcycle parking numbers

As stated in the application of the code, Clause C2.5.3 does not apply to residential use.

C2.5.4 – Loading Bays

As stated in the application of the code, Clause C2.5.4 does not apply to residential use.

C2.5.5 – Number of car parking spaces within the General Residential Zone

As stated in the application of the code, Clause C2.5.3 does not apply to residential use.

3.1.1.2 DEVELOPMENT STANDARDS FOR BUILDINGS AND WORKS

C2.6.1 - Construction of parking areas*Objective: The parking areas are constructed to an appropriate standard.***SCHEME REQUIREMENTS****A1** - All parking, access ways, manoeuvring and circulation spaces must:

- a) Be constructed with a durable all weather pavement;
- b) Be drained to public stormwater system, or contain stormwater on the site; and
- c) Excluding all uses in the Rural Zone, Agricultural Zone, Landscape Conservation Zone, Environmental Management Zone, Recreation Zone and Open Space Zone, be surfaced by a spray seal, asphalt, concrete, pavers or equivalent material to restrict abrasion from traffic and minimise entry of water to the pavement.

RESPONSE**Complies with A1**

The proposed driveway/parking areas will be sealed and drained to the public stormwater system, as illustrated the accompanying civil documentation.

C2.6.2 - Design and Layout of Parking Areas*Objective: That parking areas are designed and laid out to provide convenient, safe and efficient parking.***SCHEME REQUIREMENTS****A1.1** - Parking, accessways, manoeuvring and circulation spaces must either:

- a) comply with the following:
 - (i) have a gradient in accordance with the Australian Standard AS 2890 – Parking facilities, Parts 1-6;
 - (ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces;
 - (iii) have an access width not less than the requirements in Table C2.2;
 - (iv) have car parking space dimensions which satisfy the requirements in Table C2.3;
 - (v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirement in Table C2.3 where there are 3 or more car parking spaces;
 - (vi) have a vertical clearance of not less than 2.1m above the parking surface level; and
 - (vii) excluding a single dwelling, be delineate by line marking or other clear physical means; or
- b) comply with Australian Standard AS 2890 – Parking Facilities, Parts 1-6.

A1.2 - Parking spaces provided for use by persons with a disability must satisfy the following:

- a) be located as close as practicable to the main entry point to the building;
- b) be incorporated into the overall car park design; and
- c) be designed and constructed in accordance with Australian/New Zealand Standards AS/NZS 2890.6:2009 Parking facilities, off-street parking for people with disabilities.

...

RESPONSE**Complies with A1.1**

As outlined in the TIA, the internal layout and parking areas have been designed to comply with the Australian Standard, ensuring vehicles can enter, manoeuvre, and exit in a forward-driving direction. All parking spaces will be located on a gradient of less than five percent, delineated with pavement markings and supported with wheel stops. The internal widths of the driveways and parking aisles will be a minimum of 5.5 metres wide, accommodating two-way traffic flow and complying with table C2.2 of the planning scheme.

Overall, the parking spaces and internal layout complies with the acceptable solution A1.1 (b).

C2.6.3 - Number of Accesses for Vehicles**Objective:** *That:*

- a) *access to land is provided which is safe and efficient for users of the land and all road network users, including but not limited to drivers, passengers, pedestrians and cyclists by minimising the number of vehicle accesses;*
- b) *accesses do not cause an unreasonable loss of amenity of adjoining uses; and*
- c) *the number of accesses minimise impacts on the streetscape.*

SCHEME REQUIREMENTS**A1 - The number of access provided for each frontage must:**

- a) *be no more than 1; or*
- b) *no more than the existing number of accesses,*

whichever is the greater.

RESPONSE**Complies with A1**

A single crossover to Atkins Drive is proposed, in accordance with A1.

C2.6.5 Pedestrian Access

Objective: *That pedestrian access within parking areas is provided in a safe and convenient manner.*

SCHEME REQUIREMENTS**A1.1 - Uses that require 10 or more car parking spaces must:**

- a) *have a 1m wide footpath separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:*
 - (i) *a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or*
 - (ii) *protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and*
- b) *be signed and line marked at points where pedestrians cross access ways or parking aisles.*

P1 - Safe and convenient pedestrian access must be provided within parking areas, having regard to:

- a) the characteristics of the site;
- b) the nature of the use;
- c) the number of parking spaces;
- d) the frequency of vehicle movements;
- e) the needs of persons with a disability;
- f) the location and number of footpath crossings;
- g) vehicular and pedestrian traffic safety
- h) the location of any accessways or parking aisles; and
- i) any protective devices proposed for pedestrian safety.

RESPONSE

Complies with A1

The proposal requires more than 10 car parking spaces.

A 1m wide footpath is provided along the south-eastern side of the internal driveway. The pedestrian footpath appropriately connects the units and parking spaces with the existing footpath located along Atkins Drive. The pathway has a minimum width of 1m, constructed with a concrete surface. The driveway/parking area is a low-speed environment, unlikely to exceed 5km/hr.

Whilst several sections of the footpath directly adjoin the driveway, barrier kerbing is proposed to provide an appropriate level of pedestrian safety.

Clause 2.6.6 – Loading bays

Not applicable.

Clause 2.6.7 - Bicycle parking and storage facilities within the General Business Zone and Central Business Zone

Not applicable.

Clause 2.6.8 - Siting of parking and turning areas

Not applicable.

3.2 ROAD AND RAILWAY ASSETS CODE

3.2.1 USE STANDARDS

C3.5.1 - Traffic generation at a vehicle crossing, level crossing or new junction.

Objective: To minimise any adverse effects on the safety and efficiency of the road or rail network from vehicular traffic generated from the site at an existing or new vehicle crossing or level crossing or new junction.

SCHEME REQUIREMENTS

A1.2

For a road, excluding a category 1 road or a limited access road, written consent for a new junction, vehicle crossing, or level crossing to serve the use and development has been issued by the road authority.

RESPONSE

Complies with A1.2

There is currently no existing vehicle access to the site. Consent from Council is required as part of this application to establish the new access.

3.2.2 DEVELOPMENT STANDARDS FOR SUBDIVISION

C3.7.1 - Subdivision for sensitive uses within a road or railway attenuation area

Objective: To minimise the effects of noise, vibration, light and air emissions on lots for sensitive uses within a road or railway attenuation area, from existing and future major roads and the rail network.

SCHEME REQUIREMENTS

A1 - A lot, or a lot proposed in a plan of subdivision, intended for a sensitive use must have a building area for the sensitive use that is not within a road or railway attenuation area.

RESPONSE

Complies with A1

The proposed boundary adjustment triggers consideration under A1, however the site is not within any road or railway attenuation areas.

4. SUMMARY

The application seeks approval for eighteen (18) x 2-bedroom, two-storey social/affordable housing units at 3 Atkins Drive, Romaine – to be delivered by Homes Tasmania.

The application also includes the adjoining strata title at 11-17 / 1 Atkins Drive (CT 162223/17), which forms part of the common folio 1 Atkins Drive (CT 162223/0). The reason for this is to provide stormwater drainage and a new property sewer connection, via an existing 3m-wide service/drainage easement extending over the property.

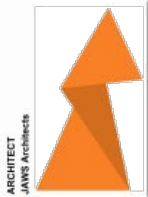
The proposed units have been sited and designed to take advantage of the sites' north-westerly orientation, whilst considering the steep gradient and desire to minimise cut and fill. Each unit is provided with generous private open space and dedicated on-site parking.

Whilst the dwelling density exceeds the permitted solution, the proposal is for social/affordable housing provided by a registered housing provider – Homes Tasmania. As such, the application will provide a significant social / community benefit and is well within 400m of various public transport routes.

The design utilises lightweight materials, colours and finishes of a high-quality, presenting a contemporary addition to the streetscape. The use of lighter colours improves ambient/natural lighting and reduces the perceived scale and bulk of the units.

Whilst portions of the north-western Units (1 to 11) extend marginally beyond the building envelope, the design/siting and offset orientation of the units appropriately mitigates the potential for amenity impacts (overlooking & visual impacts) on the adjoining property at 1 Atkins Drive.

The proposal has been assessed against the applicable provisions of the *Tasmanian Planning Scheme - Burnie* and is considered to comply.



Development Application

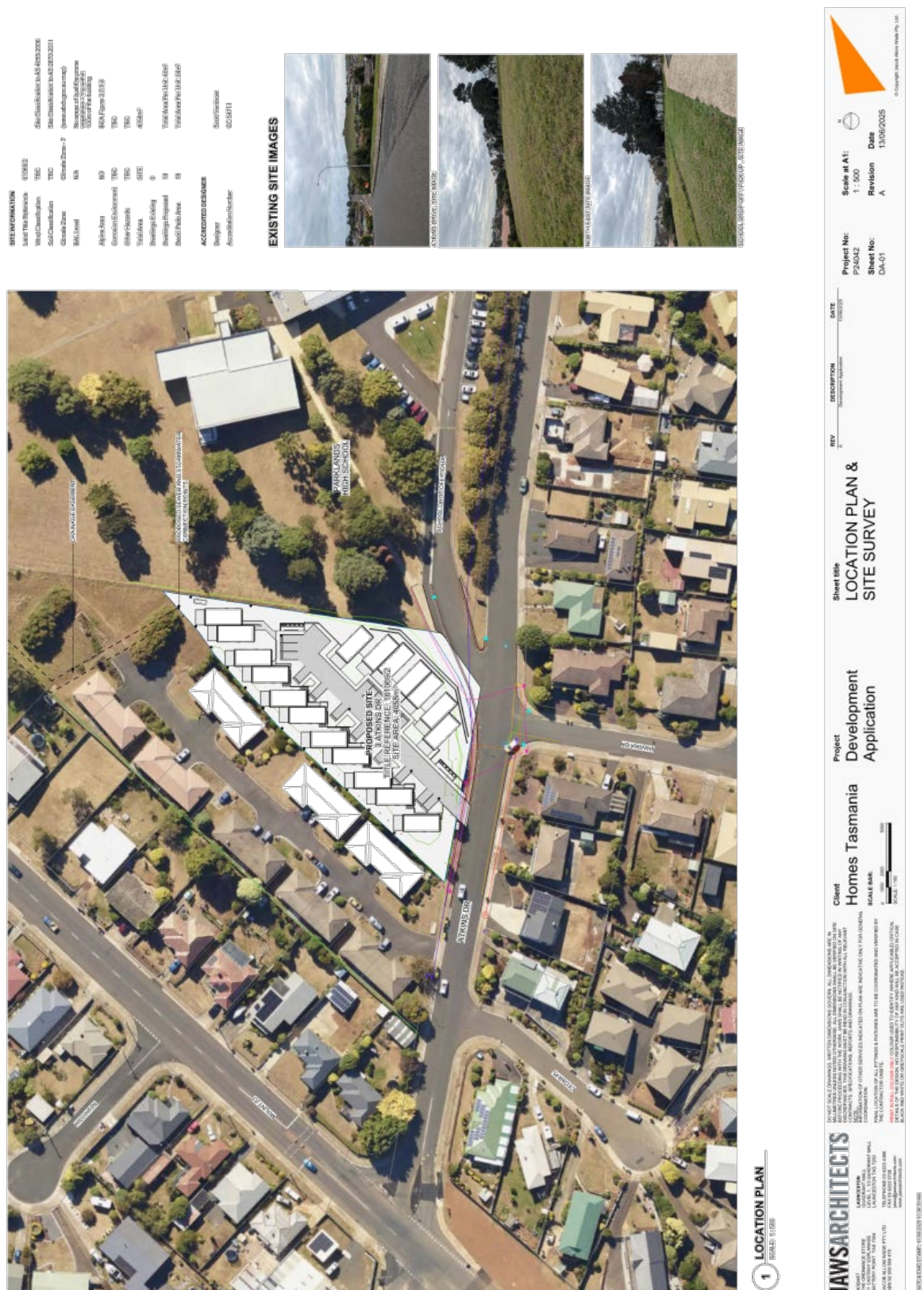
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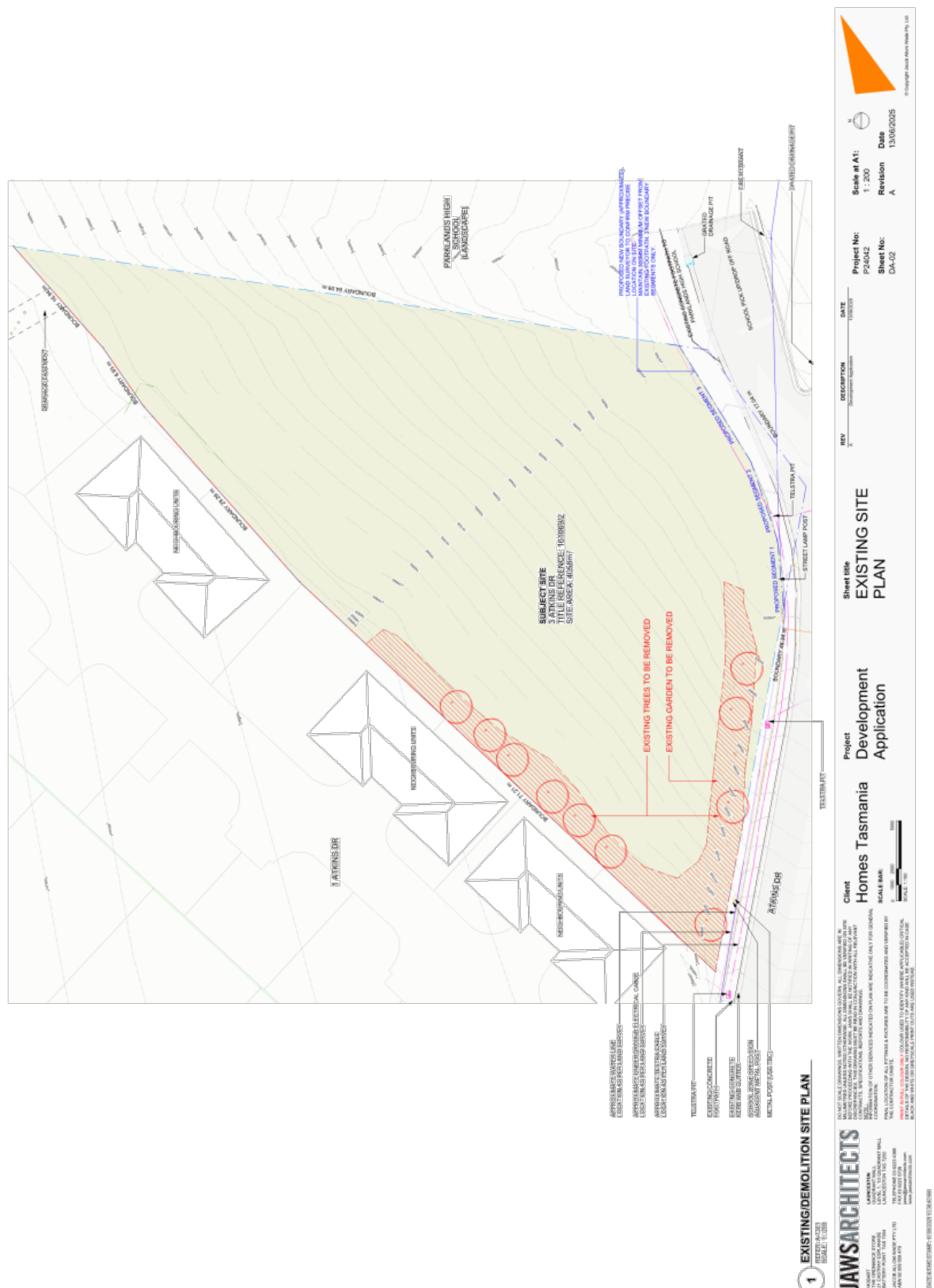
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COVER SHEET

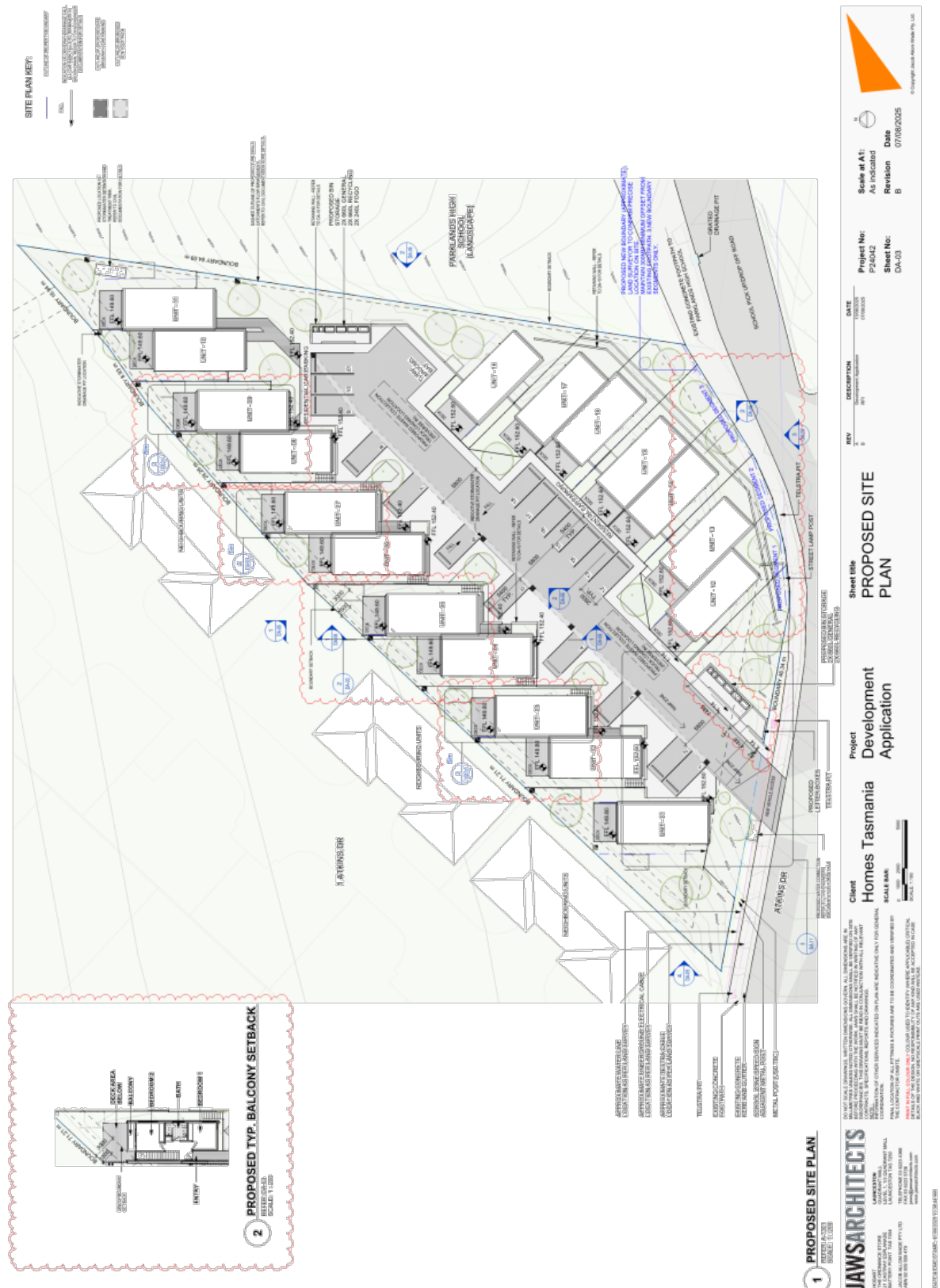
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SHEET No.	SHEET NAME	REVISION	REVISION DATE
A0 - GENERAL SHEETS			
DA-00	COVER SHEET	B	07/08/2025
07/08/2025 RFI			
A1 - SITE			
DA-01	LOCATION PLAN & SITE SURVEY	A	13/06/2025
DA-02	EXISTING SITE PLAN	A	13/06/2025
DA-03	PROPOSED SITE PLAN	B	07/08/2025
DA-04	OPEN SPACE DIAGRAMS	A	13/06/2025
DA-05	SITE ELEVATIONS	B	07/08/2025
DA-06	SITE SECTIONS - PROPOSED	A	13/06/2025
DA-07	TYPICAL UNIT PLANS	B	07/08/2025
DA-08	OVERSHADOW DIAGRAMS	A	13/06/2025
DA-09	PROPOSED LANDSCAPE PLAN	A	13/06/2025
DA-10	PROPOSED DRIVEWAY PLAN	A	13/06/2025
DA-11	OPEN SPACE DIAGRAMS - INDIVIDUAL	B	07/08/2025
DA-12	LOT AREA		
DA-13	OVERSHADOW DIAGRAMS - UNIT 12-18	B	07/08/2025
DA-14	PROPOSED SITE PLAN - UPPER FLOOR	B	07/08/2025
TOTAL SHEETS: 14			

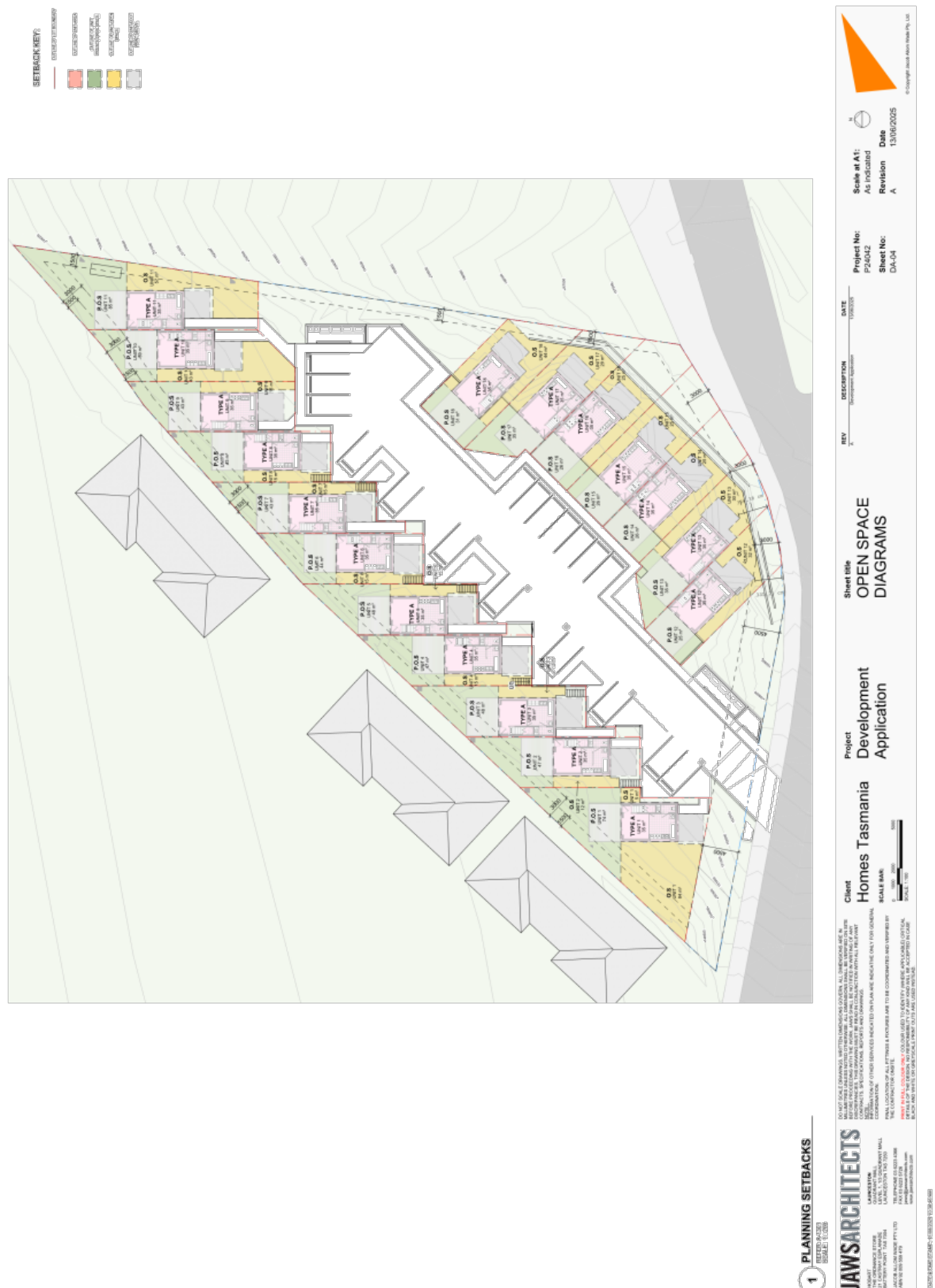


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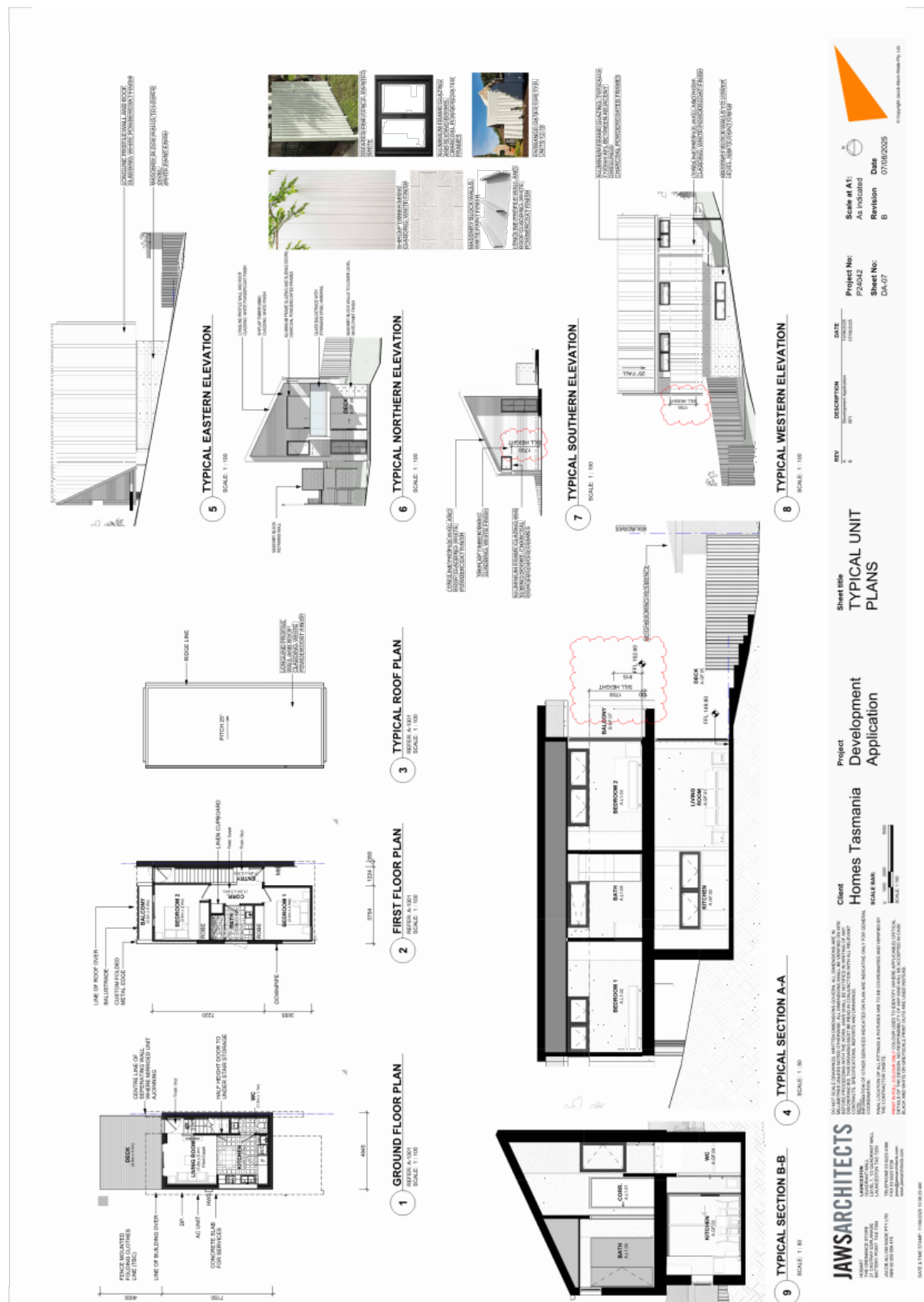




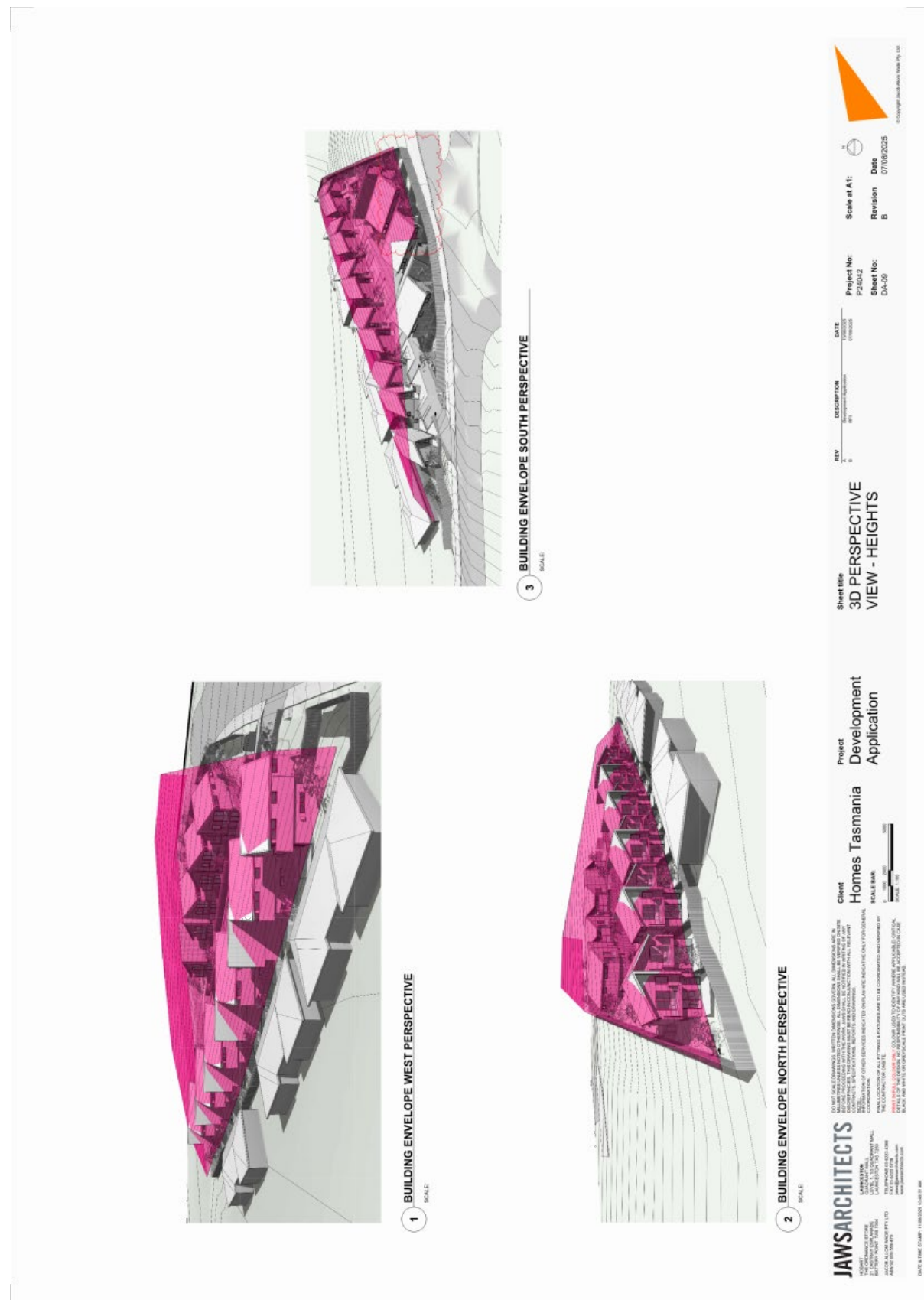


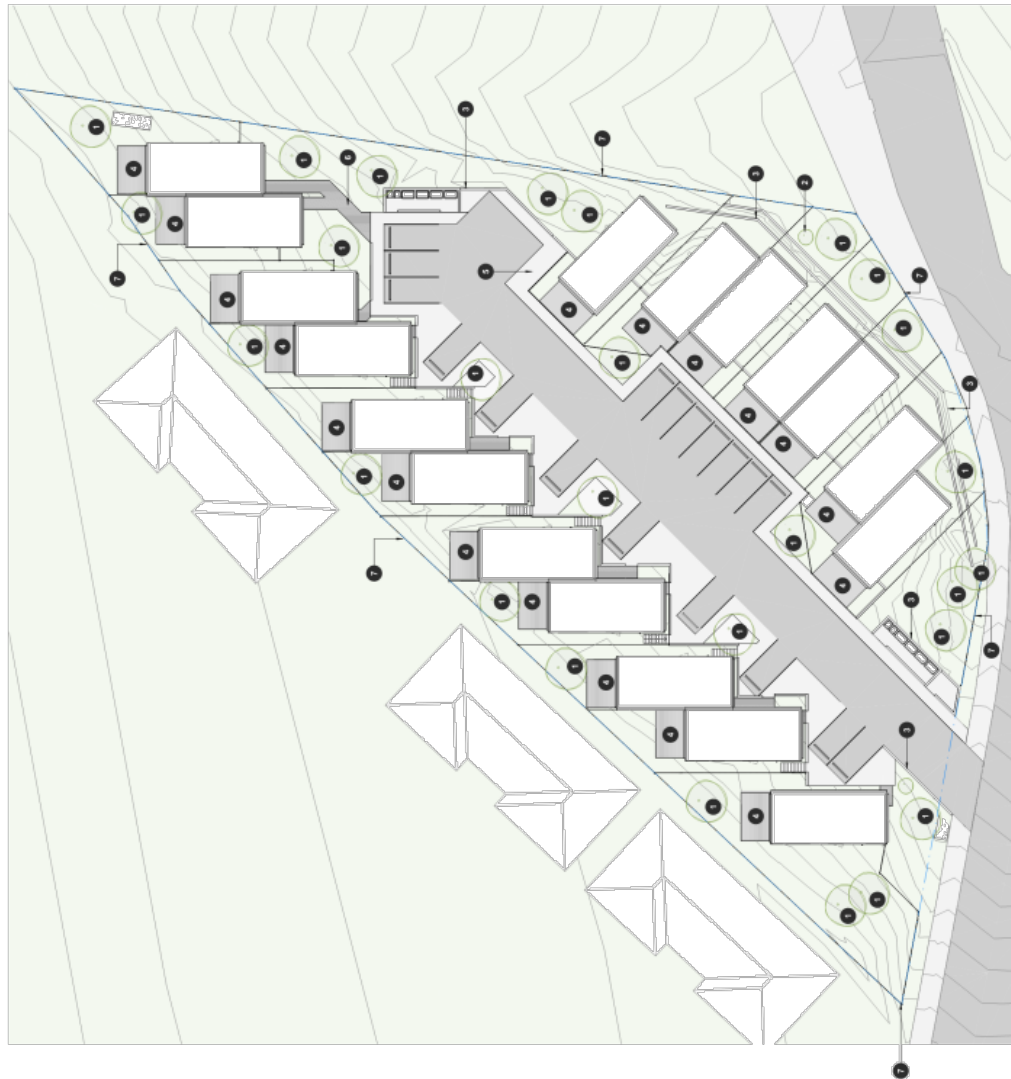
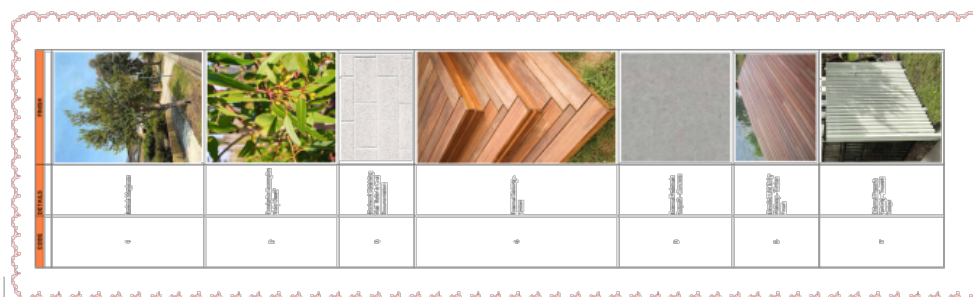
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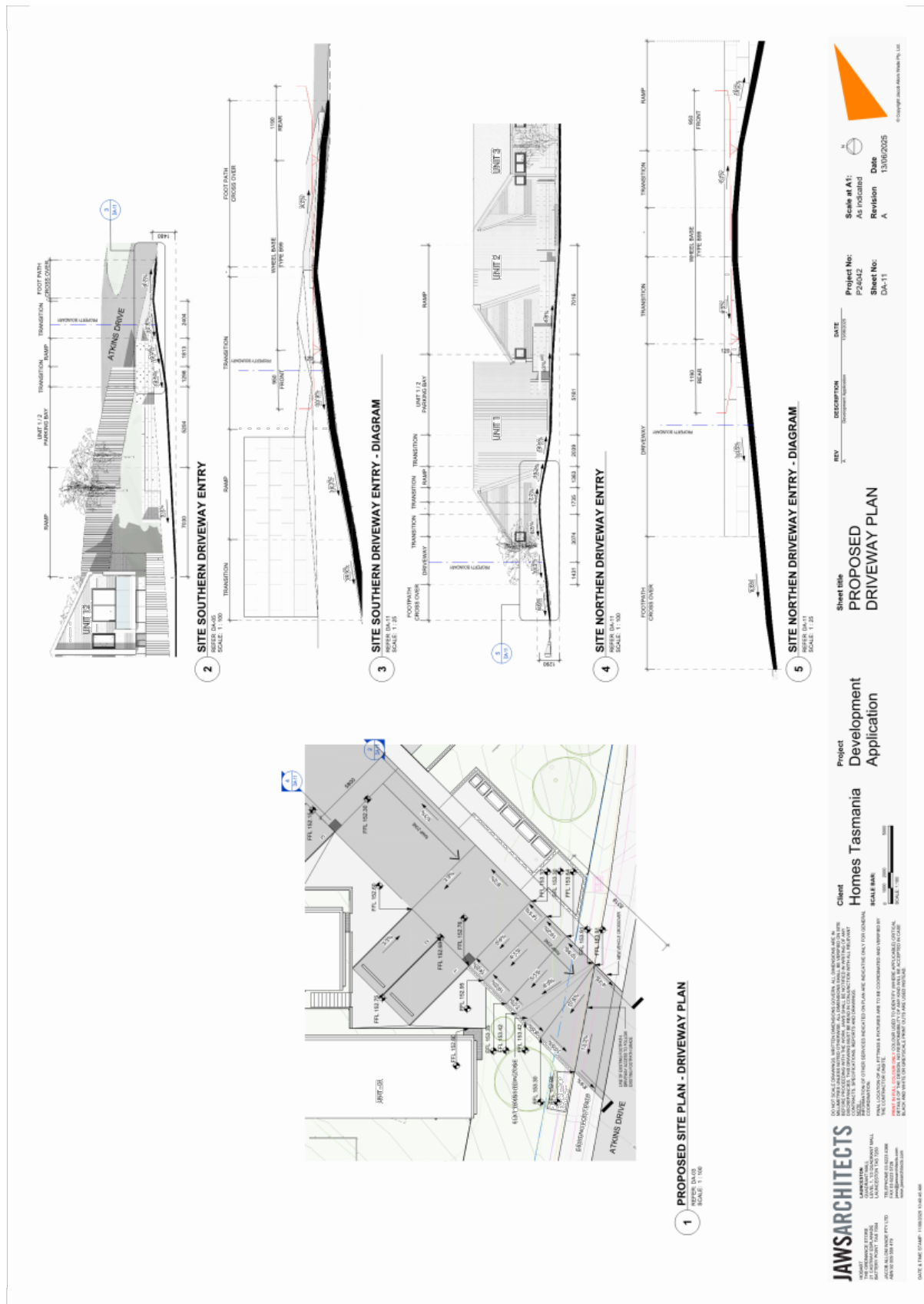


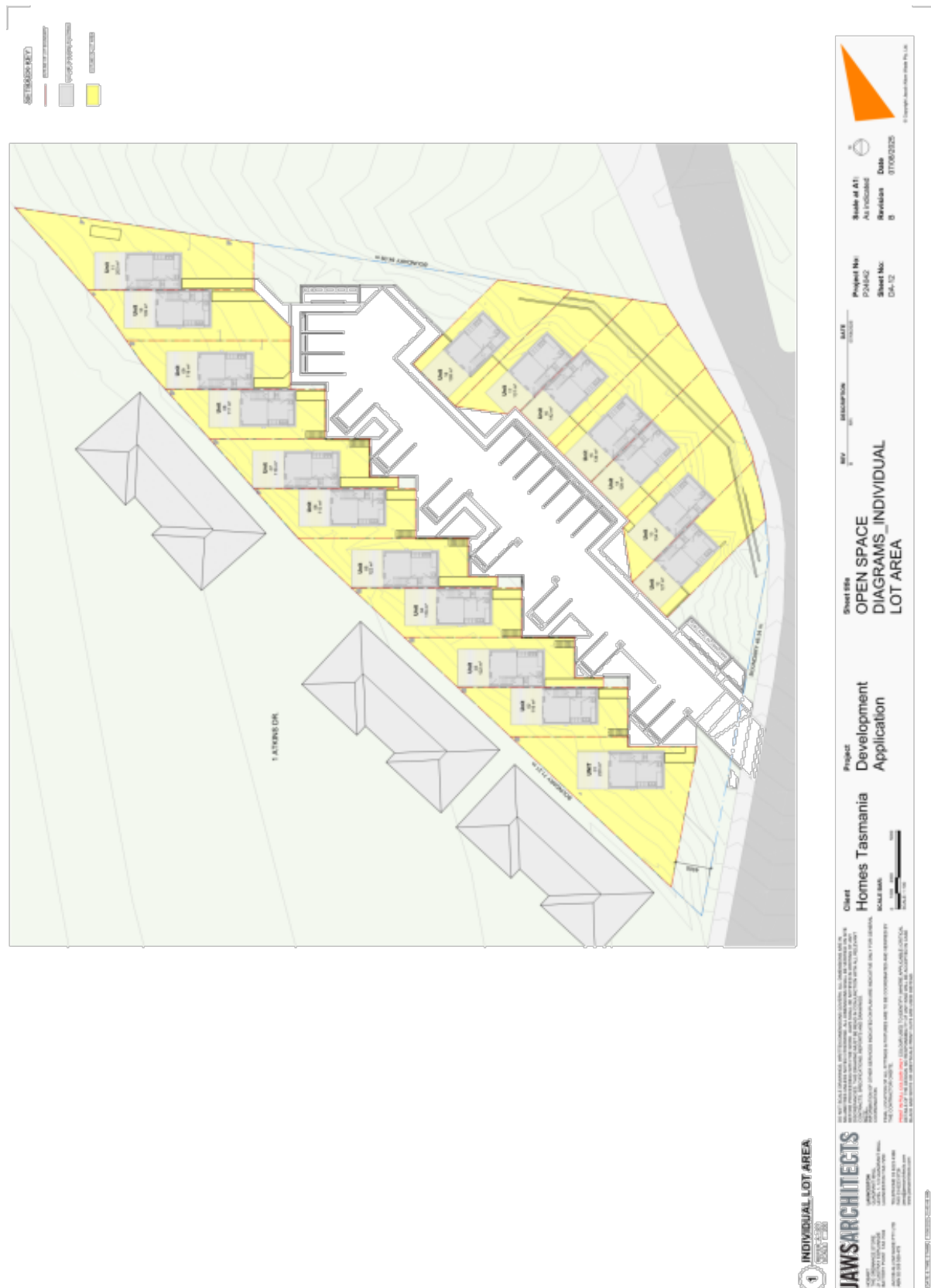


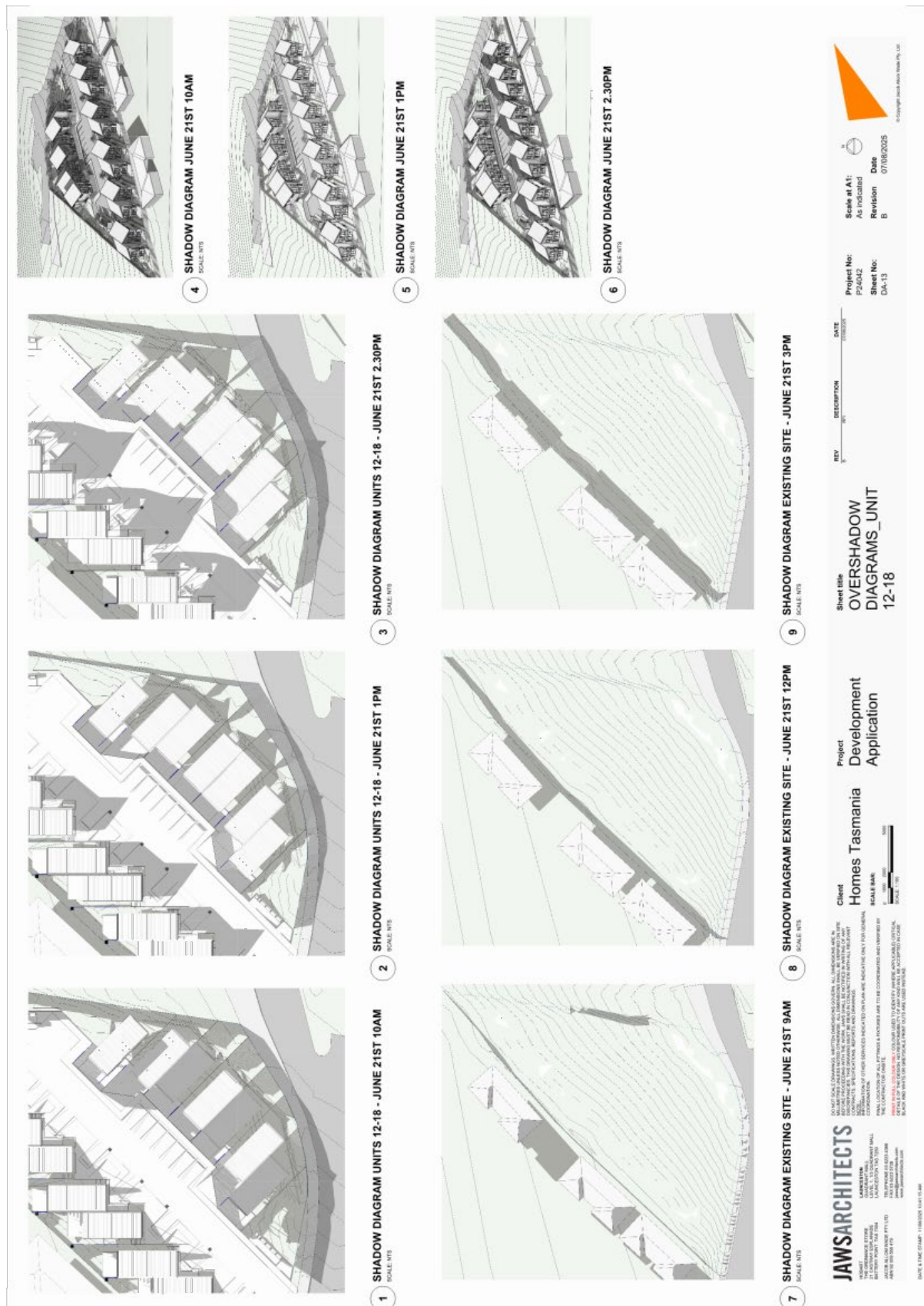


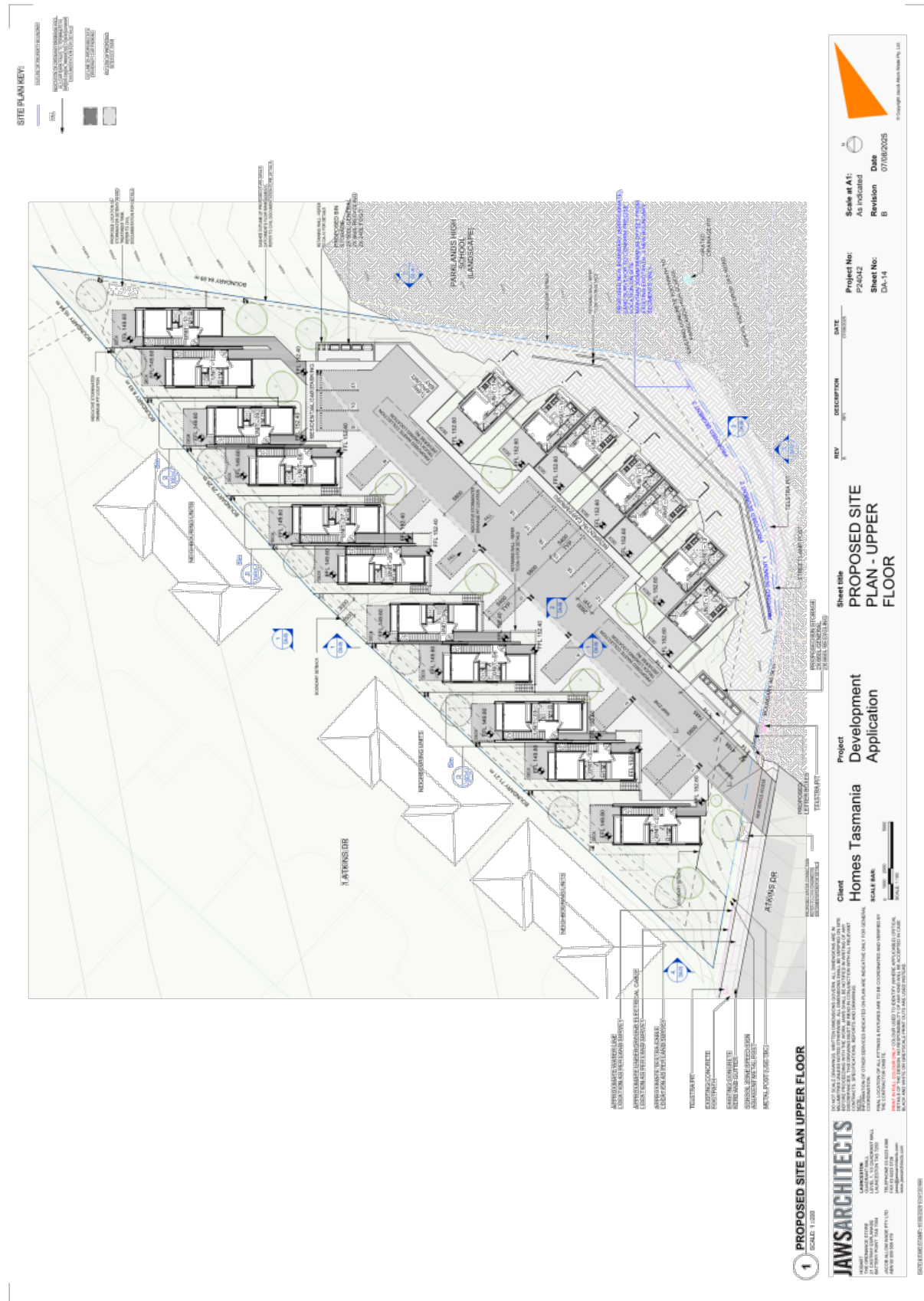














TRAFFIC IMPACT ASSESSMENT

Hubble Traffic
August 2025 - Updated

**UNIT DEVELOPMENT FOR SOCIAL
AND AFFORDABLE HOUSING
3 ATKINS DRIVE, ROMAINE**

Unit development 3 Atkins Drive, Romaine

Disclaimer: This report has been prepared based on and in reliance upon the information provided to Hubble Traffic Pty Ltd by the client and gathered by Hubble Traffic Pty Ltd during the preparation of the report. Whilst all reasonable skill, care and diligence has been used in preparation of the report, Hubble Traffic Pty Ltd take no responsibility for errors or omissions arising from misstatements by third parties.

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Version	Date	Reason for Issue
Draft	June 2025	Draft issued for client feedback
Final	June 2025	Final issued
Updated	August 2025	Driveway grades modified to suit 2% footpath



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Unit development 3 Atkins Drive, Romaine

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Unit development 3 Atkins Drive, Romaine

1. Introduction

Jaws Architects on behalf of Homes Tasmania (client), has engaged Hubble Traffic to prepare an independent Traffic Impact Assessment, for the development of 18 social and affordable housing apartments at 3 Atkins Drive, Romaine.

This report has considered the amount of traffic the development is likely to generate and how these additional traffic movements will integrate into the surrounding road network. The development has been assessed against the Tasmanian Planning Scheme C2 Parking and Sustainable Transport Code, C3 Road and Railway Assets Code, and the Australian Standard 2890.1:2004 (the Standard).

This report has been prepared to satisfy the requirements of Austroads, Guide to Traffic Management Part 12: Traffic Impacts of Developments, 2019, and referred to the following information and resources:

- Tasmanian Planning Scheme (Burnie)
- Road Traffic Authority NSW (RTA) Guide to Traffic Generating Developments
- Australian Standards AS2890 parts 1, 2 and 6
- Austroads series of Traffic Management and Road Design
 - Part 4: Intersection and crossings, General
 - Part 4a: Unsignalised and Signalised Intersections
 - Part 12: Traffic Impacts of Development
- Department of State Growth crash database
- Autoturn Online vehicle turning software
- LIST – Land Information Database



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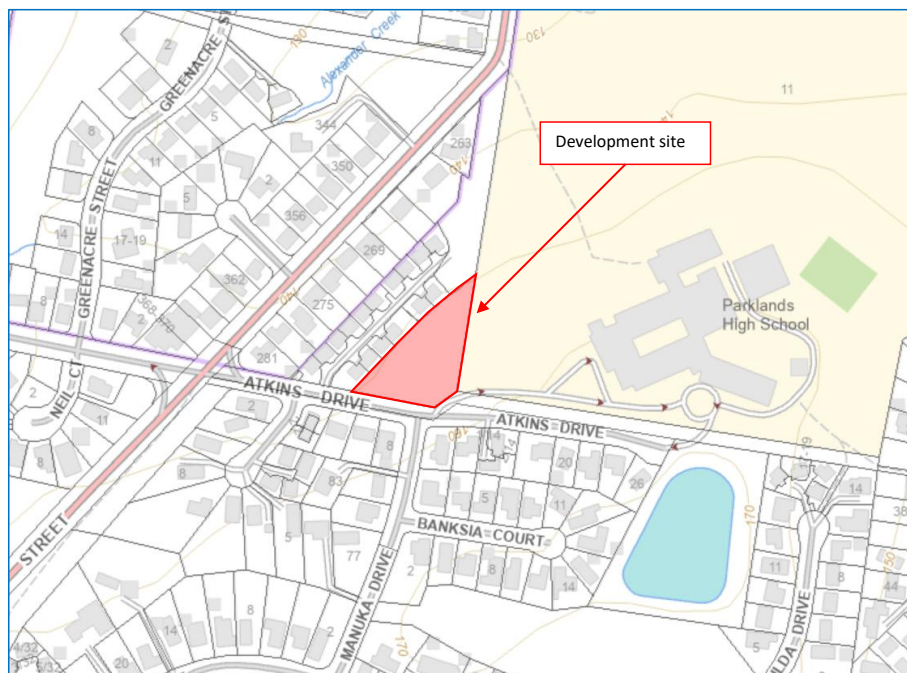
Unit development 3 Atkins Drive, Romaine

2. Site Description

Located at 3 Atkins Drive, Romaine, the development site is a large parcel of undeveloped land, situated on the side of a steep embankment.

According to the Land Information System Tasmania (LIST) Database, the site is situated within a General Residential zone. It is adjacent to Parklands High School and in close proximity to Romaine Park Primary School, local residential areas, and recreational reserves.

Diagram 2.0 – Extract from LIST land information database



Unit development 3 Atkins Drive, Romaine

3. Development proposal

The development proposal includes the construction of 18 two-bedroom residential units, with each unit allocated one on-site car parking space. Additionally, a new vehicular access will be created onto Atkins Drive.

Diagram 3.0 – Proposed development



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Unit development 3 Atkins Drive, Romaine

4. Number of car parking spaces required

4.1 Parking demand generated by social and affordable tenants

The supply of on-site car parking is an important consideration with new developments, to ensure overflow parking does not cause adverse impact to surrounding properties, particularly in residential areas.

Social and affordable housing caters to low-income households who generally have reduced car ownership, particularly when apartments are located within reasonable walking distance to services, activity centres, and frequent public transport services.

There is substantial evidence indicating that low-income households generate lower parking demand compared to private housing. Appendix A of this assessment provides data on car ownership from both mainland Australia and Tasmania, incorporating statistics from the Australian Bureau of Statistics (ABS). The most compelling data for this development comes from Tasmanian experiences, with information supplied by Centacare Evolve Housing, Homes Tasmania, and data from the Queens Walk complex. The table below illustrates the car parking demand of Tasmanian social and affordable housing sites, varying between 0.47 to 0.72 cars per dwelling/apartment.

Table 4.1A– Parking demand for Tasmanian social and affordable housing sites

Supplier	No of dwellings/units survey data is based on	Car parking demand per dwelling/unit
Centacare	Over 2100 properties	0.47
Homes Tasmania	78	0.55
Queens Walk	85	0.72 includes visitor parking
Average		0.58

The Tasmanian car ownership aligns closely with both mainland trends and ABS data. The Victorian GTA report, which focused on 67 public housing sites in Victoria comprising 802 social housing dwellings/units, is illustrated in the table below.

Table 4.1B – GTA survey data for Victorian social housing sites

Table 4.1: ABS Car Ownership Census Data for Social Housing – 2016				
Location	Bedroom Numbers	ABS Average Parking Rate (Car Spaces Per Dwelling)	Survey Data Car Parking Rate (Car Spaces Per Dwelling)	
			Average	85 th Percentile
Inner Metro	1 – 2 Bedrooms	0.41	0.36 (0 – 1.43)	0.58
	3 + Bedrooms	0.86		
Middle Metro	1 – 2 Bedrooms	0.74	0.39 (0 – 0.70)	0.65
	3 + Bedrooms	1.23		
Outer Metro	1 – 2 Bedrooms	0.96	0.63 (0 – 1)	0.88
	3 + Bedrooms	1.35		



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Unit development 3 Atkins Drive, Romaine

The GTA data is categorised into three distinct site classifications: inner metro, middle metro, and outer metro, based on the site's proximity to commercial and activity centres.

For this development site, the car parking rate for middle metro would be applicable, which is 0.65 spaces per one and two bedroom apartments. Similarly, the ABS census data for middle metro site is 0.74 spaces per two bedroom apartments.

Although the development site is constrained, there is capacity to provide 18 parking spaces, which means each unit could potentially be allocated one parking space, which is considered suitable based on the strong evidence that low-income households are likely to operate with less than one vehicle. This means the apartments are unlikely to generate parking overflow.

4.2 Planning Scheme car parking requirements

Planning scheme table C2.1 specifies the number of on-site parking spaces required for a multiple dwelling development, where a:

- two-bedroom or more dwelling (including all rooms capable of being used as a bedroom), requires 2 spaces per dwelling, and
- visitor parking for multiple dwellings of 1 dedicated space per 4 dwellings (rounded up to the nearest whole number); or if on an internal lot or located at the head of a cul-de-sac, 1 dedicated space per 3 dwellings (rounded up to the nearest whole number).

Table 4.2 - Number of parking spaces as per planning scheme requirements

Bedrooms	Number of apartment	Parking spaces required	Visitor spaces required
2 Bedroom	18	36	5

However, the planning scheme does not account for the low car ownership among social and affordable housing tenants, the reduced reliance on private vehicles due to the proximity of apartments to an activity centre, and the accessibility to a frequent public transport route.

4.3 Number of parking spaces the development will provide

In optimising the number of social and affordable housing apartments, the development will provide 18 on-site car parking spaces, meeting the reduced parking demand. Each apartment will likely be allocated one parking space, meaning visitor parking cannot be achieved on-site, and must rely on the on-street parking supply. A dedicated turning bay will be provided to ensure all vehicles can enter and leave in a forward-driving direction.



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5. Trip generation by this development

A trip in this report is defined as a one-way vehicular movement from one point to another, excluding the return journey. Therefore, a return trip to and from a land use is counted as two trips.

To determine the number of trips likely to be generated by this development, reference has been taken from the RTA Guide to Traffic Generating Developments (RTA Guide), section 3.3 residential housing.

This guide recommends for medium density residential units, (two bedrooms):

- Daily vehicle trips of 5 per unit, and
- Weekday peak trips of 0.5 per unit.

Based on the RTA guide, the 18 residential units are estimated to generate a total of 90 daily trips, with nine of these likely to occur during the peak periods.

Table 5.0 – Predicted number of trips generated from the 18 units

Dwelling type	RTA Generation rate	Number of units	Daily trips	Peak trips
Two bedroom unit	5 per day 0.5 per peak	18	90	9



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6. Existing traffic conditions

6.1 Atkins Drive

Atkins Drive functions as a local residential street within the surrounding network and is subject to the default 50 km/h urban speed limit. It extends for approximately 350 metres, terminating at the Parklands High School.

The residential properties along the street, include standalone dwellings and unit developments, all with off-street parking facilities. Parklands High School also has off-street parking facilities, with separate entry and exit access onto Atkins Drive. During school drop-off and pick-up times, 40 km/h school zone speed signs are in operation.

The road is constructed to an urban standard, with a sealed bitumen surface, concrete kerb and channel, 1.2-metre-wide concrete footpaths on both sides, and street lighting. The average road width is 10 metres, allowing on-street parking along one side while maintaining two-way traffic flow.

Although parking on both sides of the road reduces traffic flow to one lane, this is not problematic for short local roads with no through traffic, as the vehicles acts as traffic calming devices, moderating operating speeds, and this is appropriate for school environments.

Photograph 6.1 – Atkins Drive standard



Unit development 3 Atkins Drive, Romaine

6.2 Mount Street

Mount Street is part of the State Road network, managed by State Growth. It operates as a collector road within the surrounding network, connecting traffic from Bass Highway in the north to Ridgley Highway in the south.

Delineation of the straight horizontal road alignment is provided through a broken marked centreline, with the road situated within mostly flat terrain. Mount Street has a posted 60 km/h speed limit, with a 40 km/h school zone speed limit operating during school pick-up and drop-off times.

The road is constructed to an urban standard with a bitumen surface, concrete kerb and channel, concrete footpaths on either side, and street lighting. The average road width is 13 metres, suitable to accommodate on-street parking along both sides while maintaining two-way traffic flow.

Photograph 6.2 – Mount Street standard



Unit development 3 Atkins Drive, Romaine

6.3 Mount Street, Atkins Drive and Three Mile Line Road roundabout

Atkins Drive intersects Mount Street, with Three Mile Line Road on the opposite side, creating a four-way cross intersection. This intersection has recently been upgraded to a new roundabout (Mount Street roundabout), operating with a single lane for motorists approaching from each direction, with a slip lane provided for those turning left onto Three Mile Line Road from Mount Street.

Each approach provides adequate sight distance, enabling motorists to operate in a safe and efficient manner. Delineation is provided through appropriate road markings, signage, and street lighting. Pedestrian refuge crossings are available on each leg of the roundabout, allowing pedestrians to cross each road safely.

Photograph 6.3 – Mount Street roundabout



6.4 Pedestrian connectivity

A well-established footpath network on the surrounding roads provides pedestrians from the development site with a safe, accessible, and convenient route to public bus stops, recreational reserves, and local schools. These pedestrian facilities offer users an alternative mode of transport, thereby reducing reliance on private vehicles.

Pedestrian refuge crossings are provided on each leg of the Mount Street roundabout, ensuring safe pedestrian passage across each of the roads.

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6.5 Traffic activity

To understand the current traffic flow on the surrounding roads, traffic surveys were conducted during the morning and evening commuter peak periods at the Mount Street roundabout, as the majority of traffic generated from the development is expected to travel through this roundabout.

The manual surveys revealed that Mount Street generates a consistent traffic flow, with less than 600 two-way vehicles observed during the peak periods. Three Mile Line Road was found to be moderately trafficked, with a slightly higher two-way flow observed in the morning peak period.

Atkins Drive experienced a higher two-way vehicle flow during the morning peak period, with 345 vehicles observed, compared to 120 two-way vehicles in the evening peak. The increased flow in the morning is attributed to traffic generated by the school, while afternoon school traffic does not overlap with the evening commuter peak.

Table 6.5 captures the two-way flow on the surrounding roads, with the turning movements for the roundabout available in Appendix B.

Table 6.5 – Summary of traffic flows on the surrounding road network

Roundabout	Road	Peak hour two-way flows	
		Morning	Evening
Mount Street, Atkins Drive and Three Mile Line Road	Mount Street (North)	560	595
	Atkins Drive (East)	345	120
	Mount Street (South)	465	476
	Three Mile Line Road (West)	416	333



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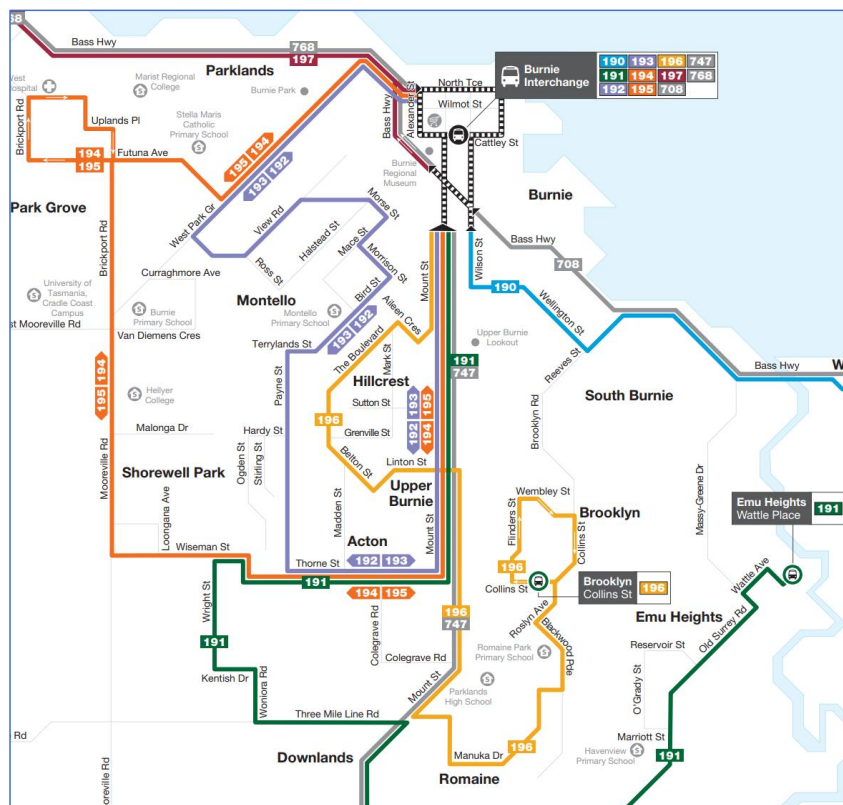
Unit development 3 Atkins Drive, Romaine

6.6 Public transport

Public bus stops are located along Atkins Drive, Mount Street and Three Mile Line Road, which are within 300 metres walking distance from the development site. These roads are serviced by two bus services, with each service connecting to the Burnie Interchange.

According to Metro Tasmania's website, route 191 operates once an hour between 7:00am and 6:00pm Monday to Saturday, while route 196 operates approximately once every two hours, Monday to Saturday.

Diagram 6.6 – Burnie bus service network



Unit development 3 Atkins Drive, Romaine

6.7 Traffic safety at this location

The Department of State Growth maintains a database of reported road crashes. A check of this database for the previous five years, found no crashes reported along Atkins Drive.

The recent implementation of the roundabout at Mount Street, Atkins Drive and Three Mile Line Road has significantly changed the traffic movements at the cross intersection. Therefore, the previous crash history at this intersection is no longer considered relevant.



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7. Impact from traffic generated by this development

As determined in section 5 of this report, the site is estimated to generate an additional 90 daily trips, with nine of these trips likely to occur during the commuter morning and evening peak hour periods.

Level of Service (LOS) is a quantifiable assessment of the factors that contribute to the traffic performance, which includes traffic density, gaps in traffic streams, expected delays, and queues. Austroads Guide provides performance criteria for urban traffic lanes (diagram 7.3) and junctions (diagram 7.2), with six levels from A to F.

LOS A provides the highest level of traffic performance, where motorists are not expected to incur traffic delays or queues, with ample gaps in the traffic stream for vehicles to turn freely and safely without disrupting other users.

7.1 Assignment of new trips generated by the development

It is common with residential developments that the majority (90 percent) of the trips leave within the morning peak, with the opposite occurring in the evening. The additional trips have been assigned to the surrounding road network, based on the current trip distribution captured by the manual surveys at the Mount Street roundabout, as shown in table 7.1.

Table 7.1 – Estimated trip distribution

Period	Mount Street roundabout							
	Vehicles arriving				Vehicles leaving			
	Left	Straight	Right	Total	Left	Straight	Right	Total
Morning peak	1	0	0	1	1	2	5	8
Evening peak	4	3	1	8	0	0	1	1



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7.2 Traffic impact efficiency at the roundabout

The simplest method to determine the traffic performance at a junction or roundabout is to use SIDRA Intersection traffic modelling software, which uses gap acceptance theory to determine the average delay, queue lengths, and degree of saturation, which are all measures of traffic congestion and level of service. Austroads Guide provides six levels of service for junctions and roundabouts as shown in the table below.

Diagram 7.2 – Austroads Guide for level of service at junctions, intersections, and roundabouts

Level of	Average delay per vehicle (secs/vehicle)	Traffic Signals and Roundabouts	Give Way and Stop controls
A	<10	Good operation	Good operation
B	10 to <20	Good with acceptable delays and spare capacity	Acceptable delays and spare capacity
C	20 to <35	Satisfactory	Satisfactory, but crash study required
D	35 to <50	Operating near capacity, acceptable for State Roads	Near capacity and crash study required
E	50 to <70	At capacity for signals, will cause excessive delays. Roundabouts require other control mode	At capacity, requires other control modes
F	>70	Saturated flows – at signals, consider additional lanes	

A traffic model was developed using SIDRA software with recent peak hour traffic flows to replicate the Mount Street roundabout.

Modelling indicates that during the morning peak, the roundabout is operating with a DOS of 0.20, representing 20 percent capacity. Motorists are provided with a high level of traffic efficiency of LOS A, with the worst delay estimated at 10.3 seconds and a maximum queue length of eight metres or less than two vehicles.

In the evening period, the roundabout is operating with a DOS of 0.29. Despite the roundabout catering for higher traffic flows, it still maintains a high level of performance, operating at LOS A overall, with an estimated delay of 10.3 seconds and a maximum queue length of 12.8 metres, or two vehicles.

The additional trips generated by the development site have been assigned to the roundabout. Modelling indicates that while the development will intensify traffic flow, it will not deteriorate the current traffic efficiency experienced by motorists.



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As the local area continues to grow, traffic on the surrounding road network is expected to increase. Incremental traffic growth has been assigned to the roundabout, based on one percent annual growth for the next 10 years. The modelling results within Table 7.2 illustrate the likely change in traffic performance for the roundabout as a whole, rather than individual turn movements. The results demonstrate no significant deterioration in performance, with the roundabout expected to provide all users with a high level of service. Modelling printouts are available in Appendix C.

Table 7.2 – Traffic modelling comparison

Period	Scenario	Total vehicles	DOS	Worst delay	Overall, LOS	Max Queue
Morning peak	Existing	832	0.202	10.3 secs	A	8.0m
	With development	841	0.202	10.3 secs	A	8.0m
	1% growth	928	0.230	10.6 secs	A	9.3m
Evening peak	Existing	758	0.289	10.3 secs	A	12.8m
	With development	766	0.294	10.3 secs	A	13.0m
	1% growth	847	0.328	10.6 secs	A	15.2m



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7.3 Lane capacity and level of service for surrounding roads

In evaluating the impact of additional vehicles on the surrounding road network, it is important to understand LOS motorists are currently receiving, which is done by comparing the peak hour traffic flow with diagram 7.3 from the RTA Guide, for urban environments.

Diagram 7.3 – Extract from the RTA Guide

Table 4.4 Urban road peak hour flows per direction		
Level of Service	One Lane (veh/hr)	Two Lanes (veh/hr)
A	200	900
B	380	1400
C	600	1800
D	900	2200
E	1400	2800

From the manual traffic surveys, all roads are operating at a high level of traffic performance of either LOS A or B during the peak periods. This means that the traffic is stable, motorists have freedom to select their own operating speed, and there should be sufficient gaps in the traffic stream to enable vehicles to enter and leave, without causing adverse impacts.

The additional peak hour trips have been assigned to the surrounding road network, with Table 7.3 demonstrating the increase in vehicular trips is not expected to cause adverse traffic impact on the surrounding road network, as level of service will not deteriorate, with all traffic lanes to continue to operate at LOS A or B.

Table 7.3 – Comparison of traffic performance on the surrounding roads

	Mount Street (North)				Atkins Drive (East)			
	Morning		Evening		Morning		Evening	
	NB	SB	NB	SB	EB	WB	EB	WB
Existing flows	358	202	242	353	163	182	80	40
Level of Service	B	B	B	B	A	A	A	A
With development	363	203	243	357	164	190	88	41
Level of Service	B	B	B	B	A	A	A	A

	Mount Street (South)				Three Mile Line Road (West)			
	Morning		Evening		Morning		Evening	
	NB	SB	NB	SB	EB	WB	EB	WB
Existing flows	315	150	182	294	194	222	187	136
Level of Service	B	A	A	B	A	B	A	A
With development	315	151	183	294	194	224	190	146
Level of Service	B	A	A	B	A	B	A	A



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7.4 Impact on residential amenity

A new development in residential areas can be concerning to local residents, and it can be difficult to argue that a traffic increase is reasonable. The RTA Guide has considered this matter and provided an environmental performance standard, as illustrated in extract 7.4.

The school generates significant traffic for about two hours each school day, with two-way traffic reaching 345 vehicles. Outside of this period, the traffic flow decreases to less than 125 vehicles. Therefore, the school's intense traffic impact is limited in duration and has minimal effect on residential amenity.

With the additional nine peak trips generated by the development, the street for the majority of the day will continue to operate with fewer than 200 vehicles per hour, thereby maintaining an acceptable level of residential amenity.

Extract 7.4 – RTA Guide performance standards for residential streets

Environmental capacity performance standards on residential streets			
Road class	Road type	Maximum Speed (km/hr)	Maximum peak hour volume (veh/hr)
Local	Access way	25	100
	Street	40	200 environmental goal 300 maximum
Collector	Street	50	300 environmental goal
			500 maximum

7.5 On-street parking impact to surrounding properties

In optimising the number of social and affordable apartments, only 18 on-street parking spaces can be achieved. If these spaces are occupied by tenants, visitors will need to utilise the available on-street parking. Lower car ownership is typically associated with low-income households, which may result in reduced visitor parking demands compared to private housing. The planning scheme specifies a visitor parking rate of one space per four multiple dwellings for private housing; however, it would be reasonable to reduce this rate to one space per eight social or affordable apartments. Consequently, the development may generate a lower visitor parking demand of two to three vehicles.

A recent site inspection found that the surrounding residential properties generate a low on-street parking demand, and the nearby school has on-site parking spaces. The development site has road frontage exceeding 30 metres, suitable for accommodating on-street parking for six vehicles. This suggests that visitor parking is unlikely to adversely impact the surrounding residential properties, and on-street parking can be accommodated without causing impact to traffic flow.



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Unit development 3 Atkins Drive, Romaine

8. Development layout

8.1 New vehicular access onto Atkins Street

The development will create a new vehicular access onto Atkins Drive, measuring at least 5.5 metres wide to accommodate two-way traffic flow. Designing the access driveway intersecting with Atkins Drive at this location will be challenging due to Atkins Drive's 13.8% grade and the existing footpath grades sloping towards the roadway with grades exceeding 4%.

The proposed access and driveway design will modify the current footpath grade to 2%. With the footpath grade falling towards the roadway and the natural slope of the site falling away from the footpath, this will necessitate the driveway to operate with a vertical crest. Along the northern side of the driveway, this vertical crest will have a change in grade of 15.2% and include a flat transition zone. On the southern side of the driveway, the maximum change in grade will be 10.4%. With crossfall occurring between the southern and northern driveway edges, the change in grade through the middle of the driveway will range between 15.2% and 10.4%.

The Standard specifies that where the change in grade exceeds 12.5% for a vertical crest, vehicle templates (appendix C of the Standard) must demonstrate sufficient ground clearance. Section B5 of the Standard indicates that for non-domestic properties, the ground clearance template for B99 vehicles should be utilised.

The driveway has been designed with a series of short transitional grades that function similarly to a vertical curve to provide gradual change. The designer has demonstrated that the proposed driveway offers sufficient ground clearance by using B99 vehicle templates along the longitudinal driveway profile on both sides of the driveway, as illustrated in the two following diagrams.

The maximum driveway grade of 19.2% is within the acceptable tolerance for a residential driveway, and overall, the driveway is expected to provide adequate ground clearance, and provide suitable level of service.



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Diagram 8.1A – Vertical grades along the southern side, with B99 vehicle template

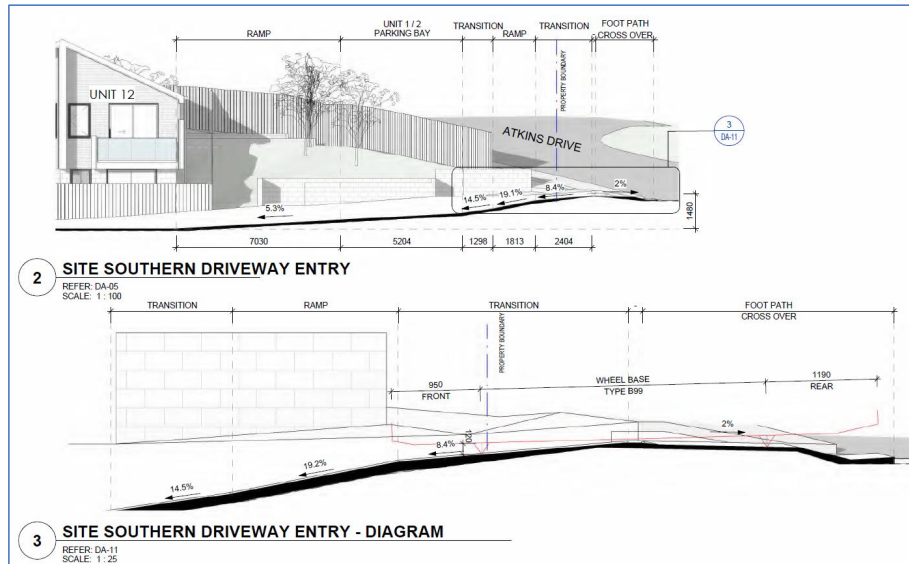
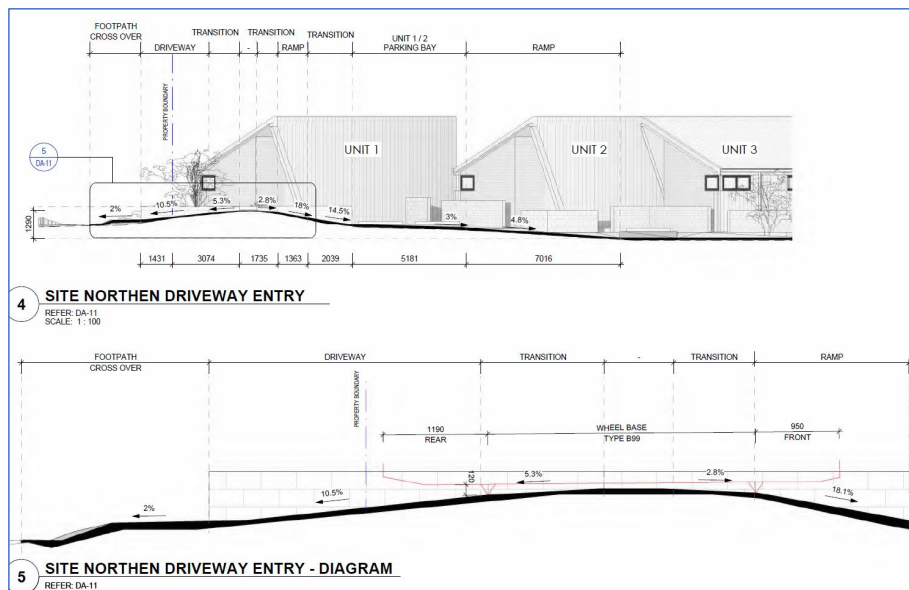
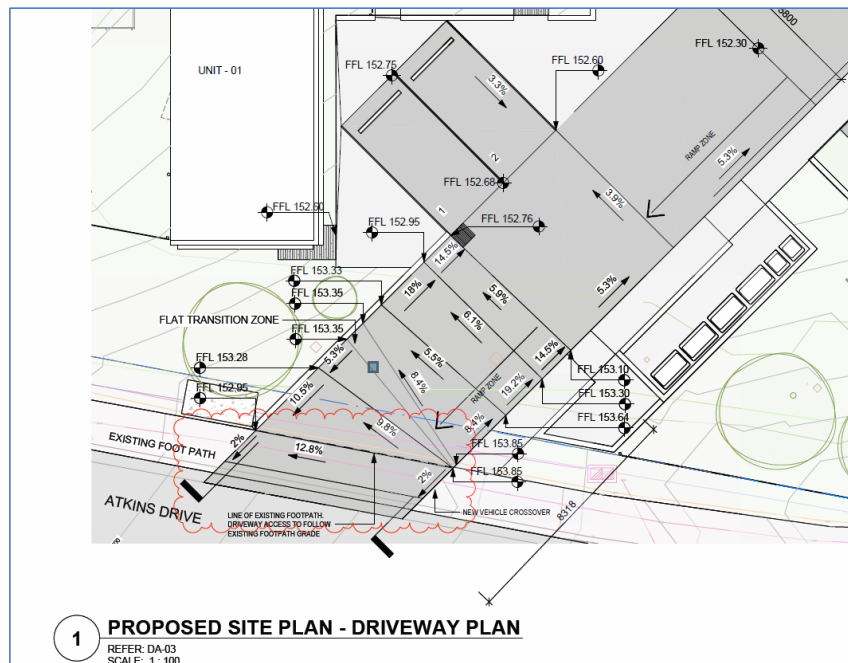


Diagram 8.1B – Vertical grades along the northern side and B99 vehicle template



Unit development 3 Atkins Drive, Romaine

Diagram 8.1C – Proposed driveway vertical grades



This new access will be designed to comply with LGAT Standard Drawing TSD-R09-v3 for an urban driveway access, and with minor modification to the current footpath grades.

Photograph 8.1 – Location of new access onto Atkins Drive

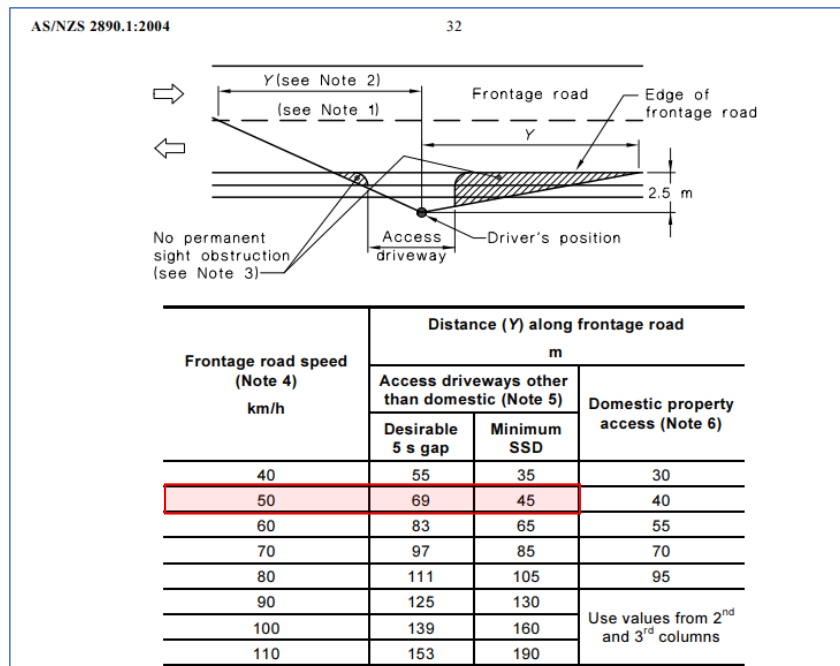


Unit development 3 Atkins Drive, Romaine

8.2 Site distance at the new driveway access

Under the Standard, the development would be classified as a residential rather than domestic property. For a residential property, the appropriate sight distance is specified in Standard section 3.2.4 and figure 3.2. Figure 3.2 specifies the desirable sight distance for a residential property within a 50 km/h speed limit as 69 metres, as shown in the extract below.

Extract 8.2 Sight distance requirements at access driveways (AS 2890.1:2004)



The available sight distance at the new driveway access was measured on-site, based on the driver being 1.1 metres above the driveway surface and an approaching vehicle being 1.2 metres high. In both directions the available sight distance exceeds 90 metres.

With the available sight distance in each direction exceeding the required 69 metres, drivers will have sufficient sight distance to enter Atkins Drive in a safe and efficient manner, as shown in the following two photographs.

Unit development 3 Atkins Drive, Romaine

Photograph 8.2A – Available sight distance to the left



Photograph 8.2B – Available sight distance to the right



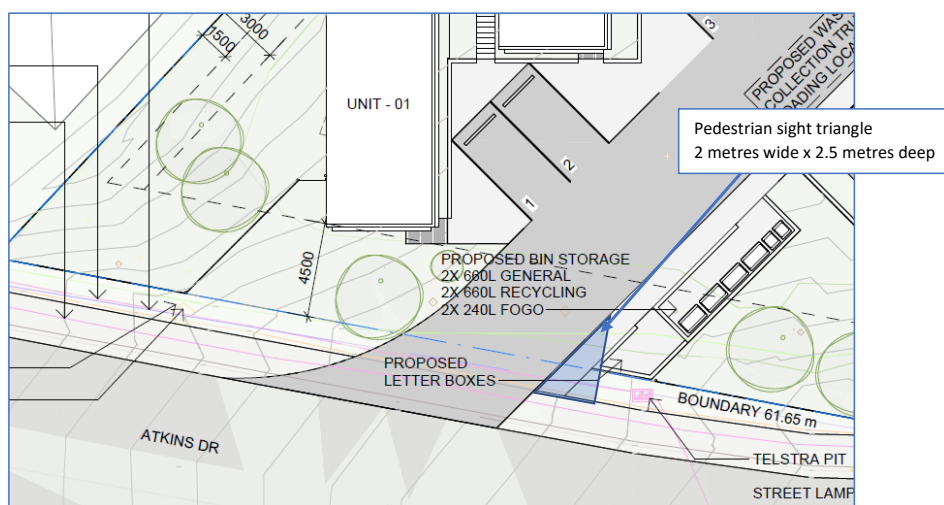
Unit development 3 Atkins Drive, Romaine

8.3 Pedestrian sight distance

It is important for drivers leaving the development site to have adequate sight lines between a driver leaving the site and pedestrians using the footpath, which will be achieved by not having any physical obstacles on either side of the driveway.

The diagram below demonstrates the pedestrian sight triangle for a driver leaving the site, as defined in the Standard figure 3.3.

Diagram 8.3 – Pedestrian sight distance



Unit development 3 Atkins Drive, Romaine

8.4 Dimensions of on-site parking spaces

The car parking spaces have been designed as User Class 1A under the Standard, suitable for residential or domestic use. Each space will be a minimum of 2.4 metres wide, 5.4 metres long, and supported with a minimum manoeuvring area or parking aisle width of 5.8 metres.

All on-site parking spaces will be situated on a grade of less than five percent, delineated with pavement markings, and supported with wheel stops. Where a parking space is located adjacent to a vertical obstruction higher than 150 millimetres, an additional 0.3 metres of width will be provided between the space and the obstruction. If there is a vertical obstruction on both sides, 0.3 metres will be provided on both sides of the space.

8.5 Vehicle manoeuvrability into and out of the parking spaces

Vehicle swept path software has been used to demonstrate B85 vehicles can enter and leave all the parking spaces efficiently. A selection of vehicle swept path diagrams are available in Appendix D.

8.6 Pedestrian access

A suitable internal pedestrian pathway will be provided, connecting the units and parking spaces with the existing footpath along Atkins Drive. This pathway will be a minimum of one metre wide, constructed with a concrete surface, separated from the driveway and parking spaces by kerbing.

Overall, pedestrians will be provided with a high level of service, allowing them to move throughout the site safely and efficiently, complying with the intent of the planning scheme.



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8.7 Other parking requirements

Motorcycle parking

Planning scheme table C2.4 specifies that a use providing less than 20 on-site car parking spaces is not required to provide any dedicated motorcycle parking spaces.

Bicycle parking

Planning scheme table C2.1 specifies that residential use, does not require dedicated bicycle parking spaces.

Accessible parking

According to the National Construction Code (NCC) the residential units would be classified as a class 1a building, which are typically single dwellings or a group of detached dwellings, being a town house or a row of houses.

The NCC specifies class 1a buildings do not require accessible parking.

8.8 Driveway standard

The design incorporates an internal driveway, extending from Atkins Drive and terminating at units 9 to 11. This driveway will be a minimum of 5.8 metres wide to accommodate two-way traffic flow and the manoeuvring of vehicles into and out of the parking spaces.

A turning bay provided at the end of the internal driveway, allows all light vehicles to turnaround on-site and leave in a forward-driving direction. The internal driveway and parking areas will be constructed with a concrete surface, supported by barrier kerbing to capture surface water, which will feed into an approved stormwater system.



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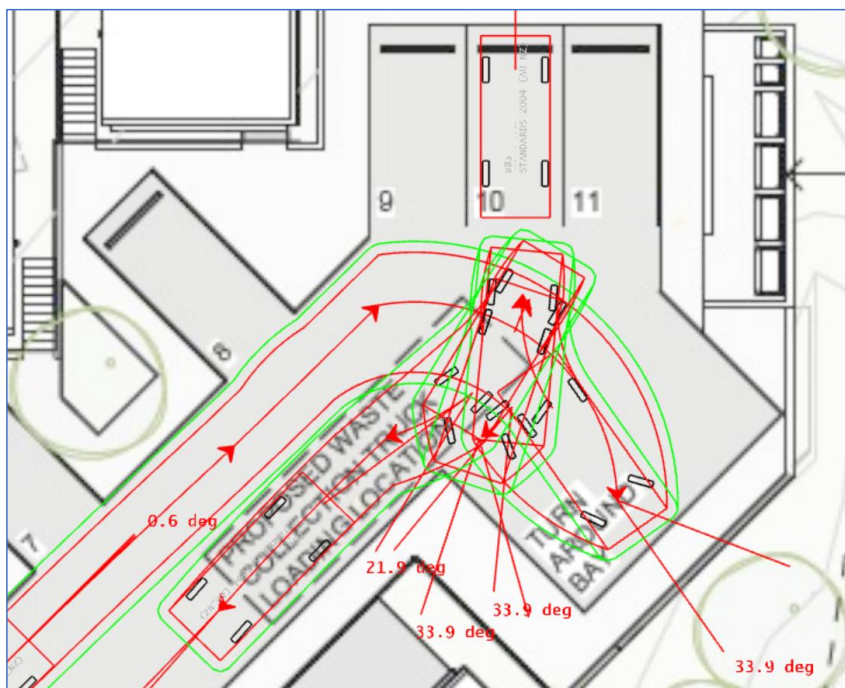
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8.9 Turning bay

Vehicle swept path software has been used to demonstrate that a B99 vehicle can enter, turn around using the turning bay, and exit in a forward-driving direction, as illustrated in the diagram below.

Diagram 8.10 – Swept path of a B99 vehicle turning around



Unit development 3 Atkins Drive, Romaine

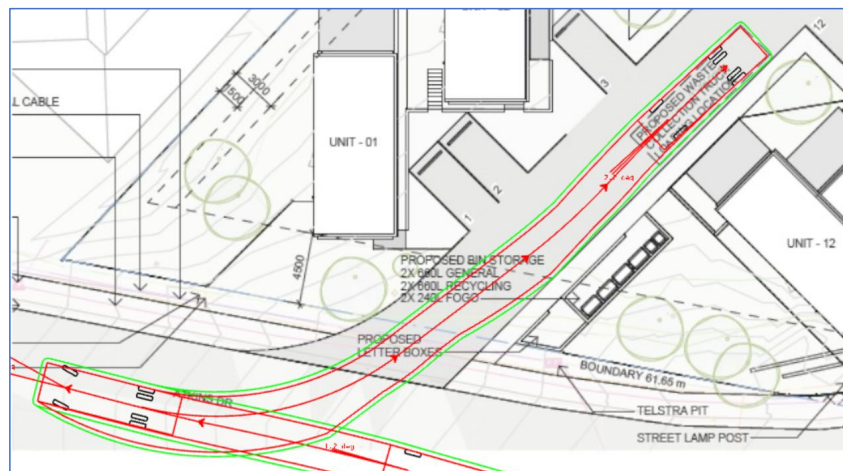
8.10 Waste collection

The client has advised that a private contractor will be engaged to service the apartments. Due to the short internal driveway, a collection vehicle can reverse into the site, collect the waste, and exit in a forward-driving direction.

This reversing manoeuvre is not anticipated to cause any adverse traffic or safety impacts. The waste collection drivers are professional operators equipped with reversing cameras, and the reversing movement is infrequent, estimated at once per week. The vehicle will arrive at the site when there are no pedestrians or cyclists in the area. The driver will then reverse the short distance into the driveway. This infrequent manoeuvre can be performed safely, without adversely impacting other users.

Vehicle swept path software has been used to demonstrate the swept path of a medium rigid vehicle, which is similar in size to a waste collection vehicle. The diagram demonstrates the vehicle can reverse into the site and leave in a forward-driving direction.

Diagram 8.11 – Swept path of a medium rigid vehicle



Unit development 3 Atkins Drive, Romaine

8.11 Access for emergency vehicles

It is important that all apartments are accessible to fire emergency vehicles. According to the Tasmania Fire Service, their newest heavy pump vehicle has dimensions similar to a medium rigid vehicle, measuring 8.8 metres in length.

The driveway will have sufficient width to accommodate a fire service vehicle entering in a forward-driving direction. Due to the inadequate space for turning around, the vehicle will need to reverse out, which is acceptable given the infrequency of this manoeuvre.



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9. Planning scheme

9.1 C2.0 Parking and Sustainable Transport Code

C2.5.1 Car parking numbers

Under table C2.1 of the planning scheme, this residential development is required to provide a total of 41 on-site car parking spaces to comply with the acceptable solution. As the planning scheme does not mention social or affordable housing, it is assumed this parking requirement is based on private housing.

Section 4 of this assessment provided strong evidence that low-income households have a lower car ownership than private housing and determined that 18 on-site car parking spaces is expected to meet the reasonable demand, minimising parking overflow. As this number of parking spaces is less than the acceptable solution, the development must be assessed against the performance criteria P1.2.

Performance criteria	Assessment
The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:	
a) the nature and intensity of the use and car parking required;	The development is for residential units, which are situated within close proximity to public bus stops, providing tenants with an alternative mode of transport. This assessment has demonstrated that one parking space per each apartment is considered acceptable, minimising parking overflow.
b) the size of the dwelling and the number of bedrooms; and	The development consists of 18 two-bedroom units, which will be allocated to social and affordable housing tenants. Each unit will be less than 80 square metres in size.
c) the pattern of parking in the surrounding area.	The surrounding residential properties and Parklands High School have off-street parking facilities, generating a low demand for on-street parking. Atkins Drive has sufficient width to accommodate on-street parking, while maintaining efficient traffic flow for the road function. The development site has at least 30 metres of road frontage to accommodate on-street parking without adversely impacting surrounding properties.

C2.5.2 Bicycle parking numbers

Planning scheme table C2.1 specifies that a residential use does not require bicycle parking.

C2.5.3 Motorcycle parking numbers

Planning scheme table C2.4 specifies that a use providing less than 20 on-site car parking spaces is not required to provide dedicated motorcycle parking spaces.



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C2.5.4 Loading bays

This clause does not apply to residential use.

C2.6. Development standards

C2.6.1 Construction of parking areas.	The driveway and parking areas will be constructed with a concrete surface and supported with appropriate kerbing to collect and direct surface water to an approved stormwater drainage system, complying with the acceptable solution A1.
C2.6.2 Design and layout of parking areas.	The internal layout and parking areas have been designed to comply with the Standard, to ensure vehicles can easily enter, manoeuvre, and leave in a forward-driving direction. There will be sufficient manoeuvring width adjacent to the parking spaces, to enable vehicles to enter and leave efficiently. All parking spaces will be located on a gradient of less than five percent, delineated with pavement markings and supported with wheel stops. The internal widths of the driveways and parking aisles will be a minimum of 5.5 metres wide, accommodating two-way traffic flow and complying with table C2.2 of the planning scheme. Overall, the parking spaces and internal layout complies with the acceptable solution A1.1 (b).
C2.6.3 Number of accesses for vehicles.	The development will create a new access onto Atkins Drive, which will be suitable to accommodate two-way traffic flow. With the development operating with a single access, it will comply with the planning scheme acceptable solution.
C2.6.4 lighting of parking areas within the general business zone and central business zone	Internal driveways, parking spaces, and pedestrian pathways, will be lit to the appropriate standards, complying with the acceptable solution.
C2.6.5 Pedestrian access.	A pedestrian pathway will be provided, connecting the units and parking spaces with the existing footpath located along Atkins Drive. This pathway will be a minimum of one metre wide, constructed with a concrete surface and separated from the driveway and parking spaces by barrier kerbing. Overall, pedestrians will be provided with a high level of service, complying with the acceptable solution of the planning scheme.
C2.6.6 Loading bays.	Not applicable for a residential development.
C2.6.7 Bicycle parking and storage facilities	Not applicable for a residential development.
C2.6.8 Siting of parking and turning areas.	Not applicable for a general residential zone.



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9.2 C3.5.1 Traffic generation at a vehicle crossing, level crossing or new intersection

The development will create a new vehicular access onto Atkins Drive and is required to be assessed against the performance criteria P1, ensuring that the access can operate safely and efficiently.

Performance criteria	Assessment
Vehicular traffic to and from the site must minimise any adverse effects on the safety of an intersection, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:	
a) Any increase in the traffic caused by the use;	The development site is estimated to generate 90 daily vehicle trips, with nine of these likely to occur during the morning and evening peak periods.
b) The nature and frequency of the traffic generated by the use;	The residential units are expected to generate light vehicles less than 5.5 metres in length. These types of vehicles are associated with urban residential living, have good manoeuvrability, and are compatible with the existing vehicles using the surrounding road network.
c) The nature of the road;	Atkins Drive is a local residential street, built to an urban standard, with sufficient road width to accommodate on-street parking, while maintaining efficient traffic flow. The proposed new vehicular access onto Atkins Drive will have sufficient available sight distance for a residential property, enabling safe vehicular movements to and from the development site.
d) The speed limit and traffic flow of the road;	Atkins Drive operates under a 50 km/h urban speed limit, with a 40 km/h school zone during drop-off and pick-up times. The school generates increased traffic flow as students arrive and leave, with 345 two-way vehicles observed during the morning peak. Outside of school periods, Atkins Drive is lightly trafficked, with the evening peak generating 125 two-way vehicles. These flows ensure both school and residential users are provided with an acceptable level of service. Traffic modelling indicates that the Mount Street roundabout provides motorists with a high level of traffic efficiency during both peak periods, allowing vehicles to enter and leave Atkins Drive in a safe and efficient manner. The additional traffic generated by the development is not expected to cause any deterioration in traffic performance or adversely impact traffic flow on the surrounding road network. Traffic modelling demonstrated there is spare capacity for incremental growth in the local area for the next decade, without adversely impacting motorists using the roundabout.
e) Any alternative access;	None.
f) The need for the access or intersection;	Infill development in established residential areas is an excellent way to meet the housing shortage, while optimising the existing infrastructure and community services.
g) Any traffic impact assessment; and	An independent traffic assessment found no reason for this development not to proceed.



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h) Any written advice received from the road authority.	Aware of none.
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10. Conclusion

From a traffic engineering and road safety perspective, additional vehicle movements generated by this development are not expected to create any adverse safety or traffic impact, as:

- the amount of traffic expected to be generated during the peak hour periods is low, and there is sufficient capacity within the surrounding street network to absorb the extra vehicle movements, without adversely impacting other users, or the level of traffic performance,
- drivers will have sufficient sight distance to enter and leave the site in a safe and efficient manner,
- based on the lower car ownership of social and affordable housing tenants, there will be a sufficient supply of on-site parking spaces to meet the reasonable demand,
- internal pedestrian pathways will allow pedestrians to move around the development site in a safe and convenient manner and to connect with the existing footpath along Atkins Drive,
- the driveway layout will be able to accommodate light vehicles to enter, circulate, and leave efficiently in a forward-driving direction, and
- there is sufficient driveway width to accommodate waste and emergency service vehicles.

This Traffic Impact Assessment found no reason for this development not to proceed.



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11. Appendix A – Parking demand generated by low-income households

The supply of on-site car parking is an important consideration with new developments, to ensure overflow parking does not cause adverse impact to surrounding properties, particularly in residential areas.

Social and affordable housing caters to low-income households, resulting in reduced car ownership. While the available survey data primarily focuses on social housing, the survey data is also suitable for affordable housing tenants, as both types operate with a low household income, leading to decreased car ownership.

Reference has been taken from two mainland documents, to provide information on car parking rates for social housing developments. Queensland Government design standard 2015 for construction of social housing dwellings and apartments, and the GTA Review of Social Housing Car Parking Demands Car Parking study 2017, commissioned by the Victorian Department of Health & Human Services. Both documents provide research data on car parking rates associated with social housing developments, with ABS car ownership rates provided in the GTA information.

These documents demonstrate that tenants of social housing within close proximity to public transport, and accessibility to activity centres that provide a range of retail and commercial facilities, have a lower car ownership compared with private housing.

It is appreciated that these reference documents are based on mainland conditions. Centacare Evolve Housing and Homes Tasmania have also provided correspondence with car parking survey data from their existing social housing sites, which is used to demonstrate that lived experience of car ownership in Tasmania strongly correlates with the mainland outcomes.

These documents provide a good resource for predicting the car parking demands for social housing apartments and are considered appropriate for use in this development, as the data shows a strong correlation, giving a high level of confidence.



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11.1 Queensland design standards for social housing developments

In 2015 the Queensland Government published design standards for construction of social housing (houses and apartments), which recognises that social housing in accessible areas requires a lower number of on-site parking spaces. This design standard allows for the reduction of car parking spaces for social housing, based on the site location. The design standards include different parking requirements based on the three site categories.

Extract 11.1 - Queensland Government Design Standards for Social Housing

TABLE 1: Reduced car parking rates Initial Allocation	
Applies To	Rate (number of covered spaces required)
First 3 Units	1 space per unit

TABLE 2: Reduced car parking rates Rates Applicable to Remaining Units After Initial Allocation (to be rounded up if required)			
Number of bedrooms	Site Location Category (refer table 3) Rate (number of covered spaces required)		
	A	B	C
Studio	1 per 4 units	1 per 2 units	NA
1 Bedroom	1 per 2 units	2 per 3 units	1 per each unit
2 Bedrooms	2 per 3 units	1 per each unit	1 per each unit

TABLE 3: Site location categories	
Category	Site location
A	Major centre Brisbane within 800m walking distance of the pedestrian entry to a train station; or within 600m walking distance of an express bus stop; or within 400m walking distance of an appropriate local bus stop* Excludes neighbourhoods zoned low density in the planning scheme (refer category C)
B	Major centre not meeting requirements of category A for proximity to public transport; or Large regional centres such as Gold Coast, Sunshine Coast, Cairns, Caboolture, Gladstone, Ipswich, Logan, Mackay, Pine Rivers, Redcliffe, Redlands, Rockhampton, Toowoomba, Townsville, Yeppoon etc. and Large towns such as Bundaberg, Maryborough, Hervey Bay within 400 metres of a local bus stop* Excludes neighbourhoods zoned low density in the planning scheme (refer category C)
C	Not located in a major centre, large regional centre or large town meeting category 'A' or 'B' criteria e.g. Beaudesert, Charleville, Dalby, Longreach, Mount Isa, Roma, St George etc. Includes sites zoned low density residential in the planning scheme (including Brisbane Suburbs Improvement Strategy sites or similar zoned low density in the planning scheme)

* Note: an appropriate local bus service constitutes a minimum of 6 days, 7am to 7pm, at least hourly



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11.2 GTA research report for the Melbourne Metropolitan area

The GTA report was commissioned by the Victorian Department of Health & Human Services, to undertake research for car parking rates associated with social housing developments. The research included the survey of 67 public housing operational sites, containing 802 social housing dwellings/apartments, the report also contains information from the Australian Bureau of Statistics (ABS) on car ownership.

The research report classified the locations of the development properties as either inner, middle, or outer metropolitan. With the methodology of data capture for the GTA report included questionnaires sent to the relevant housing organisation, which were completed by the housing officer responsible for the site, with the individual residents not surveyed. The research also analysed ABS data for car ownership for social housing.

Table 11.2 from the GTA report captures both ABS and survey data.

Table 4.1: ABS Car Ownership Census Data for Social Housing – 2016				
Location	Bedroom Numbers	ABS Average Parking Rate (Car Spaces Per Dwelling)	Survey Data Car Parking Rate (Car Spaces Per Dwelling)	
			Average	85 th Percentile
Inner Metro	1 – 2 Bedrooms	0.41	0.36 (0 – 1.43)	0.58
	3 + Bedrooms	0.86		
Middle Metro	1 – 2 Bedrooms	0.74	0.39 (0 – 0.70)	0.65
	3 + Bedrooms	1.23		
Outer Metro	1 – 2 Bedrooms	0.96	0.63 (0 – 1)	0.88
	3 + Bedrooms	1.35		

Key findings of the research report that have a direct correlation to the assessment of this development site:

- There is a significant difference in car ownership characteristics of private and social housing.
- There are clear bands of car ownership between inner, middle, and outer metropolitan areas.
- It is difficult to define exact car parking rates that can be applied to individual developments, given that each development site will inevitably have a different mix of public transport accessibility, access to walking and cycling infrastructure, surrounding activity centres including retail, commercial and education land-use.



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11.3 Social housing parking data – Tasmanian Experience

Centacare Evolve Housing (Centacare) are a Tier 1 Community Housing Provider, who own and manage over 2,100 social and affordable housing properties throughout Tasmania. Centacare arranged for a survey of car ownership on a sample of sites they managed in Southern Tasmania, which involved six properties containing 103 social housing dwellings.

The car ownership for the six sites is contained in the table below, with a maximum of 0.47 cars per dwelling, clearly demonstrating social housing generates a low car parking demand. This data is considered as lived experience for Tasmania, and strongly correlates with the mainland reference documents discussed.

Table 11.3 – Centacare car ownership rates for social housing dwellings

Property	Number of dwellings	Time period Centacare has managed the properties	Car ownership on-site over management period	Rate of car ownership to number of dwellings
A	9	1.5 years	3 to 4 cars	0.33 - 0.44
B	55	2 years	20 to 24 cars	0.36 – 0.43
C	15	2 years	6 to 7 cars	0.4 – 0.47
D	8	3 years	2 to 3 cars	0.25 – 0.38
E	9	3 years	2 to 3 cars	0.22 -0.33
F	7	25 years	1 to 2 cars	0.14 – 0.3
Total	103			Max 0.47

11.4 Homes Tasmania - Tasmanian lived experience

A new social housing development at 73A New Town Road in southern Tasmania. Mr Richard Gilmour of Homes Tasmania provided a letter in support of the development application for 22 social housing dwellings, on the basis that survey data of actual car parking demand of similar social housing developments, generate a maximum car parking rate of 0.55 vehicles per dwellings.

The traffic surveys were conducted by a qualified traffic engineer for Homes Tasmania and examined the actual parking demand from social housing developments, which had been approved with a reduction in parking spaces under the performance criteria of the planning scheme. Table 11.4 summarises this survey data.

This survey data revealed that the actual parking demand is significantly lower than the predicted model used at the planning stage, generating an oversupply of car parking spaces.



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Table 11.4 – Summary of information from Community Tasmania

Site	Number of dwellings	Number of car parking spaces approved	Survey – number of cars parked	Maximum car parking usage	Car parking demand per dwelling
23-25 Goulburn St	25	15	8	50%	0.32 spaces per dwelling
216 Harrington St	53	53	24 to 29	65%	0.45 – 0.55
Total	78				Maximum 0.55

11.5 Parking demand data for Queens Walk social housing complex

Hubble Traffic was engaged to undertake an independent Traffic Impact Assessment for expanding the social housing complex at Queens Walk, located adjacent to the Brooker Highway, Cornelian Bay.

The current multi-storey complex contains three tower buildings containing 85 social housing apartments and the proposal is to increase this number to 150 apartments. In establishing the future car parking demand, a traffic survey was conducted on the car ownership of the current 85 apartments, which found the maximum car parking rate of 0.72 cars per apartment, which occurred at 6:00am, and includes visitor parking. This maximum car parking rate was used to predict the number of car parking spaces that would be provided for the development expansion and was accepted by the relevant planning authority.

11.6 Summary of the car parking demand

The above evidence demonstrates tenants of social housing dwellings have a lower car parking rate than private housing.

There is strong correlation between the mainland reference materials and the Tasmania lived experience of car parking ownership for tenants of social housing dwellings. Social housing development should be examined under these methodologies, with consideration taken on the sites location and proximity to a town centre, tenants accessibility to public transport services, and activity centres that contain a range of retail and commercial facilities.



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12. Appendix B – Traffic surveys

12.1 Manual traffic surveys

Table 12.1A – Morning survey completed on Wednesday 21st of May 2025

Time	Mount Street (North)			Atkins Drive (East)			Mount Street (South)			Three Mile Line Road (West)		
	Right	Straight	Left	Right	Straight	Left	Right	Straight	Left	Right	Straight	Left
7:30 7:45	8	13	5	5	6	0	0	20	11	5	2	11
7:45 8:00	9	17	21	7	11	4	6	31	21	6	6	14
8:00 8:15	8	13	24	25	18	8	13	32	32	9	25	17
8:15 8:30	14	16	27	46	15	7	9	58	29	12	23	19
8:30 8:45	21	22	15	26	16	5	3	53	27	17	14	17
8:45 9:00	18	22	2	8	8	0	3	40	16	19	5	17
Total	78	103	94	117	74	24	34	234	136	68	75	95
Peak	61	73	68	105	57	20	28	183	104	57	67	70

Table 12.1B – Evening survey completed on Tuesday 20th of May 2025

Time	Mount Street (North)			Atkins Drive (East)			Mount Street (South)			Three Mile Line Road (West)		
	Right	Straight	Left	Right	Straight	Left	Right	Straight	Left	Right	Straight	Left
4:00 4:15	13	50	10	7	5	0	0	33	10	19	8	16
4:15 4:30	21	43	11	9	4	1	1	29	10	17	6	16
4:30 4:45	29	47	14	11	3	2	4	37	7	18	6	34
4:45 5:00	19	47	4	7	2	3	2	33	9	14	4	17
5:00 5:15	29	64	15	0	2	1	3	31	17	13	17	24
5:15 5:30	16	61	8	5	3	1	1	28	10	23	2	15
Total	127	312	62	39	19	8	11	191	63	104	43	122
Peak	93	219	41	23	10	7	10	129	43	68	29	90



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12.2 Peak hour traffic movements

Diagram 12.2A – Morning peak hour traffic movements

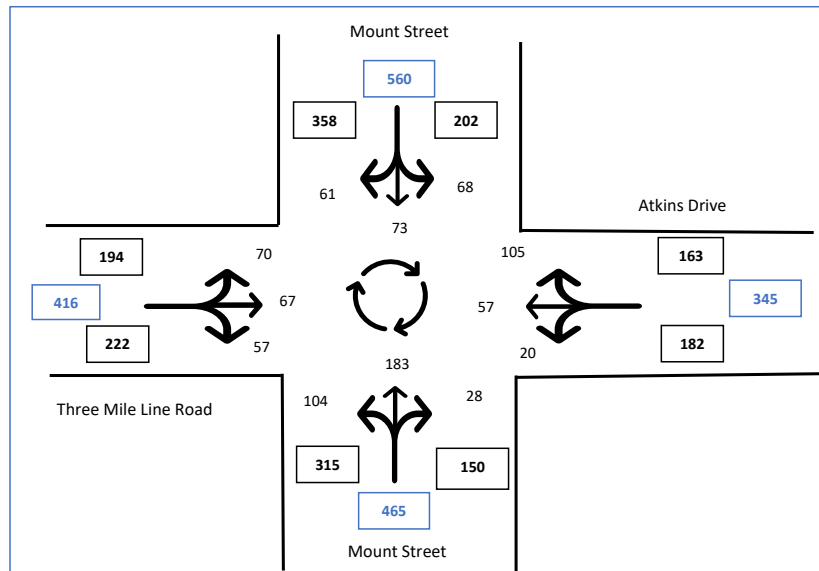
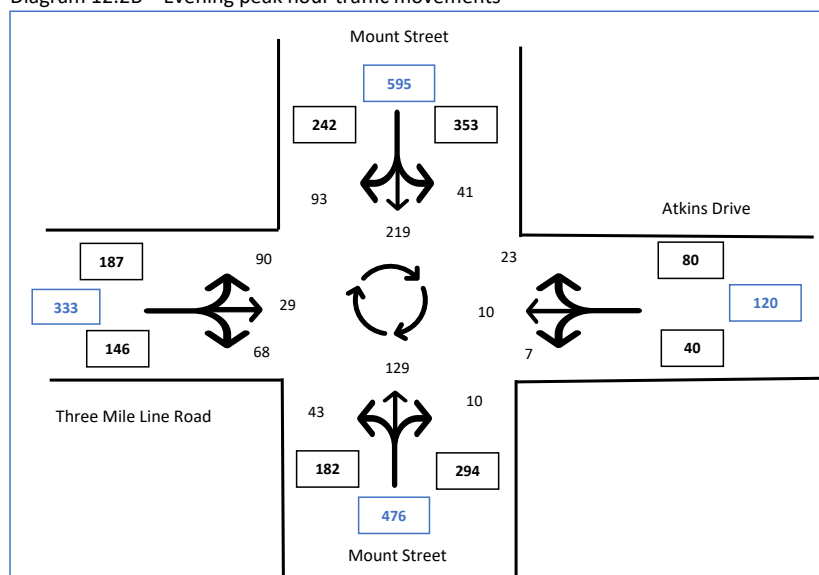


Diagram 12.2B – Evening peak hour traffic movements



Unit development 3 Atkins Drive, Romaine

13. Appendix C – Traffic modelling

Morning peak – Existing flows

MOVEMENT SUMMARY**Site: 101 [Mount - Three Mile Line - Atkins - Morning existing]**New Site
Site Category: (None)
Roundabout**Movement Performance - Vehicles**

Mov ID	Turn	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Mount Street								
1	L2	1	0.0	0.001	4.4	LOS A	0.0	0.0
2	T1	193	0.0	0.169	5.4	LOS A	1.0	6.7
3	R2	29	0.0	0.169	9.3	LOS A	1.0	6.7
Approach		223	0.0	0.169	5.9	LOS A	1.0	6.7
East: Atkins Street								
4	L2	21	0.0	0.170	5.2	LOS A	0.9	6.5
5	T1	60	0.0	0.170	5.5	LOS A	0.9	6.5
6	R2	111	0.0	0.170	9.5	LOS A	0.9	6.5
Approach		192	0.0	0.170	7.8	LOS A	0.9	6.5
North: Mount Street (Burnie)								
7	L2	72	0.0	0.182	5.0	LOS A	1.0	7.3
8	T1	77	0.0	0.182	5.3	LOS A	1.0	7.3
9	R2	64	0.0	0.182	9.2	LOS A	1.0	7.3
Approach		213	0.0	0.182	6.4	LOS A	1.0	7.3
West: Three Mile Line								
10	L2	74	0.0	0.202	6.0	LOS A	1.1	8.0
11	T1	71	0.0	0.202	6.3	LOS A	1.1	8.0
12	R2	60	0.0	0.202	10.3	LOS B	1.1	8.0
Approach		204	0.0	0.202	7.4	LOS A	1.1	8.0
All Vehicles		832	0.0	0.202	6.8	LOS A	1.1	8.0

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Evening peak – Existing flows

MOVEMENT SUMMARY
 **Site: 101 [Mount - Three Mile Line - Atkins - Evening existing]**

New Site
Site Category: (None)
Roundabout

Movement Performance - Vehicles

Mov ID	Turn	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Mount Street								
1	L2	1	0.0	0.001	4.4	LOS A	0.0	0.0
2	T1	136	0.0	0.104	4.9	LOS A	0.5	3.8
3	R2	11	0.0	0.104	8.9	LOS A	0.5	3.8
Approach		147	0.0	0.104	5.2	LOS A	0.5	3.8
East: Atkins Street								
4	L2	7	0.0	0.044	6.1	LOS A	0.2	1.6
5	T1	11	0.0	0.044	6.3	LOS A	0.2	1.6
6	R2	24	0.0	0.044	10.3	LOS B	0.2	1.6
Approach		42	0.0	0.044	8.6	LOS A	0.2	1.6
North: Mount Street (Burnie)								
7	L2	43	0.0	0.289	4.8	LOS A	1.8	12.8
8	T1	231	0.0	0.289	5.0	LOS A	1.8	12.8
9	R2	98	0.0	0.289	9.0	LOS A	1.8	12.8
Approach		372	0.0	0.289	6.1	LOS A	1.8	12.8
West: Three Mile Line								
10	L2	95	0.0	0.170	5.1	LOS A	0.9	6.5
11	T1	31	0.0	0.170	5.3	LOS A	0.9	6.5
12	R2	72	0.0	0.170	9.3	LOS A	0.9	6.5
Approach		197	0.0	0.170	6.6	LOS A	0.9	6.5
All Vehicles		758	0.0	0.289	6.2	LOS A	1.8	12.8



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Unit development 3 Atkins Drive, Romaine

Morning peak – Existing flows with development operating

MOVEMENT SUMMARY **Site: 101 [Mount - Three Mile Line - Atkins - Morning with development]**

New Site

Site Category: (None)

Roundabout

Movement Performance - Vehicles

Mov ID	Turn	Demand Flows Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Mount Street								
1	L2	1	0.0	0.001	4.5	LOS A	0.0	0.0
2	T1	193	0.0	0.170	5.4	LOS A	1.0	6.8
3	R2	29	0.0	0.170	9.4	LOS A	1.0	6.8
Approach		223	0.0	0.170	5.9	LOS A	1.0	6.8
East: Atkins Street								
4	L2	22	0.0	0.178	5.2	LOS A	1.0	6.8
5	T1	62	0.0	0.178	5.5	LOS A	1.0	6.8
6	R2	116	0.0	0.178	9.5	LOS A	1.0	6.8
Approach		200	0.0	0.178	7.8	LOS A	1.0	6.8
North: Mount Street (Burnie)								
7	L2	73	0.0	0.183	5.0	LOS A	1.1	7.4
8	T1	77	0.0	0.183	5.3	LOS A	1.1	7.4
9	R2	64	0.0	0.183	9.2	LOS A	1.1	7.4
Approach		214	0.0	0.183	6.4	LOS A	1.1	7.4
West: Three Mile Line								
10	L2	74	0.0	0.202	6.1	LOS A	1.1	8.0
11	T1	71	0.0	0.202	6.3	LOS A	1.1	8.0
12	R2	60	0.0	0.202	10.3	LOS B	1.1	8.0
Approach		204	0.0	0.202	7.4	LOS A	1.1	8.0
All Vehicles		841	0.0	0.202	6.8	LOS A	1.1	8.0

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Unit development 3 Atkins Drive, Romaine

Evening peak – Existing flows with development operating

MOVEMENT SUMMARY **Site: 101 [Mount - Three Mile Line - Atkins - Evening with development]**

New Site

Site Category: (None)

Roundabout

Movement Performance - Vehicles

Mov ID	Turn	Demand Flows Total veh/h	HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Mount Street								
1	L2	1	0.0	0.001	4.4	LOS A	0.0	0.0
2	T1	136	0.0	0.104	4.9	LOS A	0.5	3.8
3	R2	12	0.0	0.104	8.9	LOS A	0.5	3.8
Approach		148	0.0	0.104	5.2	LOS A	0.5	3.8
East: Atkins Street								
4	L2	7	0.0	0.044	6.1	LOS A	0.2	1.6
5	T1	11	0.0	0.044	6.3	LOS A	0.2	1.6
6	R2	24	0.0	0.044	10.3	LOS B	0.2	1.6
Approach		42	0.0	0.044	8.6	LOS A	0.2	1.6
North: Mount Street (Burnie)								
7	L2	47	0.0	0.294	4.8	LOS A	1.9	13.0
8	T1	231	0.0	0.294	5.1	LOS A	1.9	13.0
9	R2	98	0.0	0.294	9.0	LOS A	1.9	13.0
Approach		376	0.0	0.294	6.1	LOS A	1.9	13.0
West: Three Mile Line								
10	L2	95	0.0	0.173	5.1	LOS A	0.9	6.6
11	T1	34	0.0	0.173	5.3	LOS A	0.9	6.6
12	R2	72	0.0	0.173	9.3	LOS A	0.9	6.6
Approach		200	0.0	0.173	6.6	LOS A	0.9	6.6
All Vehicles		766	0.0	0.294	6.2	LOS A	1.9	13.0



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Unit development 3 Atkins Drive, Romaine

Morning peak – Existing flows with development operating and incremental growth at 1% per annum, for next 10 years

MOVEMENT SUMMARY

 **Site: 101 [Mount - Three Mile Line - Atkins - Morning, with development, traffic growth]**

New Site
Site Category: (None)
Roundabout

Movement Performance - Vehicles

Mov ID	Turn	Demand Flows Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Mount Street								
1	L2	1	0.0	0.001	4.5	LOS A	0.0	0.0
2	T1	213	0.0	0.191	5.5	LOS A	1.1	7.8
3	R2	33	0.0	0.191	9.5	LOS A	1.1	7.8
Approach		246	0.0	0.191	6.0	LOS A	1.1	7.8
East: Atkins Street								
4	L2	24	0.0	0.200	5.4	LOS A	1.1	7.9
5	T1	68	0.0	0.200	5.6	LOS A	1.1	7.9
6	R2	128	0.0	0.200	9.6	LOS A	1.1	7.9
Approach		221	0.0	0.200	7.9	LOS A	1.1	7.9
North: Mount Street (Burnie)								
7	L2	80	0.0	0.205	5.1	LOS A	1.2	8.5
8	T1	85	0.0	0.205	5.4	LOS A	1.2	8.5
9	R2	71	0.0	0.205	9.4	LOS A	1.2	8.5
Approach		236	0.0	0.205	6.5	LOS A	1.2	8.5
West: Three Mile Line								
10	L2	81	0.0	0.230	6.3	LOS A	1.3	9.3
11	T1	78	0.0	0.230	6.6	LOS A	1.3	9.3
12	R2	66	0.0	0.230	10.6	LOS B	1.3	9.3
Approach		225	0.0	0.230	7.7	LOS A	1.3	9.3
All Vehicles		928	0.0	0.230	7.0	LOS A	1.3	9.3



T: 0416 064 755
E: Hubbletraffic@outlook.com

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Unit development 3 Atkins Drive, Romaine

Evening peak – Existing flows with development operating and incremental growth at 1% per annum, for next 10 years

MOVEMENT SUMMARY

 **Site: 101 [Mount - Three Mile Line - Atkins - Evening with development, traffic growth]**

New Site
Site Category: (None)
Roundabout

Movement Performance - Vehicles								
Mov ID	Turn	Demand Total veh/h	Flows HV %	Deg. Satn v/c	Average Delay sec	Level of Service	95% Back of Queue Vehicles veh	Distance m
South: Mount Street								
1	L2	1	0.0	0.001	4.4	LOS A	0.0	0.0
2	T1	149	0.0	0.116	5.0	LOS A	0.6	4.4
3	R2	13	0.0	0.116	8.9	LOS A	0.6	4.4
Approach		163	0.0	0.116	5.3	LOS A	0.6	4.4
East: Atkins Street								
4	L2	8	0.0	0.052	6.4	LOS A	0.3	1.9
5	T1	12	0.0	0.052	6.6	LOS A	0.3	1.9
6	R2	28	0.0	0.052	10.6	LOS B	0.3	1.9
Approach		48	0.0	0.052	8.9	LOS A	0.3	1.9
North: Mount Street (Burnie)								
7	L2	53	0.0	0.328	4.9	LOS A	2.2	15.2
8	T1	255	0.0	0.328	5.2	LOS A	2.2	15.2
9	R2	108	0.0	0.328	9.1	LOS A	2.2	15.2
Approach		416	0.0	0.328	6.2	LOS A	2.2	15.2
West: Three Mile Line								
10	L2	104	0.0	0.194	5.2	LOS A	1.1	7.5
11	T1	37	0.0	0.194	5.4	LOS A	1.1	7.5
12	R2	79	0.0	0.194	9.4	LOS A	1.1	7.5
Approach		220	0.0	0.194	6.8	LOS A	1.1	7.5
All Vehicles		847	0.0	0.328	6.3	LOS A	2.2	15.2



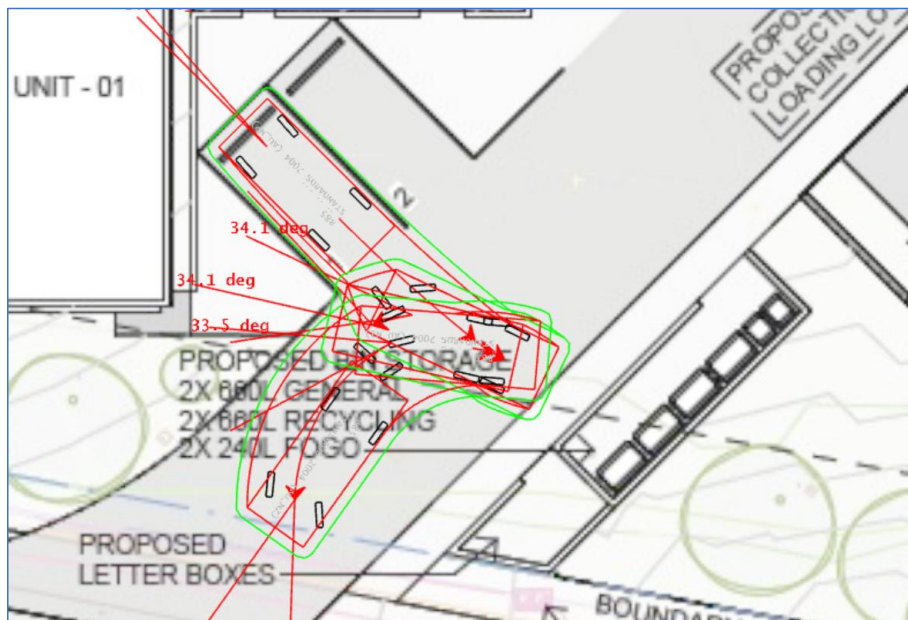
T: 0416 064 755
E: Hubbletraffic@outlook.com

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Unit development 3 Atkins Drive, Romaine

14. Appendix D – Vehicle swept paths

Swept path of B85 vehicle leaving parking space 1

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E: Hubbletraffic@outlook.com

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Unit development 3 Atkins Drive, Romaine

Swept path of B85 vehicle leaving parking space 14



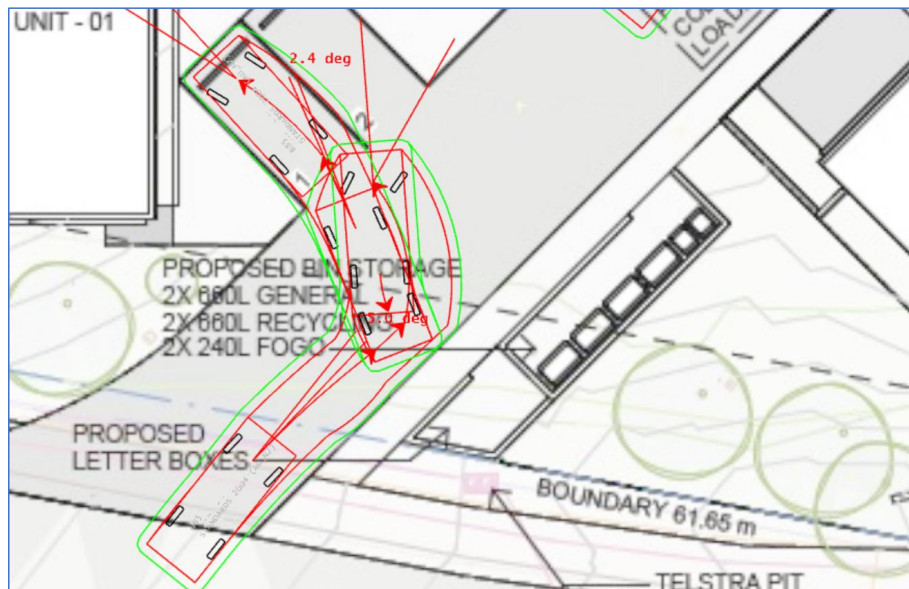
Unit development 3 Atkins Drive, Romaine

Swept path of B85 vehicle leaving parking space 9



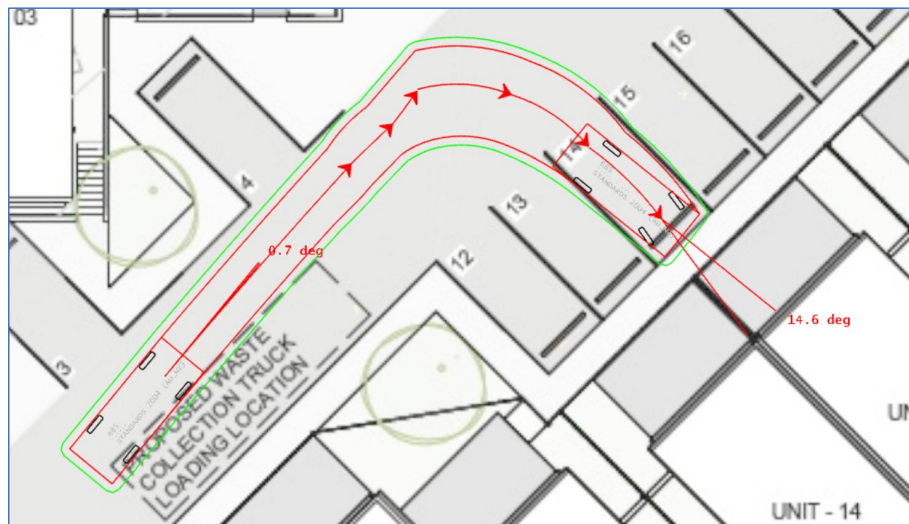
Unit development 3 Atkins Drive, Romaine

Swept path of B85 vehicle entering parking space 1



Unit development 3 Atkins Drive, Romaine

Swept path of B85 vehicle entering parking space 14



Swept path of B85 vehicle entering parking space 11





Hobart Office
40 Mole Street
Hobart TAS 7000, Australia

1300 657 402

www.adgce.com

14 August 2025

Burnie City Council
Attention: Troy McCarthy
80 Wilson Street
BURNIE, TAS 7320

Dear Troy,

Re: 3 ATKINS DRIVE, ROMAINE TAS
DA2025/38 – PERMIT APPLICATION FOR MULTIPLE DWELLING DEVELOPMENT AND
SUBDIVISION
RESPONSE TO INFORMATION REQUEST
28911.001 / L0001, HOBART

With reference to the Information Request received on 07/07/2025 as part of the Development Application, ADG is pleased to provide the following responses.

1. General Information

1.1. Please provide an amended concept servicing plan for water & sewer services which shows the following:

A. Indicative location of sewer main extensions required to service the development.

NOTE: One plan states you want to use some existing infrastructure, another shows a sewer main extension outside of the existing drainage easement. Please do the investigation now, to design the solution at this stage.

B. Indicative location of proposed TasWater easements in accordance with Sewerage Supplement.

ADG Response

Refer attached updated Preliminary Civil Services Concept Sketch and Detailed Site Survey which is provided as part of this response.

An updated survey was received on 07/08/2025. As shown on the Detailed Site Survey, the existing sewer main is a 150mm diameter PVC main traversing through the neighbouring property on 1 Atkins Drive, to the northwest of the development. The 150mm main extends into the site, providing a connection point for the development at approximately 1.7m below the surface.

The survey indicates that sections of the existing sewer line are located outside of the drainage easement; please refer to the attached survey plan for further detail. It is also noted that a section of sewer line could not be located due to horizontal alignment changes at bends.



ADELAIDE / BRISBANE / CANBERRA / DARWIN / GOLD COAST / HOBART / MELBOURNE / PERTH / SUNSHINE COAST / SYDNEY / TOOWOOMBA
ABN 63 131 876 143
CERTIFIED | QUALITY ASSURANCE ISO 9001:2015 | WORK HEALTH SAFETY ISO 45001:2018 | ENVIRONMENTAL MANAGEMENT ISO 14001:2015



Therefore, we would recommend Council, TasWater and the respective developers discuss further on how this easement and mains misalignment can be rectified in order for TasWater and Council to take ownership of the respective mains and maintain connection for the proposed development on 3 Atkins Drive.

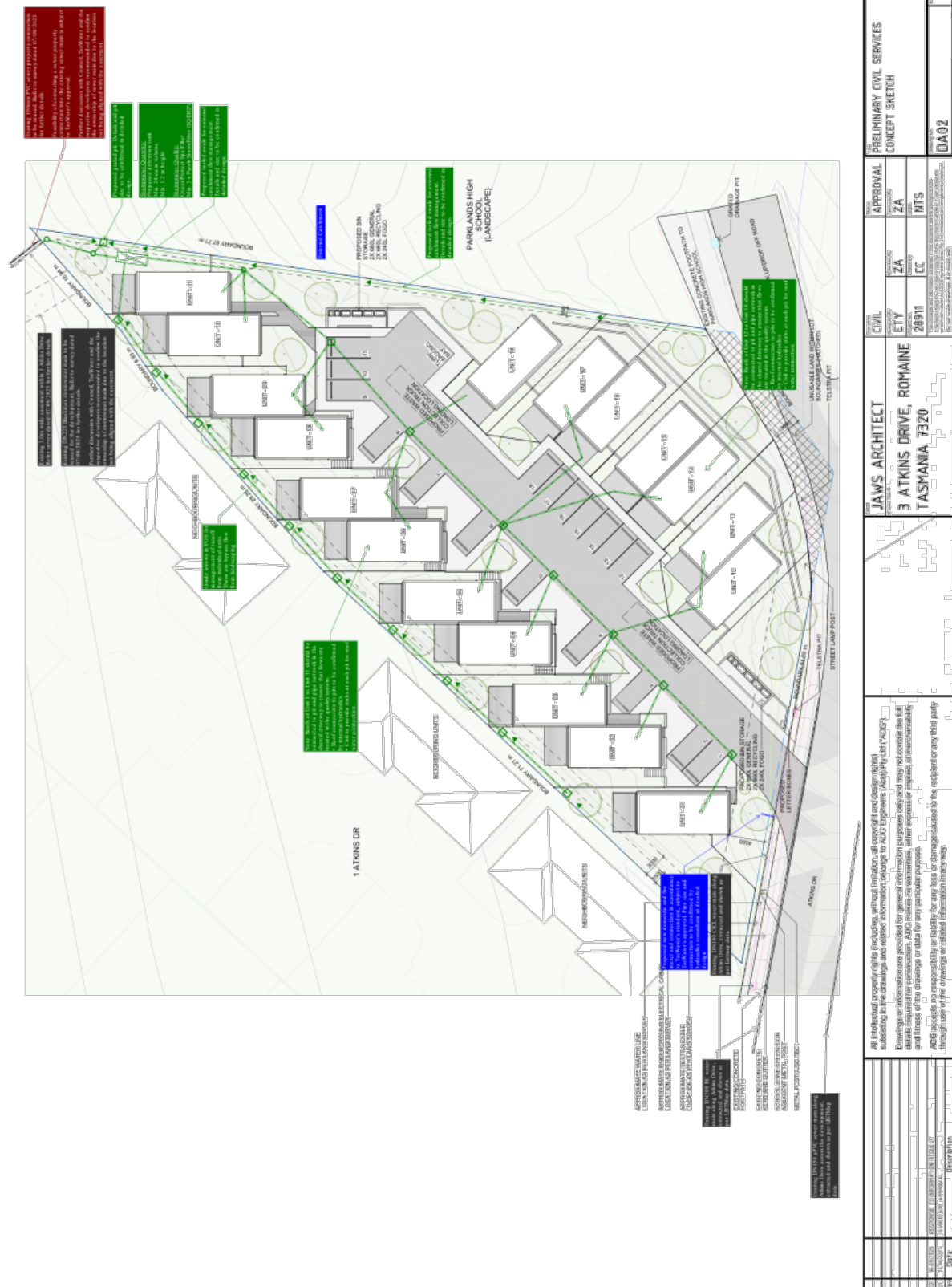
We trust that the above advice is satisfactory to the City to facilitate release of the Development Approval Decision Notice.

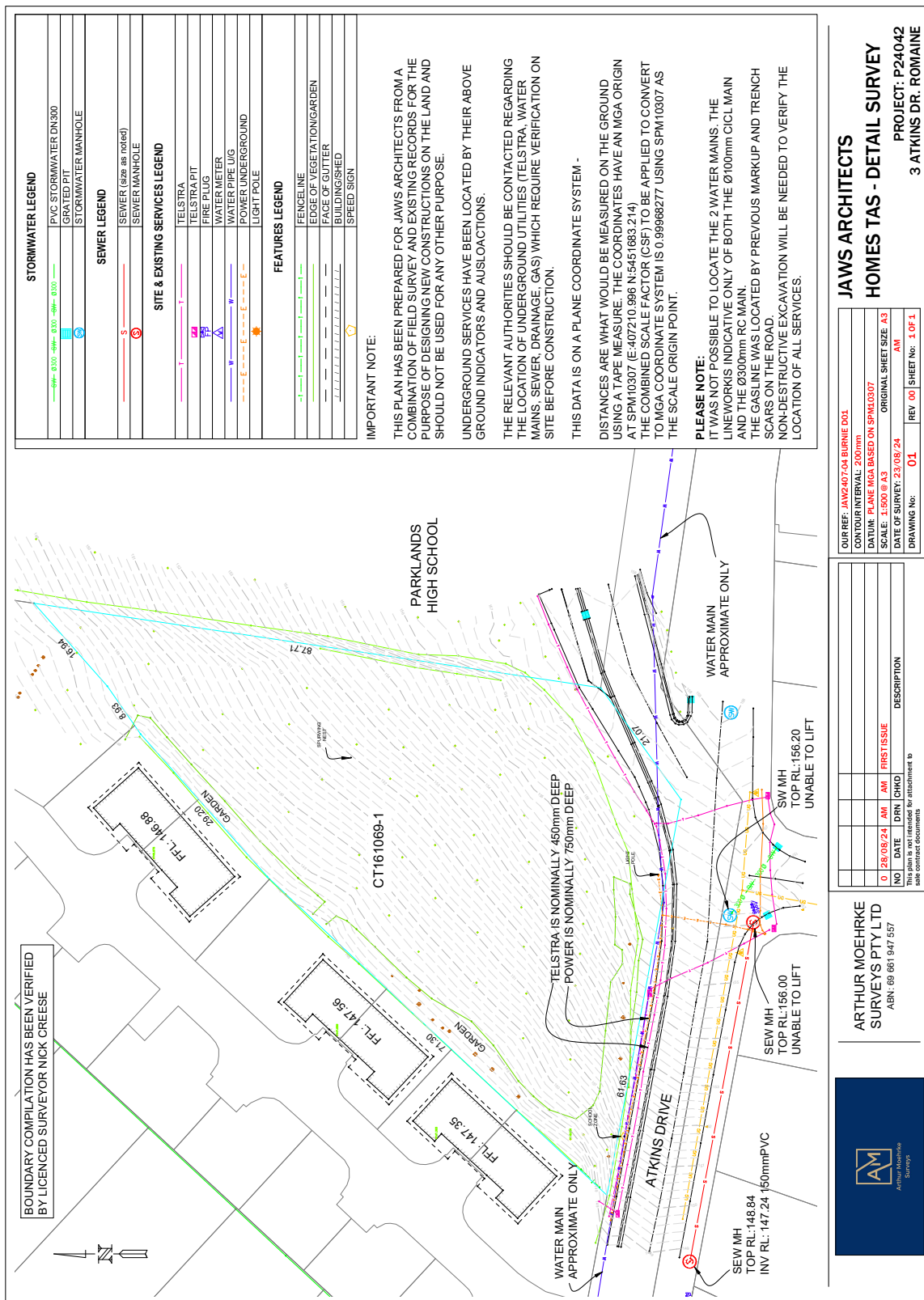
Should you require any additional information, please do not hesitate to contact us.

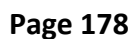
Kind regards,

A handwritten signature in black ink, appearing to read 'Darren Yuen'.

DARREN YUEN
SENIOR ENGINEER







PLEASE QUOTE

Your Ref:

Our Ref: 31/13/4; DA 2025/38; 25/22656
DD001.2025.00000038.001

Enquiries: Jon Randall (JR)

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320ABN: 29 846 979 690
Phone: (03) 6430 5700
Email: burnie@burnie.tas.gov.au
Web: www.burnie.tas.gov.auWe value your feedback on our service.
Tell us about it at burnie.tas.gov.au/feedback

2 September 2025

[REDACTED]
Ireneinc Planning and Urban Design
49 Tasman Street
NORTH HOBART TAS 7320

[REDACTED] [REDACTED] [REDACTED]
A hard copy will not be sent unless requested

Dear Mr Gartrell

COUNCIL ADVICE TO DEVELOPER
PROPOSED MULTIPLE DWELLING DEVELOPMENT – DA 2025/38
3 ATKINS DRIVE, ROMAINE

I write in relation to your recent application on 8 August 2025 seeking Statement of Compliance and the associated application (DA 2025/38) seeking grant of a permit under the *Land Use Planning and Approvals Act 1993* to carry out OR for the proposed development of a Multiple Dwellings on land at 3 Atkins Drive, Romaine.

This letter provides formal communication regarding the proposed use and development as follows:

- Part A of this letter contains written advice from Burnie City Council as a “Road Authority”, and contains the information required for assessment and determination of a permit application made under the *Land Use Planning and Approvals Act 1993* against relevant requirements of the Tasmanian Planning Scheme (TPS).

Council’s Land and Environmental Services Department has been provided with a copy of Part A to assist the Council in its role as a planning authority.

- Part B of this letter contains general information from Council, acting as a Road Authority under the *Local Government (Highways) Act 1982* and the *Roads and Jetties Act 1935*, and as a Road Manager under the *Heavy Vehicle National Law (Tasmania) Act 2013*.

It also addresses the Council’s requirements as a Stormwater Service Provider under the *Urban Drainage Act 2013*.

Part B is to inform you of matters outside of the development application process that may affect the use or development. Provision of this information at an early stage of the development process will enable informed decisions to progress the approvals process as smoothly as possible.

The information and advice in this letter is not exhaustive and does not cover matters relating to approvals that may be required from other authorities, such as the Water and Sewerage Authority (TasWater) and the Electricity Entity (TasNetworks), etc.

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PART A**“Road Authority” Advice:**

In reference to the relevant Tasmanian Planning Scheme Codes:

- **C3.0 Road and Railway Assets Code**

- C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction

We understand that you wish to rely on acceptable solution A1.2 to satisfy section C3.5.1 of the State Planning Provisions. The Road Authority therefore provides the following advice in this regard for access via Atkins Drive.

We note the submission of Traffic Impact Assessment prepared by Hubble Traffic dated June 2025 **attached**.

Access **can be provided** to the road network at 3 Atkins Drive to allow access to your proposed development, subject to the following:

- Access to the proposed development from 3 Atkins shall be located generally in accordance with the proposed driveway plan on page 20 of the Traffic Impact Assessment **attached**.
- The new crossover on Atkins Drive must be constructed in accordance with Tasmanian Standard Drawing TSD-R16-v3 **attached** for Heavy Vehicles and dowelled to the existing concrete (galvanised or fibreglass dowels). Driveway is to be a minimum of 3.6m wide. General arrangement of the driveway must be constructed in accordance with Tasmanian Standard Drawing TSD-R09-v3 **attached** to the satisfaction of Council and at the developer’s cost.
- Construction of new crossover must be in accordance with BCC Standard Drawing 7D13451 and associated Construction Note **attached**.
- New driveway to meet AS 2890 for sight distances. Plans shall be submitted to Council for approval prior to construction.
- Any recommendations from the TIA.

A ‘Permit to Conduct Work in a Council Street’ is required for any construction work within the road reserve. An application form is available on Council’s website <https://burnie.tas.gov.au/ROP>, together with the ‘General Terms and Conditions’ and ‘Requirements, Guidelines and Conditions of Constructing a Driveway to a Council Street’.

Once a permit has been approved, Council must be notified a minimum of 48 hours prior to pouring concrete and at the completion of pour, to undertake inspections and ensure all works conform to all relevant standards and specifications.

This Road Authority advice is valid for a period of 12 months from the date of this letter.

“Council” Advice:

Council are not aware of any matters that are pertinent to this development application process in relation to coastal inundation, coastal erosion, flood hazard or landslip.

The proposed development is within the urban drainage district and connections to the urban stormwater system can be provided. Detailed requirements under the *Urban Drainage Act 2013* for stormwater connection are set out in Part B.

- 3 -

PART B – GENERAL INFORMATION**Road Authority Information:**

We draw your attention to Burnie City Council Highways By-Law No. 3 of 2013, available on the Burnie City Council website <https://burnie.tas.gov.au/Council/By-laws>. Part 3 of this By-law is relevant to the proposed use and development.

Any works in the road reserve must be undertaken at the developer's expense and in accordance with *Infrastructure Assets – Standards Policy CP-CBS-SG-031* available on Council's website <https://burnie.tas.gov.au/Policies>, and the Tasmanian and Burnie City Council Standard Drawings, and to the satisfaction of Burnie City Council.

Stormwater Service Provider Information:

In accordance with Section 14 of the *Urban Drainage Act 2013*, permission must be obtained from the Stormwater Service Provider in order to connect new works to the public stormwater system.

This process does not form part of a permit application under the Tasmanian Planning Scheme.

However, it is recommended you consider the requirements of the Stormwater Service Provider prior to lodging a permit application, otherwise it is possible that changes required to satisfy drainage requirements may result in the need for an amendment to the development permit.

Council has reviewed the submitted drawings, and provides the following preliminary feedback in relation to the proposed stormwater discharge arrangements.

The development site currently has a connection to the stormwater network located near the north corner of the site. The new development may utilise this existing connection subject to the following:

- a) Due to downstream network limitations, the following requirements shall apply:
 - i. A Permissible Site Discharge (PSD) rate shall be calculated for the development site using the rational method and the following data:
 - Run-off coefficient 0.55.
 - Intensity 100mm/hr (or as calculated by a suitably qualified person based on the time of concentration of the entire development site and a 20% AEP event).
 - Area shall be the entire area of the site.
 - ii. The Design Site Discharge (DSD) rate shall be calculated based on the critical storm duration of the entire development catchment calculated by a suitably qualified person.
 - iii. Where the DSD is greater than the PSD the increase in site stormwater discharge shall be accommodated by providing on-site detention (OSD) to limit stormwater runoff to the satisfaction of Council.
 - iv. In calculating the DSD and any required OSD, the following design criteria are to be used:
 - For the purposes of calculating the DSD, a runoff coefficient of no less than 0.9 shall be used for impervious areas and a coefficient of no less than 0.4 shall be used for pervious areas unless otherwise approved by Council.
 - Roofs, driveways and carports, and other impervious hard standing areas shall be 100% impervious for drainage calculation purposes.
 - Driveways constructed with gravel or "grass-crete" or pervious pavers shall be 80% impervious for drainage calculation purposes.

- 4 -

- Courtyards and paths paved with pervious pavers and with a proper subsurface drainage system constructed underneath shall be 25% impervious for drainage calculation purposes.
 - Unroofed decks constructed with open timber decking shall also be considered 25% impervious.
 - The Site Storage Requirement (SSR) for detention systems shall be determined by calculating runoff volumes for the full range of site storm durations for the 10% AEP storm event to determine the maximum SSR using Boyd's formula, computer modelling, or other approved methods.
 - Calculations must be submitted to the Council for review.
- v. Where OSD is provided; the following three (3) main elements shall be included in the OSD system design:
- Temporary storage: Wherever reasonably practicable temporary storage is to consist of an open surface pond or an in-ground tank. Where these types of storage are not reasonably practicable, Council may consider allowing the use of above ground tanks. The storage must be designed to contain the excess volume of stormwater resulting from limits on the peak discharge flow rate.
 - Discharge control pit or weir: A flow control pit or weir with a litter and sediment removal component must be included in the OSD design. An outlet/orifice shall be a minimum internal diameter or width of at least 25mm and protected by an approved mesh screen. An overflow structure must be designed to cater for flows exceeding the capacity of the OSD. The overflow structure must direct excess flows in a manner to minimise any detrimental effects on downstream properties.
 - Maintenance Scheme: A maintenance schedule/plan for the OSD must be submitted to the Council with the OSD engineering design. The cleaning of below-ground storage facilities shall be conducted in accordance with the requirements and risk control measures specified in AS 2865-2009 Confined Spaces.

Neither this letter nor any land use permit issued constitutes permission to connect the development to the stormwater network. This permission needs to be arranged separately by contacting Council at stormwater@burnie.tas.gov.au or call (03) 6430 5770.

The following basic information is required to enable Council to assess the request for permission to connect to the stormwater network:

Information in relation to stormwater quantity and conveyance requirements:

- Locations and sizes of pipes, pits, spoon drains etc.
- Invert levels for pipes, manholes, drains and culverts.
- Location and relative level of proposed connection to the stormwater network.
- Details of any proposed detention system and associated calculations (as aforementioned).
- 1% AEP storm event drainage path locations, direction, widths, and flow depths indicated on plans, including any existing flow paths that cross or are in close proximity to the proposed development.

- 5 -

Should you have any further queries, please contact Council's Technical Officer, Jon Randall on (03) 6430 5736 or email: jrandall@burnie.tas.gov.au

Yours faithfully



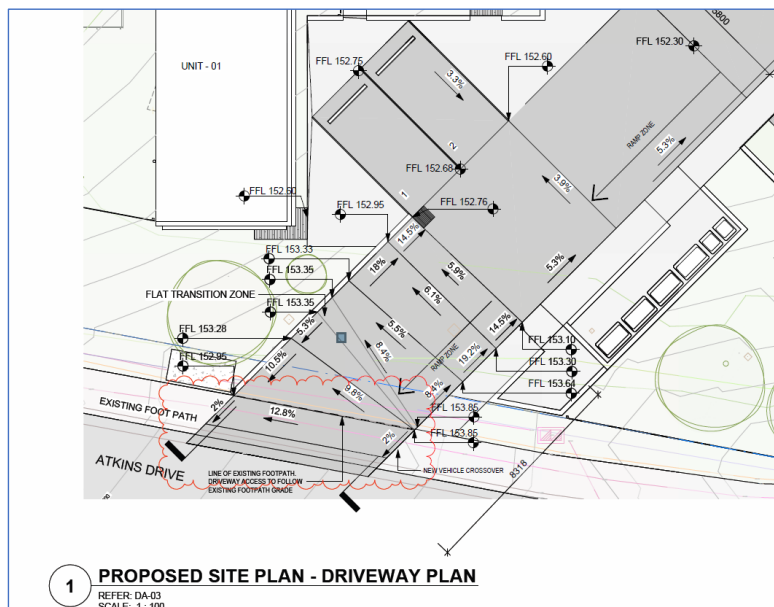
Damien Aherne
DIRECTOR OF OPERATIONS

Enc

Planning Officers, Land and Environmental Services – Burnie City Council
Jon Randall, Technical Officer – Burnie City Council

Unit development 3 Atkins Drive, Romaine

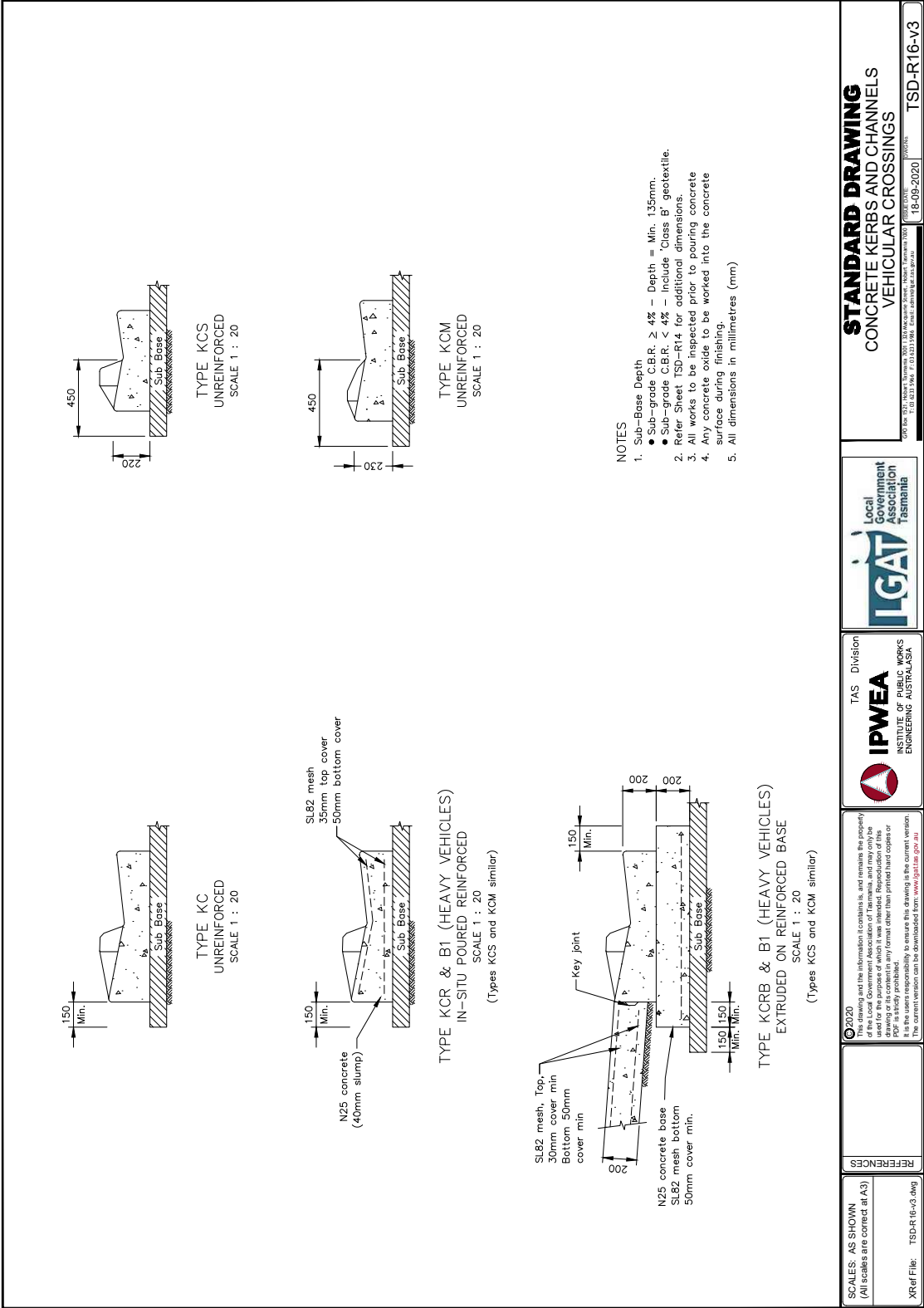
Diagram 8.1C – Proposed driveway vertical grades



This new access will be designed to comply with LGAT Standard Drawing TSD-R09-v3 for an urban driveway access, and with minor modification to the current footpath grades.

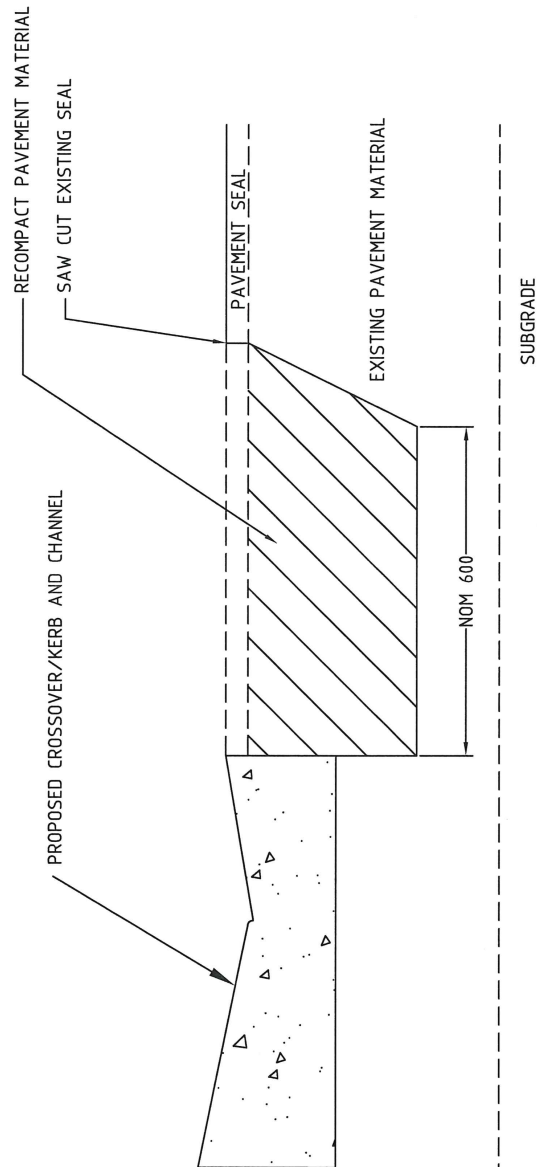
Photograph 8.1 – Location of new access onto Atkins Drive





CONSTRUCTION PROCEDURE

1. SAW CUT EACH END OF CROSSOVER/KERB AND CHANNEL AND REMOVE ALL THE EXISTING CONCRETE ELEMENTS
2. SAW CUT EXISTING SEAL AND REMOVE PAVEMENT MATERIAL AS REQUIRED. STOCKPILE FOR RE-USE
3. CONSTRUCT CROSSOVER/KERB AND CHANNEL IN ACCORDANCE WITH MUNICIPAL STANDARDS
4. NOTIFY COUNCIL FOR INSPECTIONS AS REQUIRED
5. REPLACE PAVEMENT MATERIAL AND COMPACT IN 100 MAXIMUM THICK LAYERS
6. REINSTATE SEAL



150mm FULL SCALE

ACAD FILENAME= 19-651-01.Dwg									
DRAWING No.									
CROSSOVER/KERB & CHANNEL STANDARD CONSTRUCTION DETAIL									
7D 13451									
A4									
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Submission to Planning Authority Notice

Application details

Council Planning Permit No.	DA 2025/38
Council notice date	2/07/2025
TasWater Reference No.	TWDA 2025/00741-BCC
Date of response	20/08/2025
TasWater Contact	Shaun Verdouw
Phone No.	0467 901 425

Response issued to

Council name	BURNIE COUNCIL
Contact details	planning@burnie.tas.gov.au

Development details

Address	3 ATKINS DR, ROMAINE
Property ID (PID)	3121705
Description of development	Multiple Dwellings x 18 + boundary adjustment

Schedule of drawings/documents

Prepared by	Drawing/document No.	Revision No.	Issue date
JAWS Architect	28911 – DA02	2	14/08/2025
	P24042 DA-01 to DA-11	A	13/06/2025

Conditions

Pursuant to the *Water and Sewerage Industry Act 2008* (TAS) Section 56P(1) TasWater imposes the following conditions on the permit for this application:

CONNECTIONS, METERING & BACKFLOW

1. A suitably sized water supply with metered connection(s) and sewerage system and connection to the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.
2. Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.
3. Prior to commencing construction of the development, any water connection utilised for construction must have a backflow prevention device and water meter installed, to the satisfaction of TasWater.

**INFRASTRUCTURE WORKS**

4. Prior to applying for a Certificate for Certifiable Works, the developer must physically locate all existing infrastructure to provide sufficient information for accurate design and physical works to be undertaken.
5. Plans submitted with the application for Certificate for Certifiable Work (Building and/or Plumbing) must, to the satisfaction of TasWater show, all existing, redundant and/or proposed property services and mains.
6. Prior to undertaking any works related to water and sewerage, physical markers must be in place that clearly identify where water and/or sewer connections are to be made in accordance with any approved plan to TasWater's satisfaction.

DEVELOPER CHARGES

7. Prior to TasWater issuing a Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$17,218.60 to TasWater for water infrastructure for 9.8 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.
8. Prior to TasWater issuing a Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$21,962.50 to TasWater for sewerage infrastructure for 12.5 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.
9. In the event Council approves a staging plan, prior to TasWater issuing a Certificate(s) for Certifiable Work (Building) and/or (Plumbing) for each stage, the developer must pay the developer charges commensurate with the number of Equivalent Tenements in each stage, as approved by Council.

DEVELOPMENT ASSESSMENT FEES

10. The applicant or landowner as the case may be, must pay a development assessment fee of \$802.53 to TasWater, as approved by the Economic Regulator and the fee will be indexed, until the date paid to TasWater.

The payment is required within 30 days of the issue of an invoice by TasWater.

Advice**General**

For information on TasWater development standards, please visit

<https://www.taswater.com.au/building-and-development/technical-standards>

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Important Notice Regarding Plumbing Plans and Associated Costs

The SPAN includes references to documents submitted as part of the application. These plans are acceptable for planning purposes only and are subject to further detailed assessment and review during the next stage of the development proposal.

TasWater's assessment staff will ensure that the design contains sufficient detail to assess compliance with relevant codes and regulations. Additionally, the plans must be clear enough for a TasWater contractor to carry out any water or sewerage-related work.



Depending on the nature of the project, your application may require Building and/or Plumbing permits or could be exempt from these requirements. Regardless, TasWater's assessment process and associated time are recoverable through an assessment fee. Please be aware that your consultant may need to make revisions to their documentation to ensure the details are fit for construction. Any costs associated with updating these plans should be discussed directly with your consultant.

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For information on Developer Charges please visit the following webpage – <https://www.taswater.com.au/building-and-development/developer-charges>

Water Submetering

As of July 1, 2022, TasWater's Sub-Metering Policy no longer permits TasWater sub-meters to be installed for new developments. Please ensure plans submitted with the application for Certificate(s) for Certifiable Work (Building and/or Plumbing) reflect this. For clarity, TasWater does not object to private sub-metering arrangements. Further information is available on our website (www.taswater.com.au) within our Sub-Metering Policy and Water Metering Guidelines.

Service Locations

Please note that the developer is responsible for arranging to locate the existing TasWater infrastructure and clearly showing it on the drawings. Existing TasWater infrastructure may be located by a surveyor and/or a private contractor engaged at the developers cost to locate the infrastructure.

- a. A permit is required to work within TasWater's easements or in the vicinity of its infrastructure. Further information can be obtained from TasWater.
- b. TasWater has listed a number of service providers who can provide asset detection and location services should you require it. Visit <https://www.taswater.com.au/building-and-development/service-locations> for a list of companies.
- c. Sewer drainage plans or Inspection Openings (IO) for residential properties are available from your local council.

Declaration

The drawings/documents and conditions stated above constitute TasWater's Submission to Planning Authority Notice.



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PLEASE QUOTE

Your Ref:

Our Ref: 31/13/4; DA 2025/38; 25/22656
DD001.2025.00000038.001

Enquiries: Jon Randall (JR)

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320ABN: 29 846 979 690
Phone: (03) 6430 5700
Email: burnie@burnie.tas.gov.au
Web: www.burnie.tas.gov.auWe value your feedback on our service.
Tell us about it at burnie.tas.gov.au/feedback

2 September 2025

Phil Gartrell
Ireneinc Planning and Urban Design
49 Tasman Street
NORTH HOBART TAS 7320Email: phil@ireneinc.com.au
A hard copy will not be sent unless requested

Dear Mr Gartrell

COUNCIL ADVICE TO DEVELOPER
PROPOSED MULTIPLE DWELLING DEVELOPMENT – DA 2025/38
3 ATKINS DRIVE, ROMAINE

I write in relation to your recent application on 8 August 2025 seeking Statement of Compliance and the associated application (DA 2025/38) seeking grant of a permit under the *Land Use Planning and Approvals Act 1993* to carry out OR for the proposed development of a Multiple Dwellings on land at 3 Atkins Drive, Romaine.

This letter provides formal communication regarding the proposed use and development as follows:

- Part A of this letter contains written advice from Burnie City Council as a “Road Authority”, and contains the information required for assessment and determination of a permit application made under the *Land Use Planning and Approvals Act 1993* against relevant requirements of the Tasmanian Planning Scheme (TPS).

Council’s Land and Environmental Services Department has been provided with a copy of Part A to assist the Council in its role as a planning authority.

- Part B of this letter contains general information from Council, acting as a Road Authority under the *Local Government (Highways) Act 1982* and the *Roads and Jetties Act 1935*, and as a Road Manager under the *Heavy Vehicle National Law (Tasmania) Act 2013*.

It also addresses the Council’s requirements as a Stormwater Service Provider under the *Urban Drainage Act 2013*.

Part B is to inform you of matters outside of the development application process that may affect the use or development. Provision of this information at an early stage of the development process will enable informed decisions to progress the approvals process as smoothly as possible.

The information and advice in this letter is not exhaustive and does not cover matters relating to approvals that may be required from other authorities, such as the Water and Sewerage Authority (TasWater) and the Electricity Entity (TasNetworks), etc.

- 2 -**PART A****“Road Authority” Advice:**

In reference to the relevant Tasmanian Planning Scheme Codes:

- **C3.0 Road and Railway Assets Code**

- C3.5.1 Traffic generation at a vehicle crossing, level crossing or new junction

We understand that you wish to rely on acceptable solution A1.2 to satisfy section C3.5.1 of the State Planning Provisions. The Road Authority therefore provides the following advice in this regard for access via Atkins Drive.

We note the submission of Traffic Impact Assessment prepared by Hubble Traffic dated June 2025 **attached**.

Access **can be provided** to the road network at 3 Atkins Drive to allow access to your proposed development, subject to the following:

- Access to the proposed development from 3 Atkins shall be located generally in accordance with the proposed driveway plan on page 20 of the Traffic Impact Assessment **attached**.
- The new crossover on Atkins Drive must be constructed in accordance with Tasmanian Standard Drawing TSD-R16-v3 **attached** for Heavy Vehicles and dowelled to the existing concrete (galvanised or fibreglass dowels). Driveway is to be a minimum of 3.6m wide. General arrangement of the driveway must be constructed in accordance with Tasmanian Standard Drawing TSD-R09-v3 **attached** to the satisfaction of Council and at the developer’s cost.
- Construction of new crossover must be in accordance with BCC Standard Drawing 7D13451 and associated Construction Note **attached**.
- New driveway to meet AS 2890 for sight distances. Plans shall be submitted to Council for approval prior to construction.
- Any recommendations from the TIA.

A ‘Permit to Conduct Work in a Council Street’ is required for any construction work within the road reserve. An application form is available on Council’s website <https://burnie.tas.gov.au/ROP>, together with the ‘General Terms and Conditions’ and ‘Requirements, Guidelines and Conditions of Constructing a Driveway to a Council Street’.

Once a permit has been approved, Council must be notified a minimum of 48 hours prior to pouring concrete and at the completion of pour, to undertake inspections and ensure all works conform to all relevant standards and specifications.

This Road Authority advice is valid for a period of 12 months from the date of this letter.

“Council” Advice:

Council are not aware of any matters that are pertinent to this development application process in relation to coastal inundation, coastal erosion, flood hazard or landslip.

The proposed development is within the urban drainage district and connections to the urban stormwater system can be provided. Detailed requirements under the *Urban Drainage Act 2013* for stormwater connection are set out in Part B.

- 3 -

PART B – GENERAL INFORMATION**Road Authority Information:**

We draw your attention to Burnie City Council Highways By-Law No. 3 of 2013, available on the Burnie City Council website <https://burnie.tas.gov.au/Council/By-laws>. Part 3 of this By-law is relevant to the proposed use and development.

Any works in the road reserve must be undertaken at the developer's expense and in accordance with *Infrastructure Assets – Standards Policy CP-CBS-SG-031* available on Council's website <https://burnie.tas.gov.au/Policies>, and the Tasmanian and Burnie City Council Standard Drawings, and to the satisfaction of Burnie City Council.

Stormwater Service Provider Information:

In accordance with Section 14 of the *Urban Drainage Act 2013*, permission must be obtained from the Stormwater Service Provider in order to connect new works to the public stormwater system.

This process does not form part of a permit application under the Tasmanian Planning Scheme.

However, it is recommended you consider the requirements of the Stormwater Service Provider prior to lodging a permit application, otherwise it is possible that changes required to satisfy drainage requirements may result in the need for an amendment to the development permit.

Council has reviewed the submitted drawings, and provides the following preliminary feedback in relation to the proposed stormwater discharge arrangements.

The development site currently has a connection to the stormwater network located near the north corner of the site. The new development may utilise this existing connection subject to the following:

- a) Due to downstream network limitations, the following requirements shall apply:
 - i. A Permissible Site Discharge (PSD) rate shall be calculated for the development site using the rational method and the following data:
 - Run-off coefficient 0.55.
 - Intensity 100mm/hr (or as calculated by a suitably qualified person based on the time of concentration of the entire development site and a 20% AEP event).
 - Area shall be the entire area of the site.
 - ii. The Design Site Discharge (DSD) rate shall be calculated based on the critical storm duration of the entire development catchment calculated by a suitably qualified person.
 - iii. Where the DSD is greater than the PSD the increase in site stormwater discharge shall be accommodated by providing on-site detention (OSD) to limit stormwater runoff to the satisfaction of Council.
 - iv. In calculating the DSD and any required OSD, the following design criteria are to be used:
 - For the purposes of calculating the DSD, a runoff coefficient of no less than 0.9 shall be used for impervious areas and a coefficient of no less than 0.4 shall be used for pervious areas unless otherwise approved by Council.
 - Roofs, driveways and carports, and other impervious hard standing areas shall be 100% impervious for drainage calculation purposes.
 - Driveways constructed with gravel or "grass-crete" or pervious pavers shall be 80% impervious for drainage calculation purposes.

- 4 -

- Courtyards and paths paved with pervious pavers and with a proper subsurface drainage system constructed underneath shall be 25% impervious for drainage calculation purposes.
 - Unroofed decks constructed with open timber decking shall also be considered 25% impervious.
 - The Site Storage Requirement (SSR) for detention systems shall be determined by calculating runoff volumes for the full range of site storm durations for the 10% AEP storm event to determine the maximum SSR using Boyd's formula, computer modelling, or other approved methods.
 - Calculations must be submitted to the Council for review.
- v. Where OSD is provided; the following three (3) main elements shall be included in the OSD system design:
- Temporary storage: Wherever reasonably practicable temporary storage is to consist of an open surface pond or an in-ground tank. Where these types of storage are not reasonably practicable, Council may consider allowing the use of above ground tanks. The storage must be designed to contain the excess volume of stormwater resulting from limits on the peak discharge flow rate.
 - Discharge control pit or weir: A flow control pit or weir with a litter and sediment removal component must be included in the OSD design. An outlet/orifice shall be a minimum internal diameter or width of at least 25mm and protected by an approved mesh screen. An overflow structure must be designed to cater for flows exceeding the capacity of the OSD. The overflow structure must direct excess flows in a manner to minimise any detrimental effects on downstream properties.
 - Maintenance Scheme: A maintenance schedule/plan for the OSD must be submitted to the Council with the OSD engineering design. The cleaning of below-ground storage facilities shall be conducted in accordance with the requirements and risk control measures specified in AS 2865-2009 Confined Spaces.

Neither this letter nor any land use permit issued constitutes permission to connect the development to the stormwater network. This permission needs to be arranged separately by contacting Council at stormwater@burnie.tas.gov.au or call (03) 6430 5770.

The following basic information is required to enable Council to assess the request for permission to connect to the stormwater network:

Information in relation to stormwater quantity and conveyance requirements:

- Locations and sizes of pipes, pits, spoon drains etc.
- Invert levels for pipes, manholes, drains and culverts.
- Location and relative level of proposed connection to the stormwater network.
- Details of any proposed detention system and associated calculations (as aforementioned).
- 1% AEP storm event drainage path locations, direction, widths, and flow depths indicated on plans, including any existing flow paths that cross or are in close proximity to the proposed development.

- 5 -

Should you have any further queries, please contact Council's Technical Officer, Jon Randall on (03) 6430 5736 or email: jrandall@burnie.tas.gov.au

Yours faithfully



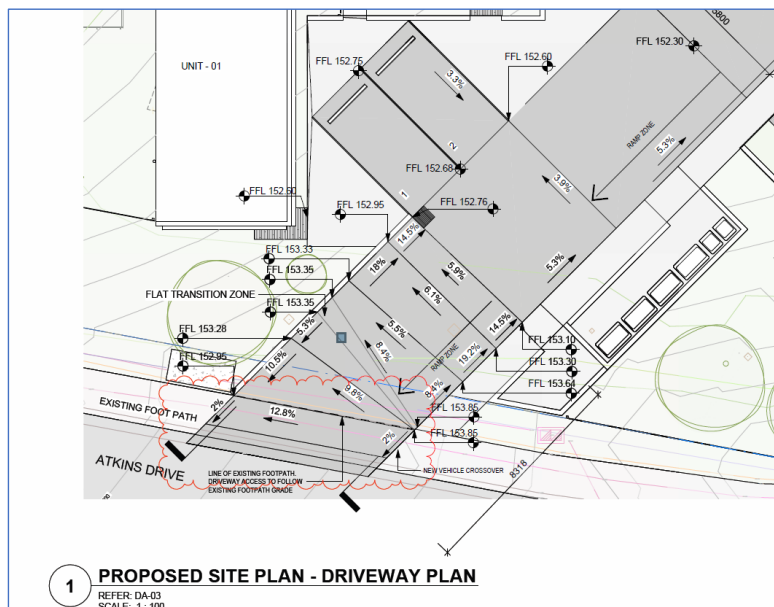
Damien Aherne
DIRECTOR OF OPERATIONS

Enc

Planning Officers, Land and Environmental Services – Burnie City Council
Jon Randall, Technical Officer – Burnie City Council

Unit development 3 Atkins Drive, Romaine

Diagram 8.1C – Proposed driveway vertical grades



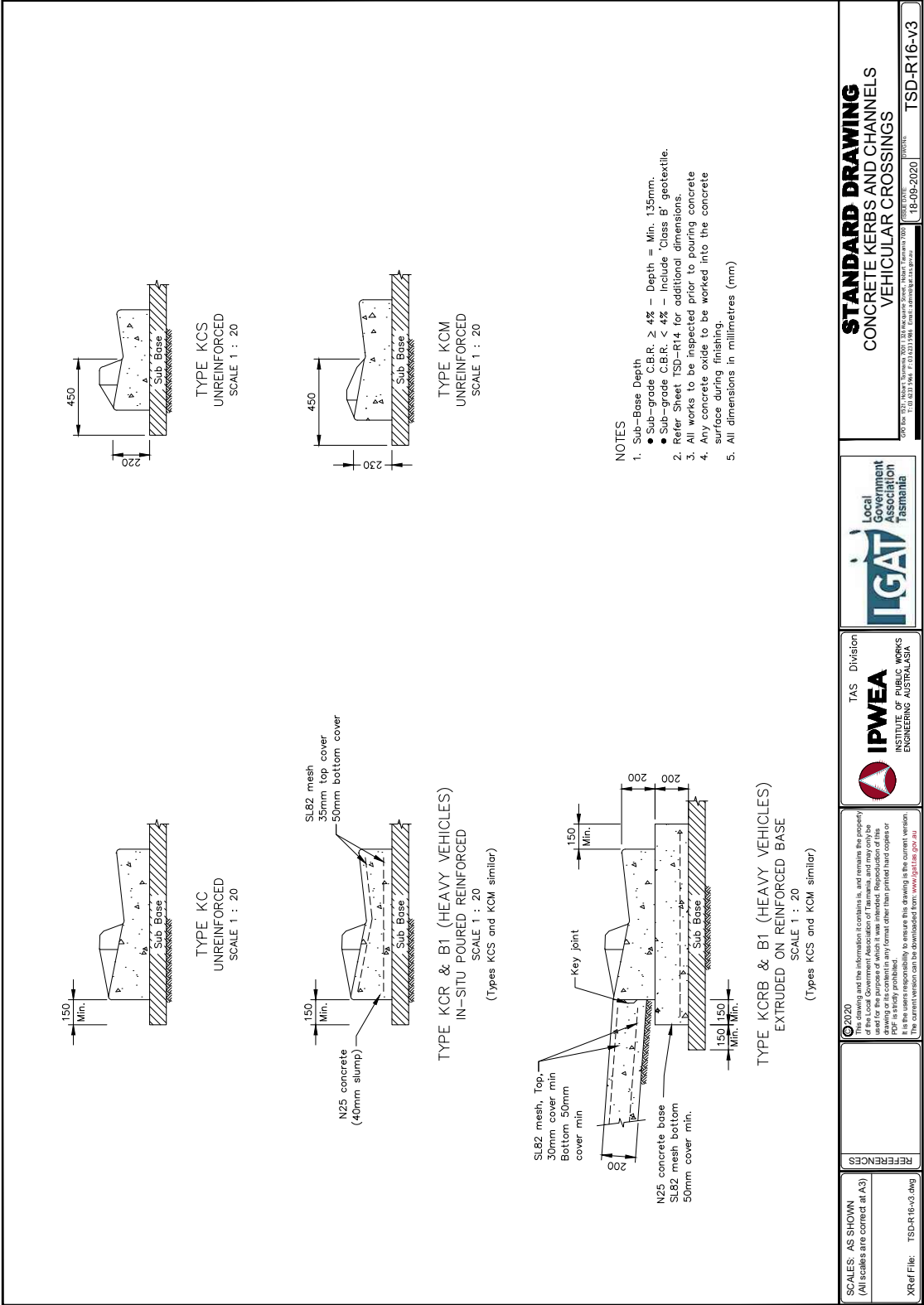
This new access will be designed to comply with LGAT Standard Drawing TSD-R09-v3 for an urban driveway access, and with minor modification to the current footpath grades.

Photograph 8.1 – Location of new access onto Atkins Drive



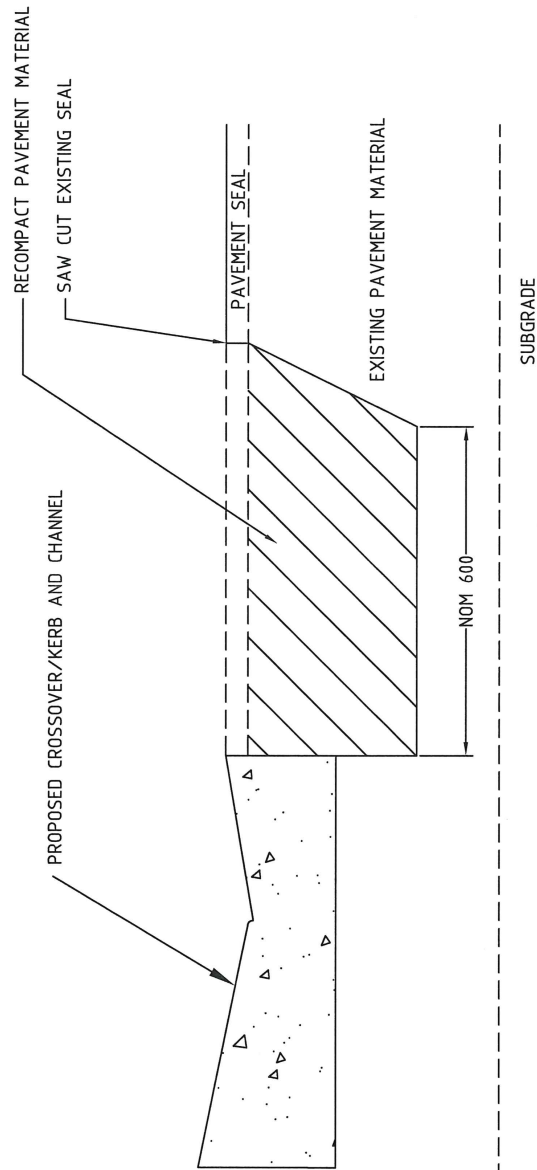
T: 0416 064 755
E: Hubbletraffic@outlook.com

20



CONSTRUCTION PROCEDURE

1. SAW CUT EACH END OF CROSSOVER/KERB AND CHANNEL AND REMOVE ALL THE EXISTING CONCRETE ELEMENTS
2. SAW CUT EXISTING SEAL AND REMOVE PAVEMENT MATERIAL AS REQUIRED. STOCKPILE FOR RE-USE
3. CONSTRUCT CROSSOVER/KERB AND CHANNEL IN ACCORDANCE WITH MUNICIPAL STANDARDS
4. NOTIFY COUNCIL FOR INSPECTIONS AS REQUIRED
5. REPLACE PAVEMENT MATERIAL AND COMPACT IN 100 MAXIMUM THICK LAYERS
6. REINSTATE SEAL



150mm FULL SCALE

[illegible]

General Manager,
Burnie City Council,



Subject: Development application DA 2025/38

Please be advised that we wish to lodge a formal objection to this project going through in its' current form.

We realise that the subject property is a prime development site. If a complex such as ours at 1 Atkins Drive were proposed, or a development aimed at seniors, we would have little cause to object. But what is proposed here is a low income high density project. This in an area containing many quality homes. It is understandable that affected homeowners will not welcome such a development because of quality of life concerns and possible negative impact on real estate values in the future.

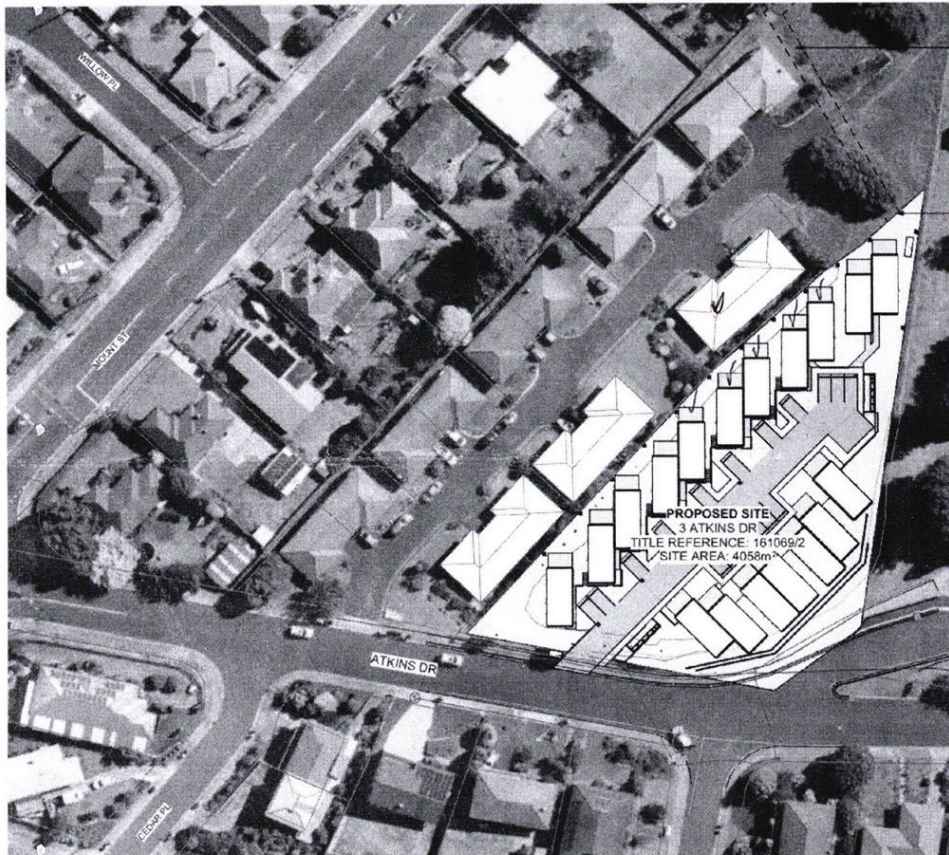
However, that is not the basis of our objection. Our complex will be adversely affected in a much more direct manner. The Developers have obviously chosen a two story development to maximise the number of units. In doing so, these new units will be relatively tiny. Those directly overlooking our complex have an interior size listed as 35 m2. By comparison our unit 18 at 1 Atkins drive, while also being a 2 bedroom unit, comes in at 58.86m2 (plus 24.48 m2 for the built in garage).

It seems inevitable that these new residents will seek to use their balconies whenever weather conditions allow, to gain some relief from their narrow confines. We have a compounding negative effect here. To begin with, this development site is already elevated. Going to two stories increases this, and all those balconies mentioned above will directly look down into our complex. This completely destroys the privacy we currently enjoy. The plans include studies done to alleviate concerns about shadows being cast. To us, that is a minor matter compared to the lack of privacy caused by these balconies. The need for a sewage pumping station for the new complex may also be a cause for concern, depending upon location and any resulting noise factor. It is also a reasonable concern that those using their balconies may also be inclined to own and use stereos, adding the noise factor to privacy concerns. These balconies also provide one of the few places for any children to play, further adding to potential noise.

Please consider the enclosed picture taken from the development application. This clearly shows 11 balconies directly overlooking our end unit. When we bought unit 18, we hoped it would be a long term investment, and planned to retire there once we sold our current property. Now those plans are in doubt. Sadly, if this proposed development goes ahead with its' current two story format and balcony arrangement, we will be forced to actively consider selling our unit and buying elsewhere.

William and Rosemary Harper

W.S. Harper
Rosemary Harper



Note that the balconies on those new units on the right face inwards to their parking areas. While those at the top directly overlook our complex, rather than their own parking spaces. Judging by this picture, all six units of our complex closest to the new development will have these balconies hanging directly over our back yards. There appears to be no setback whatever from the boundary line. Indeed, a case may be made that the left corner of these balconies infringe on our air space. Anything dropped from them would fall directly into our back yards. That's how close they seem to crowd us on that side.

Further, due to the added elevation of the proposed site, even those units on the other side of our complex will suffer the same lack of privacy.

**Subject: Formal Objection to Proposed Development at 1 & 3 Atkins Drive,
Romaine, TAS. DA 2025/38**

To General Manager, Burnie City Council, PO Box 973, Burnie 7320

17/09/2025

I am writing to express my strong objection to the proposed development at 1 & 3 Atkins Drive, Romaine, Tasmania CT:161069/2, 16223/0, as outlined in the recent planning application submitted by Homes Tasmania.

As the owner of an investment property in Romaine, I am deeply concerned about the detrimental impact this development will have on the surrounding area, its residents, and the long-term sustainability of our neighbourhood. My objections are based on the following grounds:

1. Traffic and Safety Concerns

The proposed development will dramatically increase traffic volume on Atkins Drive, a narrow road ill-equipped for high-density usage. This poses a serious risk to road safety, particularly for pedestrians and schoolchildren. The lack of adequate traffic management measures is unacceptable.

2. Environmental Impact

The site includes mature vegetation and borders sensitive ecological zones. The removal of trees and disruption of natural habitats will irreversibly damage local biodiversity, threatening native bird species and small mammals. This destruction is both short-sighted and irresponsible.

3. Neighbourhood Character

Romaine is a peaceful, low-density residential area with a unique character. The scale and design of the proposed building are completely out of step with the existing streetscape and will erode property values and community cohesion. This development is incompatible with the spirit of our neighbourhood.

4. Privacy and Amenity

The height and positioning of the proposed structure will directly overlook neighbouring properties, including mine, resulting in a significant loss of privacy and natural light. This will promote mould and negatively affect the health and wellbeing of long-term tenants who cherish the tranquil setting.

5. Infrastructure Strain

Local infrastructure is already under pressure. The development will overwhelm water supply, sewage systems, and waste management services, which are not designed to

support increased demand. This strain will degrade the quality of life for existing residents.

6. Property Devaluation and Income Loss

The proposed development will significantly devalue surrounding properties, including my own investment units. The loss of privacy, increased noise, and diminished neighbourhood appeal will make these properties less attractive to tenants, resulting in reduced rental income and long-term financial harm to owners.

I urge the planning authority to reject this proposal and conduct a comprehensive review of its far-reaching consequences. Community consultation must be prioritized to ensure that the voices of local residents and property owners are not ignored.

Thank you for your attention to this matter.

Sincerely,

Vince Tamburo

2905 Drouin-Korumburra Road, Poowong East, VIC, 3988

Owner of Units 6, 17, 19, 20, 21/1 Atkins Drive in Romaine, TAS

████████████████████
████████████████████

From: [burnie](#)
To: [Planning](#)
Subject: FW: Representation – DA 2025/38 (1–3 Atkins Drive, Romaine)
Date: Monday, 22 September 2025 1:22:31 PM

From: Albert Trinh [REDACTED]
Sent: Monday, 22 September 2025 1:12 PM
To: burnie <burnie@burnie.tas.gov.au>
Subject: Re: Representation – DA 2025/38 (1–3 Atkins Drive, Romaine)



Good afternoon.

Just to edit my representation as I have gotten feedback from TasWater regarding the sewer connection this morning. Please see struck out and highlighted section which I wish to remove from my representation.

Regards,

Albert Trinh
Principal
[Trinh Capital](#) | 320 Pitt St, Sydney NSW 2000

From: Albert Trinh [REDACTED]
Date: Sunday, 21 September 2025 at 15:23
To: burnie <burnie@burnie.tas.gov.au>
Subject: Representation – DA 2025/38 (1–3 Atkins Drive, Romaine)

Dear Burnie Council,

I am the adjoining owner of 1 Atkins Drive, Romaine, and write in relation to the current application (DA 2025/38) for multiple dwellings and subdivision at 1–3 Atkins Drive.

Support for the Development

I wish to express my support for this application. I believe it will be a strong addition to the Romaine area, providing much-needed housing and helping create a hub for new residential development, including my own.

Sewer Asset Issue

[REDACTED]

My only concern relates to the existing TasWater sewer line that traverses my property at 1 Atkins Drive. It appears the pipes were laid incorrectly many years ago or the easements were not properly aligned. This misplacement has created an ongoing conflict between the asset and the development potential of the land.

From my perspective, the most equitable outcome would be to have the pipe relocated back into the designated easement. Ideally, this would be undertaken by TasWater given the historic error, or otherwise in a pragmatic, cost-effective way that does not place an undue burden on either myself or the current applicant.

Conclusion

Subject to resolution of the sewer matter, I remain strongly supportive of the development and look forward to seeing it progress.

Regards,

Albert Trinh
Principal
[Trinh Capital](#) | 320 Pitt St, Sydney NSW 2000

Representation against Proposal for 3 Atkins Drive Romaine Burnie Application
number DA 2025/38

From: Frank and Julie Enkelaar

To: burnie@burnie.tas.gov.au

Date: Thursday 18 September 2025 at 02:32 pm AEST

RECEIVED

22 SEP 2025

BURNIE CITY COUNCIL

The General Manager Burnie City Council

Dear Sir,

In regard to this proposal we wish to make an objection against it. Our reasons for this objection is that on two previous occasions planning approval for single story units was denied on the basis that there would be too many units in the one area.

Therefore double story units that will impact negatively on the existing units seems totally unfair and inappropriate for this area.

Provision for excess parking, that is visitors or a household with more than one vehicle is virtually non existent.

This in our opinion will impact negatively on the flow of traffic in this area, because people are going to park in Atkins Drive as they already do quite frequently, and park half on the footpath. It is quite a narrow road for School Buses and school traffic to have to contend with.

In anticipation of your response

Julie and Frank Enkelaar
8 Atkins Drive Romaine Burnie

Samara & Simon Lynd

6 Atkins drive

Brian & Merile Scott
1/2 ATKINS DRIVECarole A Long
Unit 22 1 Atkins DriveWERNER SCHYNS
10 ATKINS DRIVE

ROMAINE

Sonia O'Shannessy
711 ATKINS DRIVE

Emma McAdie & Brandon Fahey
269 Mount Street
Upper Burnie TAS 7320

19 September 2025

General Manager
Burnie City Council
PO Box 973
Burnie TAS 7320

Subject: Objection – DA 2025/38, 1 & 3 Atkins Drive, Romaine

Dear General Manager,

We, the adjoining landowners at 269 Mount Street, Upper Burnie, lodge a formal objection to Development Application DA 2025/38 for multiple dwellings and subdivision at 1 & 3 Atkins Drive, Romaine.

We do not support this application. Our concerns are as follows:

1. Retaining Wall & Land Stability

- Our property lies directly below the retaining wall that supports existing dwellings at 1 Atkins Drive.
- This wall already shows visible cracking.
- Adding 18 more dwellings at 3 Atkins will increase stormwater and soil pressure through infrastructure at 1 Atkins.
- This creates a serious and foreseeable safety risk.
- An independent geotechnical/structural assessment should be required before approval.

2. Stormwater Management

- The development relies on existing drainage and easements at 1 Atkins.
- There is no evidence these systems can handle the additional load.
- The planning scheme requires stormwater disposal not to adversely affect adjoining land.
- In our view, this standard is not met.

3. Building Envelope & Bulk

- Units 1–11 extend up to 0.9m outside the permitted building envelope.
- This breach increases building bulk along the boundary.
- It unreasonably impacts the visual amenity of adjoining properties.

4. Density & Overdevelopment

- The scheme requires 325m² of land per dwelling.
- This proposal provides only 225m² per dwelling.
- This represents clear overdevelopment and is inconsistent with the area's character.

5. Traffic, Parking & Safety

- No visitor parking is proposed despite 34 dwellings across 1 & 3 Atkins combined.
- This will cause overflow parking and congestion on Atkins Drive.
- The site is located beside Parklands High School's drop-off/pick-up zone.
- This area already experiences heavy traffic and pedestrian movement at peak times.
- Adding further vehicles here poses increased safety risks for students and residents.
- A proper traffic impact assessment should be required.

6. Process & Documentation

- The application bundles 1 and 3 Atkins together in a confusing way.
- It is misleading for adjoining owners.
- It fails to properly assess the cumulative impacts of both sites, particularly in relation to drainage, traffic, and amenity.

In summary:

This application breaches multiple planning standards, fails to protect the safety and amenity of adjoining landowners, and has not demonstrated compliance with the performance criteria in a satisfactory way. We strongly object to DA 2025/38 proceeding in its current form.

Please record our objection and notify us of Council's decision.

Kind regards,

Emma McAdie & Brandon Fahey
269 Mount Street
Upper Burnie TAS 7320

PLANNING AUTHORITY**AO173-25 TASMANIAN PLANNING SCHEME
PERMIT APPLICATION DA 2025/64 - 2 MORRIS STREET, COOEE
DWELLING ALTERATIONS, PARTIAL DEMOLITION AND ADDITIONS**

FILE NO: DD001.2025.00000064.001
PREVIOUS MIN:

1.0 RECOMMENDATION:

“THAT Council in its role as a planning authority under the Land Use Planning and Approvals Act 1993 determine in accordance with section 51(2)(c) and section 57(6) of the Land Use Planning and Approvals Act 1993 and State Planning Provision 6.8.1 to GRANT a permit to application DA 2025/64 for dwelling alterations, partial demolition and additions for a Single Dwelling on land described in CT 21960/3 and located at 2 Morris Street, COOEE.

Nature of Approval

- 1) *That the development is to proceed and be maintained generally in accordance with the descriptions, commitments and requirements contained in the following documents - copies of which are attached and endorsed to form part of this Permit:-*
 - a) *Supporting Statement, as prepared by Starbox Architecture, dated 26 June 2025;*
 - b) *Site Plan and Site Plan – Coverage, as prepared by Starbox Architecture, project no. 300125, drawing no, A01, revision DA7, dated 25 September 2025;*
 - c) *Demolition Plan, Ground Floor Plan and First Floor Plan, as prepared by Starbox Architecture, project no. 300125, drawing no, A03, revision DA7, dated 25 September 2025;*
 - d) *Elevations, as prepared by Starbox Architecture, project no. 300125, drawing no, A03, revision DA7, dated 22 September 2025; and*
 - e) *Building Envelope Perspectives, as prepared by Starbox Architecture, project no. 300125, drawing no, A04, revision DA7, dated 22 September 2025.*

Operation and Management Constraints

- 1) *A Little Penguin Management Plan is to be developed and submitted to the Planning Authority prior to issuance of a Certificate of Likely Compliance for*
-

building.

The Management Plan must include the timing of works, what to do if Little Penguins are found on the site and operation of certain activities on site during and after construction.

The applicant must liaise with the Conservation Branch within the Department of Natural Resources and Environment Tasmanian to establish if a permit is required if penguins are located on the site.

Notes:

1) It is the responsibility of the Permit holder to:-

- a. Identify the correct boundaries of the land and to ensure the structures will be located where approved;*
- b. Cover all costs associated with the provision and construction of access to the site and with the connection of utilities;*
- c. Take all reasonable measures to locate and protect any public utility installation within the land or in the vicinity of works, and to make good any damage which may occur;*
 - i. In this regard no use is to be made of a public road for the loading, unloading, storage or handling of goods and materials without the prior approval of the Burnie City Council;*
 - ii. The Permit holder must ensure reasonable measures are in place to avoid the tracking of mud and debris from the site onto a public road; and to immediately remove and clean up any mud or debris which may inadvertently be carried onto a road;*
 - iii. Have regard to the peaceful enjoyment and protection of occupation and use and development on adjacent land, and to program the nature and timing of work to minimise potential for disturbance, disruption or harm*
- d. Identify the need for and obtain all other permits or approvals which may be required by the law of Tasmania in relation to the conduct of the use and the carrying out of development on the land.*
- e. Notify the Planning Authority when the development is ready for occupation to enable conduct of an inspection to verify compliance to the terms and conditions of this Permit.*

2.0 EXECUTIVE SUMMARY

Purpose and Background

Application has been made to Council seeking grant of a permit under the *Land Use Planning and Approvals Act 1993* for alterations, partial demolition and additions to an existing Single Dwelling at 2 Morris Street, Cooee.

This report provides an assessment of the proposed development against the relevant requirements of the *Tasmanian Planning Scheme* taking into consideration the matters contained within four (4) representations received during the public exhibition period under section 57 of the Act.

Key Issues

The proposal relies on the following Performance Criteria for grant of a permit:-

- Clause 8.4.3 – Site coverage and private open space for all dwellings in relation to private open space (P2);
- Clause C7.6.1 – Buildings and works within a waterway and coastal protection area or a future coastal refugia area (P1.1) in relation to the northwestern portion of the site which is impacted by a waterway and coastal protection overlay.

SUPPORTING INFORMATION

Strategic Alignment

This report relates to the Council Plan Goal 2: Making liveable local communities; by ensuring Council remains compliant with its statutory and regulatory obligations and contributes to the regulatory environment which affects our community.

Legal

The Council is established as a planning authority by definition under section 3(1) of the Act and must enforce the planning scheme under section 48. The purpose of this report is to enable the planning authority to determine application DA 2025/29.

In accordance with section 57 of the Act and the Scheme, this proposal is an application for a discretionary permit. Council, as the planning authority, may approve or refuse discretionary permit applications after considering both Council's planning scheme and the public representations received.

The provisions of the Act require a planning authority to take all reasonable steps to ensure compliance with the planning scheme.

Council's assessment of this proposal should also consider the issues raised in any representations received, the outcomes of the State Policies and the objectives of Schedule 1 of the Act.

This report details the reasons for the officer recommendation. The planning authority must consider this report but is not bound to adopt the recommendation. Broadly, the planning authority can either:

- 1) adopt the recommendation, or
- 2) vary the recommendation by adding, modifying, or removing recommended reasons and conditions or replacing an approval with a refusal (or vice versa).

Any alternative decision requires a full statement of reasons to comply with the *Judicial Review Act 2000* and the *Local Government (Meeting Procedures) Regulations 2015*.

Finance

There are no financial impacts resulting from this report.

There is likelihood if a decision on a permit application is appealed, that the planning authority will be responsible for the costs of representation and appearance before the Tasmanian Civil and Administrative Tribunal.

Relation to Council Policy

There are no Council policies relating to this report.

Discussion

Refer to Report Detail and Attachments for further information.

Health and Safety

There are no health and safety impacts resulting from this report.

Risk

There is risk -

- a) The decision of a planning authority may be appealed if the applicant, or any person who has made a relevant representation during the exhibition period, is dissatisfied with the decision;
- b) The applicant or a third party may allege breach of procedural fairness in relation to the execution of one or more of the statutory processes applicable to assessment and determination of a permit application;

- c) The applicant will bring an action before the Tasmanian Civil and Administrative Tribunal (TASCAT) if the planning authority fails to make a decision within the relevant statutory timeframe, in which event the planning authority will be liable for the costs of all parties;
- d) A person may allege the planning authority has failed to exercise, or improperly exercised, a statutory power relating to assessment and determination of a permit application; or
- e) A person may allege a planning authority, or a person appointed or employed by a planning authority has engaged in improper conduct relating to assessment and determination of a permit application.

A planning authority may minimise risk by –

- a) Determining a permit application by reference only to the information provided with the permit application or in any representation received during the exhibition period;
- b) Determining compliance by reference only to the regulatory provisions and processes which are relevant to the use or development described in the permit application;
- c) By remaining impartial, observe all relevant processes, and not indicating any position on the application until the matter is presented for decision;
- d) Not accept or invite any inducement relating to a decision on a permit application.

Environmental Sustainability

There are no environmental sustainability impacts to be considered in this report apart from those specifically outlined in any relevant provisions of the Scheme.

Consultation

Relevant Council Officers have been consulted in the preparation of this report.

Conflict Of Interest

No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

DETAIL

The application seeks a permit to undertake extensive renovations, alterations and additions and partial demolition to an existing Single Dwelling at 2 Morris Street, Cooe.

The site is zoned General Residential under the *Tasmanian Planning Scheme* and is partially subject to a waterway and coastal protection overlay on its northwestern side.

The site has a total area of 612m², is triangular in shape and located at the western end of Morris Street which terminates in a cul-de-sac adjacent to the site.

The site is located in a developed residential area consisting predominately of existing single dwellings built in the 1940's with one set of multiple dwellings built in the 1980's.

The site fronts onto Crown land to the northwest of the site which contains the coastal pathway.



Figure 1 – Aerial image of the property (Source: The List Maps – accessed 7/8/2025)

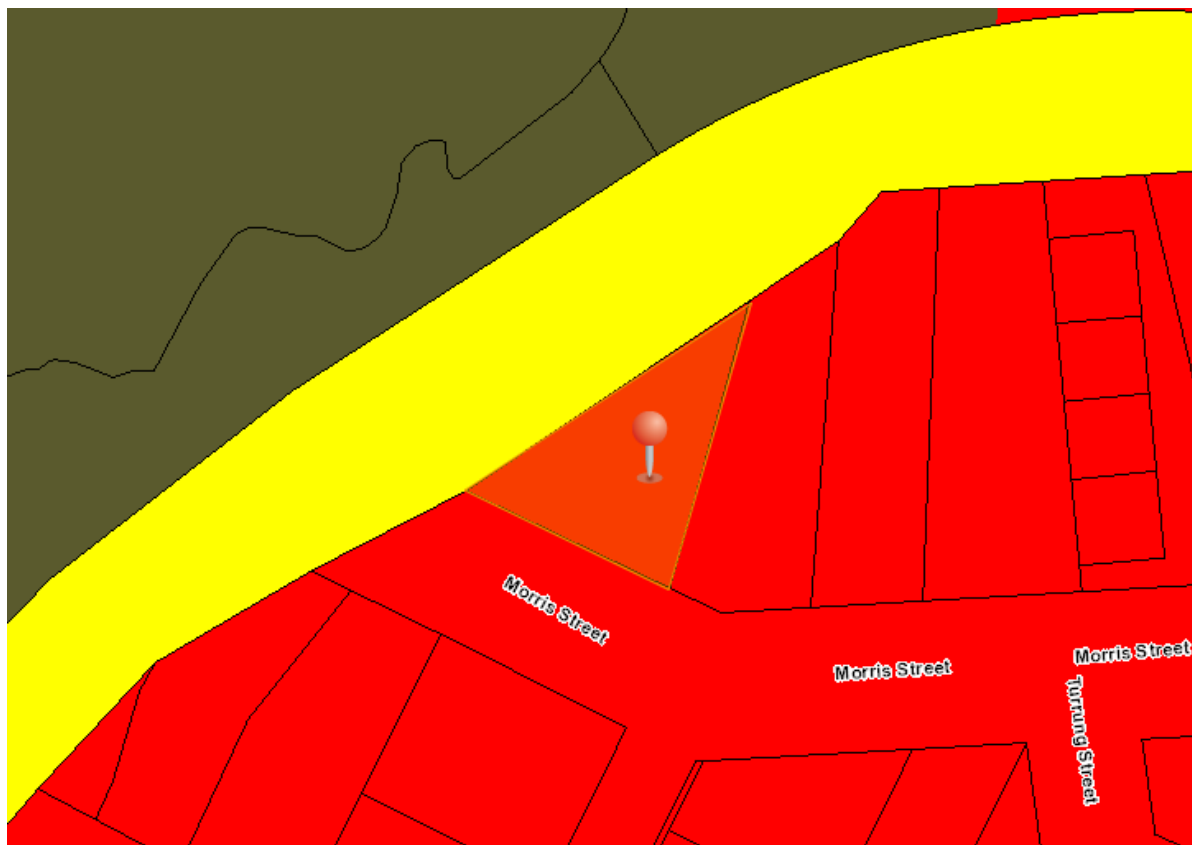


Figure 2 – Zoning map (Source: The List Maps – accessed 7/8/2025)

■ General Residential ■ Utilities ■ Environmental Management

The proposal involves the addition of a second story to the dwelling along with ground floor additions to the west of the existing dwelling which would include a new entry, single car garage, office and gym. Much of the existing dwelling will be demolished. The ground floor will retain the existing floor layout in terms of having three square shaped rooms on either side of an entry hallway with bathroom towards the rear.

The second floor will include a deck at the rear of the dwelling designed in accordance with the triangular shape of the lot.

The eastern elevation will feature no perforations on the second floor with minimal openings on the ground floor aside from bedroom highlight windows and glazed laundry door.

The southern elevation (fronting the street) will see a minimal look with a saw tooth type of façade featuring northeast facing windows.

The majority of windows within the dwelling will feature on the northwestern elevation (looking towards Bass Strait).

External finishes consist of Colorbond roofing in Surfmist colour and weatherboard wall cladding also in Surfmist.



Figure 3 – Shows the current single-story dwelling on the site (Source Google Street View)



Figure 4 – Shows the view from the Coastal Pathway to looking to the site

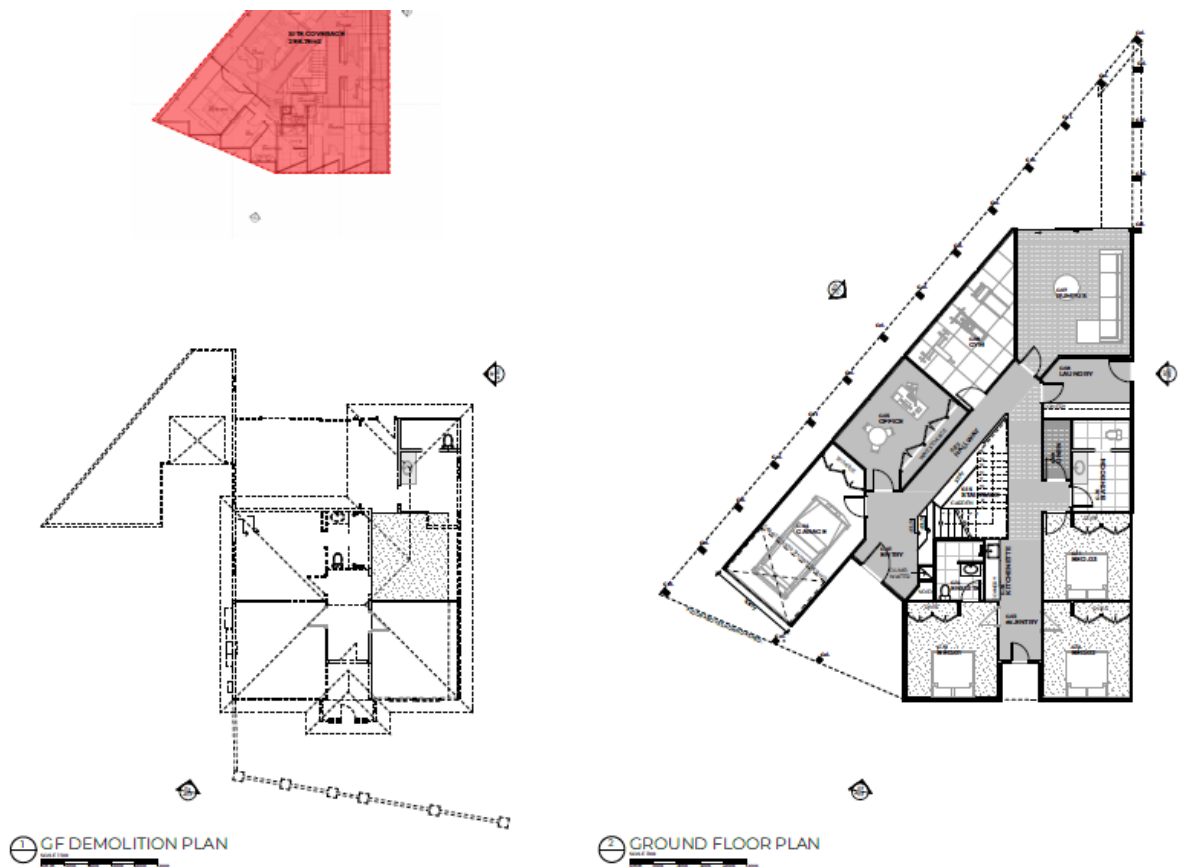


Figure 5 – Proposal plans showing demolition plan on the left and new ground floor plan on the right

Pursuant to clause 6.2.6 of the *Tasmanian Planning Scheme* the proposed development is categorised into ‘Residential’ use class as listed in Table 6.2 of the Scheme. A residential use (if for a Single Dwelling) within the General Residential zone is ‘no permit required’ provided it can meet all the applicable Acceptable Solutions.

In this instance, the application is discretionary as it cannot meet all Acceptable Solutions and relies on the following Performance Criteria for grant of a permit:-

- 1) Clause 8.4.3 – Site coverage and private open space for all dwellings in relation to private open space (P2); and
- 2) Clause C7.6.1 – Buildings and works within a waterway and coastal protection area or a future coastal refugia area (P1.1)

Assessment against these provisions is provided in the following tables.

Table 1 – Assessment of Clause 8.4.3 (P2) of the Scheme

8.4.3 Site coverage and private open space for all dwellings	
Objective:	That dwellings are compatible with the amenity and character of the area and provide: <ul style="list-style-type: none">(a) for outdoor recreation and the operational needs of the residents;(b) opportunities for the planting of gardens and landscaping; and

(c) private open space that is conveniently located and has access to sunlight.	
Acceptable Solutions	Performance Criteria
<p>A2</p> <p>A dwelling must have private open space that:</p> <ul style="list-style-type: none"> (a) is in one location and is not less than: <ul style="list-style-type: none"> i) 24m²; or ii) 12m², if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer); (b) has a minimum horizontal dimension of not less than: <ul style="list-style-type: none"> i) 4m; or ii) 2m, if the dwelling is a multiple dwelling with a finished floor level that is entirely more than 1.8m above the finished ground level (excluding a garage, carport or entry foyer); (c) is located between the dwelling and the frontage only if the frontage is orientated between 30 degrees west of true north and 30 degrees east of true north; and (d) has a gradient not steeper than 1 in 10. 	<p>P2</p> <p>A dwelling must have private open space that includes an area capable of serving as an extension of the dwelling for outdoor relaxation, dining, entertaining and children's play and is:</p> <ul style="list-style-type: none"> (a) conveniently located in relation to a living area of the dwelling; and (b) orientated to take advantage of sunlight.
<p>Assessment:</p> <p>P2</p> <p>The application cannot meet the Acceptable Solution as the area of private open space nominated on the site plan on the northern side of the dwelling is segmented by a proposed operable fence. This fence is positioned approximately 3.0m inwards of the title boundary.</p> <p>Therefore, the proposal must rely on assessment against Performance Criteria. The Performance Criteria states that a dwelling must have private open space that includes an area capable of serving as an extension of the dwelling for outdoor relaxation, dining, entertaining and children's play and is conveniently located in relation to a living area of the dwelling and orientated to take advantage of sunlight.</p> <p>The applicant has provided the following justification: -</p> <ul style="list-style-type: none"> a) <i>The private open space proposed is directly adjacent to the rumpus room on the ground floor of the dwelling. There is a large sliding door connecting the two spaces. When the sliding door opens the private open space is perceived as an extension of the rumpus room.</i> b) <i>The proposed private open space is located to the north and has ample amount of solar access. The allocated private open space has a operable fence running through it. This permeable and transparent fence is designed to be opened to enhance visual and sensory connections between the private open space and the surrounding</i> 	

coastline, beach, local vegetation and climate.

The nominated area of private open space is on the northern side of the site and therefore orientated to take advantage of sunlight.

The area of private open space is directly access via glass sliding doors from the rumpus room.

An upper floor open roofed deck with a floor area of approximately 19.5m2 is accessed from the kitchen, dining and living area.

A 3D building envelope prepared by the applicant and copied in figure 6 below shows the upper level open roofed deck area and the lower-level area nominated as private open space.

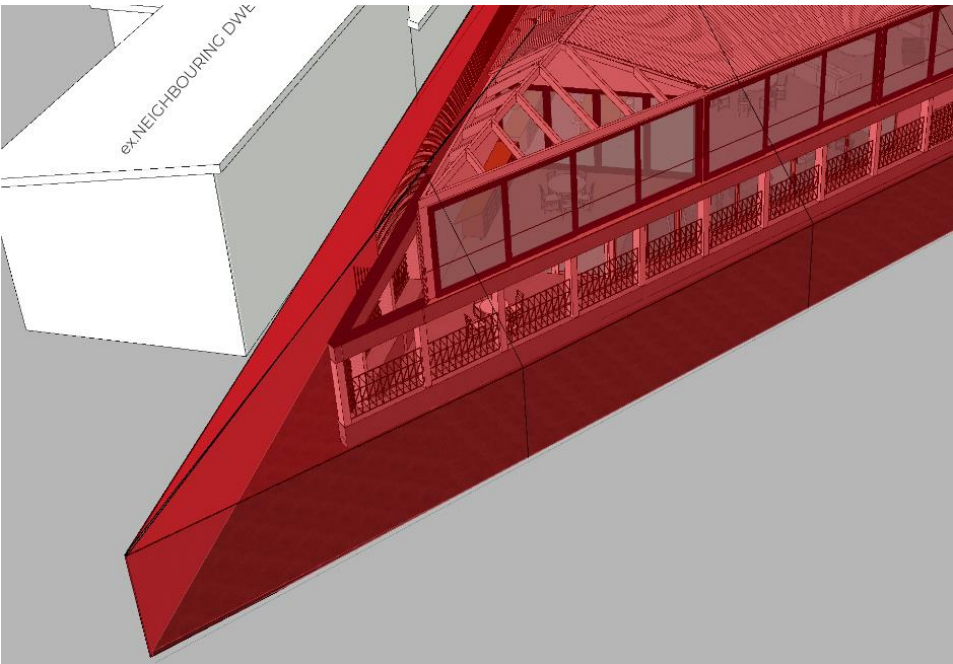


Figure 6 – Extract from the proposal plans as prepared by Starbox showing the building envelope diagram on the sites northern end which provides a view of the nominated area of private open space and the first floor balcony space.

It is considered that these areas are capable of serving as an extension of the dwelling for outdoor recreation, dining, entertaining and children’s play, conveniently located in relation to living area of the dwelling and orientated to the northern side of the site to take advantage of sunlight. The site plan indicates dedicated garden areas on the northwestern side of the dwelling and part of the northeastern side of the dwelling. New garden is also indicated at the front of the dwelling.

Compliance with the Performance Criteria is considered to be achieved.

Table 2 – Assessment against Clause C7.6.1 – Buildings and works within a waterway and coastal protection area or a future coastal refugia area (P1.1)

C7.6.1 – Buildings and works within a waterway and coastal protection area of a future costal refugia area	
Objective:	That buildings and works within a waterway and coastal protection area or future coastal refugia area will not have an unnecessary or unacceptable impact on natural assets.
Acceptable Solutions	Performance Criteria
A1	P1.1

<p>Buildings and works within a waterway and coastal protection area must:</p> <p>(a) be within a building area on a sealed plan approved under this planning scheme;</p> <p>(b) in relation to a Class 4 watercourse, be for a crossing or bridge not more than 5m in width; or</p> <p>(c) if within the spatial extent of tidal waters, be an extension to an existing boat ramp, car park, jetty, marina, marine farming shore facility or slipway that is not more than 20% of the area of the facility existing at the effective date.</p>	<p>Buildings and works within a waterway and coastal protection area must avoid or minimise adverse impacts on natural assets, having regard to:</p> <p>(a) impacts caused by erosion, siltation, sedimentation and runoff;</p> <p>(b) impacts on riparian or littoral vegetation;</p> <p>(c) maintaining natural streambank and streambed condition, where it exists;</p> <p>(d) impacts on in-stream natural habitat, such as fallen logs, bank overhangs, rocks and trailing vegetation;</p> <p>(e) the need to avoid significantly impeding natural flow and drainage;</p> <p>(f) the need to maintain fish passage, where known to exist;</p> <p>(g) the need to avoid land filling of wetlands;</p> <p>(h) the need to group new facilities with existing facilities, where reasonably practical;</p> <p>(i) minimising cut and fill;</p> <p>(j) building design that responds to the particular size, shape, contours or slope of the land;</p> <p>(k) minimising impacts on coastal processes, including sand movement and wave action;</p> <p>(l) minimising the need for future works for the protection of natural assets, infrastructure and property;</p> <p>(m) the environmental best practice guidelines in the <i>Wetlands and Waterways Works Manual</i>; and</p> <p>(n) the guidelines in the <i>Tasmanian Coastal Works Manual</i>.</p>
<p>Assessment: P1.1</p> <p>The proposal cannot meet the Acceptable Solution as there is no building area on a sealed plan approved under this planning scheme, there is no class 4 watercourse and the proposal is not within the spatial extent of tidal waters.</p> <p>Therefore, the proposal must rely on assessment against the Performance Criteria.</p> <p>The proposed development is within the boundaries of an existing urban site that has been utilised for residential purposes since at least the 1940's. The site contains an existing dwelling and residential landscaping. The performance criteria is addressed below:-</p> <p>a) During construction appropriate measures will be put in place to avoid impacts such as sedimentation and runoff. This would form a condition of any permit issued.</p> <p>b) There will be no impact to riparian vegetation as the location is not within the vicinity of a water source, wetland, lake or recharge basin. There will be no impact to littoral vegetation as works will occur within a fully urbanised and modified site. To the north west of the site is also a modified landscape containing the coastal pathway (former railway line).</p>	

- c) There are no watercourses that run through the property.
- d) There are no watercourses that run thorough the property.
- e) All stormwater will be drained to the public stormwater system. There are no known natural flow paths that run through the site.
- f) There are no watercourses that run through the property.
- g) The site is not within the vicinity of a wetland.
- h) The proposed development is within an existing residential site that has been used for residential purposes since at least the 1940's.
- i) There is minimal cut and fill required.
- j) The proposed building design responds to the shape of the site.
- k) There will be no impact on coastal processes including sand movement and wave action due to the proximity of the existing developed and urbanised site from the area where these coastal processes occur.
- l) The development is within the boundaries of an existing site utilised for residential purposes.
- m) Section 10 of the *Tasmanian Coastal Works Manual* relates to Wildlife and Pest Management which includes Little Penguins (*Eudyptula minor*). Little Penguins are present in coastal areas around much of Tasmania and are known to inhabit the coastal area around Coosue and are listed in Schedule 1 – Protected Wildlife of the *Natural Conservation (Wildlife) Regulations 2021*.

Little penguins are known to nest in coastal buildings and domestic gardens.

Part 10.2.4 of the manual identifies guidelines for working in little penguin habitat including managing the timing of works.

Works in areas containing Little Penguins are best undertaken outside of the breeding and moulting seasons.

As the site is within close proximity to an area of Little Penguin habitat, if a permit eventuates it would be recommended that a condition be applied to any permit that may be granted that a Little Penguin Management Plan is to be developed and submitted to the Planning Authority prior to issuance of a Certificate of Likely Compliance for building.

The Management Plan must include the timing of works, what to do if Little Penguins are found on the site and operation of certain activities on site during and after construction.

The applicant would also need to liaise with the Conservation Branch within the Department of Natural Resources and Environment Tasmania to establish if a permit is required if penguins are located on the site.

It is considered that the proposal, if conditioned accordingly, can achieve compliance with the Performance Criteria under C7.6.1 and will not have an unnecessary or unacceptable impact on natural assets.

Representations

Clause 6.10.1 of the Scheme requires a decision on a permit application must take into consideration all applicable standards and requirements of the planning scheme and any representations made during the public exhibition period, but only insofar as each such matter is relevant to the particular discretion being exercised.

In this instance, the application is also discretionary as it relies on Performance Criteria for grant of a permit including:-

- 1) Clause 8.4.3 – Site coverage and private open space for all dwellings in relation to private open space (P2); and
- 2) Clause C7.6.1 – Buildings and works within a waterway and coastal protection area or a future coastal refugia area (P1.1) in relation to the northwestern portion of the site which is impacted by the overlay.

There were four (4) representations received during the public exhibition period.

The representations and planning responses to the issues raised are provided below. While every effort has been made to include all issued raised, this summary should be read in conjunction with the representations which are included as an enclosure to this report.

Matter Raised	Comment
Outlook The natural outlook from the street, that is the entitlement for all Burnie residents to enjoy, will be lost.	The planning scheme allows for a dwelling to be built within the building envelope described in the Acceptable Solution to Clause 8.4.2. This allows for a building to be built up to a height of 8.5m. The proposal complies with the requirements of Acceptable Solution A3 of Clause 8.4.2 and is fully contained in the permitted building envelope.
Private Open Space The design of this house seeks to own the beach view, with the balcony barely off the fenceline overlooking the public land, used by hundreds of walkers and riders using the pathway each day. To further this domination of the public space, on the ground level, the proposal includes an open able fence into the public land. It is unclear how this provides privacy to the minimal private open space on ground level for the	There is nothing that states land must be fenced and if fenced there is nothing to state that that fence cannot be operable. The fence proposed is in between the support columns for the first floor of the dwelling and is located approximately 3.0m from the northwestern title boundary. The land occupied by the Coastal pathway is Crown Land, therefore, any direct access issues would need to be raised with Crown Land.

<p>applicant and how comfortable young families and older residents will feel being watched from this property.</p> <p>Object to the proposed use of the Shared Coastal Pathway for access, or adoption by the landowner of the segment of pathway and landscaped area adjoining their land.</p> <p>The fence should secure all of the Private Open Space for 2 Morris Street, not require the opening of the fence (not on the site boundary) or allow for movement of plant and equipment in through public land that is not a made road.</p>	<p>The first floor open roofed deck/balcony is approximately 3.0m off the northwestern side title boundary.</p> <p>The northwestern elevation has a fully operable full height window system with a glass balustrade provided along the northwestern frontage however no deck.</p> <p>The planning scheme does not state that an area of private open space is required to be enclosed with a solid fence.</p>
<p>Character of the area</p> <p>All along this side of Cooe residents are renovating and maintaining their homes and making the most of this beautiful location. This development, if Council approves it, will set the tone for an unfortunate change in the character of the area. This is something that as a long term resident, I do not support.</p> <p>This land is a focal point of Morris and Burnell Streets, where the residential area and the coastline meet. This means there needs to be clever design to link the natural and human world. Instead of a development that is sympathetic to these qualities, the applicant continues to propose a massive, bulky and imposing structure that respects neither the lowline coastal beauty of Cooe or the open, garden-based residential area.</p> <p>The proposal is too huge for this corner, presenting big walls of construction to the street, the pathway and the neighbouring house. It will be overwhelming in the extreme.</p>	<p>The proposed building is within the building envelope permitted under the Acceptable Solution of the planning scheme.</p> <p>It is noted that the site is of a triangular shape having a frontage (meaning a boundary of a lot which abuts a road) and two side boundaries.</p> <p>The planning scheme allows a permitted building envelope as per the diagram below to a height of 8.5m. The permitted setback from a side boundary is 1.5m.</p> <div data-bbox="810 1263 1295 1554" data-label="Diagram"> </div> <p><i>Figure 7 – Building envelope diagram – extract from the Tasmanian Planning Scheme</i></p> <p>The building envelope shown indicates a slope in line with the slope of the site.</p> <p>Diagrams submitted with the proposal demonstrate that based on the site contours the proposal fits within the permitted building envelope.</p> <p>Council cannot control the design of a building beyond the requirements of the planning scheme which</p>

	include (but not limited to) site coverage, building envelope and height, the width of garage openings within a façade, setbacks and frontage fences.
<p>Purpose and use of dwelling</p> <p>This house has been used for commercial purposes as an Airbnb rental since December 2023. Nothing in the supporting report indicates that the owners of the property intent to live in it and as my understanding that the size of this house and the size of their family do not align. Is this a mini-motel development by stealth, where we see a subsequent application for another Air B'n'B?</p> <p>The current building is not used as a residence but as short-term accommodation. It is believed the building will be used for these purposes after the additions and alterations which is essentially creating a two-storey hotel in a front-line coastal position, which is not within keeping of the current usage of the greater Coode area.</p>	<p>Council is not concerned with the size of the dwelling in terms of the suitability for its intended occupants. This is not something applicable to assessment under the <i>Tasmanian Planning Scheme</i>.</p> <p>The applicant has stated within the application form that the use class to which the development applies is residential.</p> <p>The application does not mention Visitor Accommodation.</p> <p>As such the proposal is assessed as residential.</p> <p>A change of use to any other use class within the planning scheme must be assessed in accordance with the requirements of the <i>Tasmanian Planning Scheme</i> and may require a new planning application.</p>
<p>Parking and Traffic</p> <p>With the four bedrooms as well as a gym and office proposed, it is unclear how the allocated single garage and two parking spots on a driveway is adequate to handle the needs of the property.</p> <p>If parking cannot be provided on the land itself, then this will lead to further congestion in the head of the cul de sac at the end of Morris Street, and increase the danger for vehicles moving through the area.</p> <p>It has already been my experience that the corner of Morris Street and Burnell Street attracts accidents and vehicle damage because of the parking in both streets.</p> <p>The new home is proposed to be significantly larger than the current house and I think it is reasonable to assume that with extra bedrooms and</p>	<p>The planning scheme requires for a 2 or more bedroom dwelling in the General Residential zone 2 parking spaces per dwelling of which the dwelling provides through an attached single car garage and tandem parking within the driveway which has the ability to provide for an additional two vehicles.</p> <p>The planning scheme states that tandem parking may be used to serve a dwelling.</p> <p>Therefore, the dwelling meets the car parking requirements set out within the planning scheme for a residential dwelling.</p> <p>Council cannot compel a development to provide a greater number of parking spaces.</p> <p>Council's Technical Services Department have not raised any issues regarding traffic flow or footpaths in the area.</p> <p>Clause 8.4.6 regulates shared parking onsite only. There is not shared spaces that forms part of this application (in the context in which it is defined). On-street parking cannot be considered as the proposal</p>

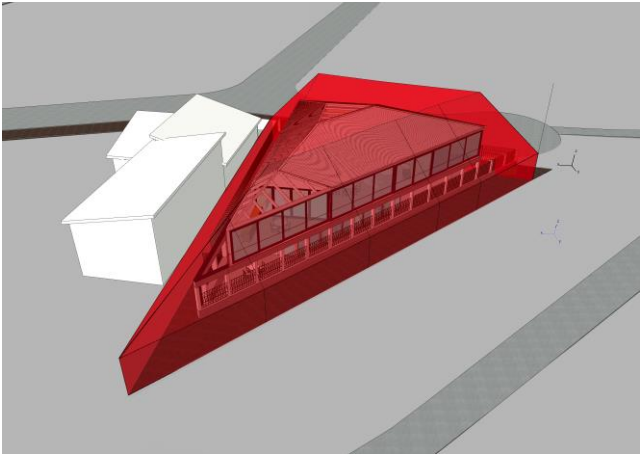
<p>space, extra people and cars can be expected to go to and from the property every day. Over the time I have lived here I have seen significant changes in the level of traffic in the area and this land funnels up Burnell Street past my home.</p> <p>Clause 8.4.6 P3 Shared driveway or parking spaces - The Morris St cul-de-sac is a popular parking area for people accessing Cooee beach and the coastal pathway, particularly in summer. Since starting operations as short-term accommodation, large vehicles which do not fit on the property have been parked on Morris St. Moving the location of the driveway at 2 Morris St and the increased traffic caused by the accommodation business will impact on parking in the cul-de-sac.</p>	<p>satisfies the permitted requirements of the Scheme (discussed above).</p>
<p>Drainage/Stormwater</p> <p>I do not know where the drainage from this project will go as this is not covered in the material on display. In the past I have had issues with water backing up from Morris Street into the intersection near my property.</p> <p>stormwater runoff – as the site is more than 80% impervious surfaces, rainwater will either end up in our property, the road or public land in greater volumes and increased velocity creating an off-site hazard and reducing the amenity of our property and risking the landscape work done by Council adjoining this site.</p>	<p>All stormwater would be directed to the existing stormwater connection. Council's Technical Services Department (as representatives of the Stormwater Authority) have reviewed the proposal and raised no concerns.</p>
<p>Overdevelopment of the site</p> <p>Being substantially oversized, compared to the surrounding area will not lead to improvements in our area. Instead, it will make it busier and less peaceful.</p> <p>The proposal still represents an overdevelopment of the site using all acceptable solutions to their maximum extent.</p>	<p>The proposal meets all Acceptable Solutions in relation to building envelope and site coverage.</p>
<p>The building filling the western boundary of the land it will shade the public pathway in the morning throughout the year and the 8.5m roof height, will be the</p>	<p>Shading of the public pathway is not relevant to the assessment of the proposal against the <i>Tasmanian Planning Scheme</i>.</p>

dominant feature on the landscape. We have other two storey homes along this stretch of the coast, but these are all well setback from their fencelines, respecting the privacy of the homeowners and giving the people walking and riding a sense of freedom.	
Amenity and Streetscape Maximising the cover of the site will create a significant negative impact on the amenity and streetscape	The proposal provides for a site coverage of 48.81% and therefore meets the Acceptable Solution within the planning scheme under Clause 8.4.3 (A1) for site coverage which requires a site coverage of not more than 50%.
Imposing on public space If this development fills the land at the end of Morris Street with a building that is four times the height of the current fence, it will impose itself on the precious public space (beach path and beach) and all those that love it. This is a very exposed and overpowering proposal and I question how it will encourage locals to continue their existing practices for exercise, mediation and relaxation. Code C7.0 Natural Assets Code. Respecting the visual landscape character of the area through low profile, non-dominant design – The proposed building will be two storeys with a long and imposing western façade which will dominate the landscape, in direct contravention to the code. The majority of dwellings in the area are single storey and of vastly smaller overall footprint, height and width. As the proposed dwelling is frontline coastal it (within 25m of the high tide level) the proposed design will dominate the natural landscape and set a precedent in the area.	The proposal meets the Acceptable Solution within Clause 8.4.2 of the <i>Tasmanian Planning Scheme</i> relating to setbacks, height and building envelope for all dwellings.
In my beach trips, I regularly clean up the washed in litter to make the area safer for the penguins, pademelons, water rats and other animals we share this space with. I fear that the construction of this property, further new fencing and no real yard will impact the land these animals rest and graze on.	This is something that cannot be accessed under the <i>Tasmanian Planning Scheme</i> .

<p>Code C7.0 Natural Assets Code Maintaining ecological function –</p> <p>The area of glazing on the western facing wall will increase from the current 1 window of approximately 1.0m x 1.2m to what appears to be 23 windows/glass panels of greater size. The total of the proposed glazed area will span two floors creating a large wall of glass which is a potential hazard to wildlife in two ways:</p> <ol style="list-style-type: none"> 1. The glazing and the direction it faces has the potential to cause bird strikes against the glass including migratory species of threatened and conservation significant birds which have been recorded within an 800m radius of the site (reference: Listmap). These include red capped plover (<i>Charadrius ruficapillus</i>) swift parrot (<i>Lathamus discolor</i>) and white throated needletail (<i>Hirundapus caudacutus</i>). 2). The western boundary fence lies within 20m of a little penguin (<i>Eudyptula minor</i>) colony and the much-increased light emissions in terms of both intensity and area have the potential to disturb and disorientate the penguin colony. <p>It is likely that the fence will impede penguin movement through this site (I have video evidence of penguins nesting on 2 Morris Street from the land in December 2023 and we regularly have penguins moving through our front courtyard and can provide video and photos to confirm this). Likewise, we have seen pademelons and water rats move through both properties. Excluding these creatures for the ability to forage suggests that the applicants hold no value for the coastal environment this site occupies.</p> <p>The minimal strip of 3m of landscaping along the western boundary has little prospect of either contributing to the ecology, or integrating the build form into the surround, if planted as shown. It will add to the existing planted area, but without significantly more detail on the</p>	<p>The proposal is reliant on assessment against the Performance Criteria under Clause C7.6.1 – Buildings and works within a waterway and coastal protection area or a future coastal refugia area (P1.1)</p> <p>A permit could be conditioned accordingly to ensure compliance with the Performance Criteria – see discussion in body of report under C7.6.1 (P1.1).</p> <p>As the site is within close proximity to an area of Little Penguin habitat, if a permit eventuates it would be recommended that a condition be applied to any permit that may be granted that a Little Penguin Management Plan is to be development and submitted to the Planning Authority prior to issuance of a Certificate of Likely Compliance for building.</p> <p>The Management Plan must include the timing of works, what to do if Little Penguins are found on the site and operation of certain activities on site during and after construction.</p> <p>The applicant would also need to liaise with the Conservation Branch within the Department of Natural Resources and Environment Tasmanian to establish if a permit is required if penguins are located on the site.</p> <p>The planning scheme does not regulate the building materials of a dwelling within the General Residential zone.</p> <p>The subject site and surrounds have been developed and urbanised since the 1940's. The land is zoned for residential use and can be developed within the parameters and opportunities available under the planning scheme.</p> <p>Requirements for sediment control and runoff during construction are not managed under the Tasmanian Planning Scheme and are managed under separate legislation.</p>
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<p>native plants proposed and mature trees (which we are confident won't be wanted by the applicant to obstruct the view or allowed by Taswater due to the risk to the sewer line) this is not believable.</p> <p>The construction is likely to encroach on the coastal zone to access part of the sites and the ultimate development seeks to usurp public space through the openable fence.</p> <p>As noted elsewhere the land is a habitat for a range of creatures of note for the area. This will impact their movement, mating locations and foraging.</p> <p>It is likely that run off from the site whilst construction is in progress will impact water quality in the coastal zone and there is insufficient area within the block to capture, provide for sediment to settle or control offsite impacts.</p> <p>Object to the proposal ignoring the presence of the penguin colony on Cooee Point and Cooee Beach when this site has had penguins nesting under the deck, as the owners are aware and have shared with us previously.</p> <p>The proposal will also affect our local penguin colonies and other small animals that frequent our beautiful suburb. The fencing proposed does not support these animals moving naturally through this area.</p> <p>The renders misrepresent the interface of this site with the Coastal zone protected by the Code 7.6.1. The statements with the application also dismiss the sensitivity of this area. Given more than half of the land is covered by this Code it is not appropriate to give the assessment of the proposal so little credence against these requirements.</p>	
<p>State Planning Provisions 15.4.1. The proposed building does not meet the performance criteria of P2 (a), and (c) as it will cause unreasonable loss of amenity to neighbouring properties. The dwelling</p>	<p>Clause 15.4.1 relates to building height in the General Business zone and is not relevant to assessment of this proposal.</p>

<p>next door will suffer significant loss of light to due to the size and scale of the proposal.</p>	
<p>Size and scale</p> <p>The overall size and scale of the development is not appropriate in a frontline coastal position and a proposal on a much-reduced scale, being more sympathetic to the local environment, would be welcomed.</p> <p>The scale and brutality of built form is at odds with the seaside landscape values of the area when compared with the development footprint on our adjoining lot and along the interface of other properties with Morris, Burnell and Turrung Streets.</p> <p>It is not considered that the building design responds to the size, shape, contours or context of the land. The report with the application states that the proposal meets the performance requirements by respecting the he visual landscape character of the area through low-profile, non dominant design. This assertion is not justified and a stroll along the Coastal Share Pathway from the truck wash to the new seating, sauna and public parking/toilets will highlight that this visually exposed site is proposed to be developed in a high impact and dominant manner without comparison in this established residential area.</p> <p>This project presents a monument to the landowners, rather than reflecting the coastal beauty and functional homes of Cooee. The houses in our suburb have patches of back yard for play, to soak the rain and private space. The proposed new building of glass and bright reflective surfaces will not sit in the coastal landscape; with its height and bulkiness it will monster over those moving past.</p> <p>Everything about this proposal screams excess. For example: the design includes a water front balcony sitting atop 14 columns extending the full length of the</p>	<p>Whilst the proposal presents as a certain style that may not appeal to all tastes, the planning scheme does not specifically regulate the look and style of a building beyond where it is located on a site in terms of setbacks, its size in terms of building envelope, site coverage, width of openings for a garage or carport within 12m of a frontage and controls around privacy in relation to decks with a floor level more than 1m, above ground level.</p> <p>The consideration of the visual appearance of a building beyond the standards within the planning scheme are matters for the designer and owner.</p> <p>It is acknowledged that the area predominately contains single story detached dwellings built in the 1940's and 50's however the planning scheme allows scope for larger dwellings to be built consistent with building design trends and modern ways of living.</p> <p>Bulk and scale of appearance are assessed under the performance Criteria P3 of Clause 8.4.2 regarding building envelope.</p> <p>It is noted that the site is of a triangular shape having a frontage (meaning a boundary of a lot which abuts a road) and two side boundaries.</p> <p>The planning scheme allows a permitted building envelope as per the diagram below to a height of 8.5m. The permitted setback from a side boundary is 1.5m.</p> <p>Diagrams submitted with the proposal demonstrate that based on the site contours the proposal fits within the permitted building envelope.</p> <p>The proposal meets the Acceptable Solution relating to building envelope and does not rely on Performance Criteria.</p> <p>As the proposal meets the Acceptable Solution for setbacks and building envelope for all dwellings it is deemed to comply with the relevant Clause in relation to sitting and scale.</p>

ocean side frontage (there is no dimension provided).	
<p>Clause 8.4.2</p> <p>We also submit that whilst meets most acceptable solutions for Clause 8.4.2 Setbacks and building envelope for all dwellings with millimetres to spare in the case of the side western side setback and building height, it does not achieve the Objectives of this Clause.</p> <p>Detrimental impact on the amenity and streetscape of the neighbourhood as a consequence of failing to respond to the frontage to the street. The proposal will be a massive blank wall on the upper storey, as all windows are orientated to the ocean and the excessive balcony along the western boundary intrudes onto the use by the many walkers and cyclists enjoying the Shared Coastal Pathway.</p> <p>a) Object to the overdevelopment of the site, facilitated by the excessive site coverage, and suggest that the proposal stretches the maximum possible from the site to the extent that it does not achieve the Objectives of Clause 8.4.2 as the proposal will have a detrimental impact on:</p> <p>i. the amenity and streetscape of the neighbourhood as a consequence of failing to respond to the frontage to the street. The proposal will be a massive blank wall on the upper storey, as all windows are orientated to the ocean and the excessive balcony along the western boundary intrudes onto the use by the many walkers and cyclists enjoying the Shared Coastal Pathway.</p> <p>The encroachment into the side setbacks to our property on the eastern boundary are likewise unacceptable. The proposal lacks respect for the human scale of the area and the natural values of the locality;</p> <p>ii. our passive solar access, reducing</p>	<p>The proposal meets the objectives to this clause through compliance with the Acceptable Solution pathway A1 (a), A2 and A3 of the Clause 8.4.2 as discussed above in comments relating to 'size and scale'.</p> <p>If a proposal meets the Acceptable Solution it is deemed to meet the objectives to the Clause and Council cannot assess against the standard any further.</p> <p>Proposal plans demonstrate that the proposed dwelling additions and alterations will not project further forward from the existing established setback.</p> <p>The dwelling is a minimum of 1.5m from each side boundary with the exception of proposed slimline water tanks identified on the site plan and shown on the eastern elevation. These water tanks have a height of only 1.153m and meet the exemption for water tanks under Clause 4.6.13 of the planning scheme.</p> <p>A 3D building envelope is also provided as shown in figure's 8 and 9 below demonstrating compliance.</p>  <p><i>Figure 8 – Extract from drawing set, 'Building envelope Perspectives, drawing no. A04.</i></p>

the amenity of our home's primary habitable areas on both the ground and upper levels (split level at the rear of the lot) and leading to increased heating costs to warm our home between Autumn and Spring.

iii. Access to a view of the sky from our kitchen and living area on the ground level.

iv. stormwater runoff – as the site is more than 80% impervious surfaces, rainwater will either end up in our property, the road or public land in greater volumes and increased velocity creating an off-site hazard and reducing the amenity of our property and risking the landscape work done by Council adjoining this site.

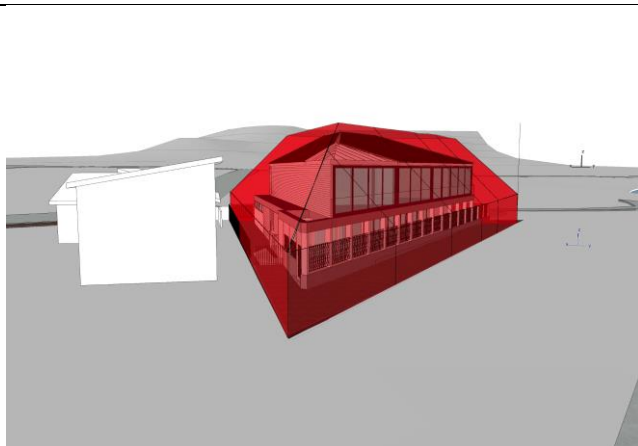


Figure 9 - Extract from drawing set, 'Building envelope Perspectives, drawing no. A04.

Clause 8.4.3

In terms of Clause 8.4.3 Site Coverage and private open space for all dwellings, again the objectives are not met with no provision for the play of young children, minimal landscaping and no space for any larger plantings to soften the impact of the structure and the private open space has little access to direct sunlight.

Whilst the applicant may argue that their children no longer need garden space, the building is expected to have a life of 50 years or more (many homes in this neighbourhood are 75 years old) the space provision is for many generations of households.

It is also argued that in relation to Acceptable Solution A2 the provision of almost one quarter of the 24m² beyond the actual (non-boundary) fence of the site does not meet the definition of private open space.

Whilst in a legal sense this land belongs to the site, in a pragmatic sense the proposal will appear to co-opt the public land.

Further it is unclear from the plans if this will be part of the constructed patio or grassed area or some other treatment. Given this is the only private open space and all other parts of the land are long narrow linear spaces, this is not considered to achieve standard expected

The proposal relies on assessment under the Performance Criteria under Clause 8.4.3 (P2) – See discussion within the body of report.

<p>for A2.</p> <p>*See 'Attachment 1' in Hausknecht submission</p>	
<p>Clause 8.4.7</p> <p>Assessing the design against Clause 8.4.7 Frontage Fences for all dwellings, again it fails to achieve the Objective (c) being is reasonably consistent with that on adjoining properties. The nature of the construction, height and length exceed any of the established front fences in the neighbourhood.</p>	<p>The proposed front fence to consist of breezeblocks is 1.8m in height and provides a uniform transparency of no less than 30% complying with the exemption in Clause 4.6.3 – fences within 4.5m of a frontage.</p>
<p>Plans unnecessarily complex to decipher/distorted 3D renders</p> <p>We would also like to highlight that the plans provided with the application remain inconsistent and unnecessarily complex to decipher. Common practices such as showing windows on the site plan are not observed, dimensions from boundaries are not fully marked and need to be extrapolated from other measurements, and renders have not all been updated to reflect the floor plan changes. Consequently, there is an unreasonable need to trust that what is being reviewed is what is being built.</p> <p>The 3D renders at A00, A04, A05 and A06 do not align accurately with the site, ground floor and first floor plans provided. Whilst the 3D renders may have assisted in selling the design to the owners, they can play no role in the planning assessment NOR SHOULD THEY FORM PART OF THE ENDORSED PLANS TO AVOID FUTURE DISPUTES. They are distorted perspectives that do not align with plans A02, A03 and A03 or existing site conditions. Plans A02, A03 and A03 are either missing dimensions or they are illegible. **please note there REMAINS an error with the numbering of the plans. There are two plans purporting to be A03**.</p> <p>***Examples are provided within representation – Hausknecht submission</p>	<p>The level of details shown on the plans provides all relevant information to allow assessment of the application against the <i>Tasmanian Planning Scheme</i>.</p> <p>A planning application must include the necessary information for Council to determine compliance with relevant standards set out under the <i>Tasmanian Planning Scheme</i>.</p> <p>The planning application includes such relevant information.</p> <p>It is acknowledged that the render on the cover page, drawing A00 and the last three renders showing the northwestern elevation and dwelling frontage to Morris Street (drawings un-numbered) appear to be incorrect with regards to the removal of the deck on the northwestern side of the building (as opposed to original plans in which a deck was proposed along the northwestern side of the building). This deck has since been removed and become a fully operable full height window system with a balustrade directly adjacent to the wall of the dwelling. This appears incorrectly on the 3D renders.</p> <p>Consequently, the 3D renders will not be endorsed as documents forming part of a permit if a permit is granted.</p> <p>The building envelope diagrams, however, appear consistent with the site, floor plans and elevations.</p> <p>It is acknowledged that the plans show that the sheets labelled floor plans and elevations are both given drawing no. A03. This is a mistake on the title block of the plans and is not significant in the assessment of the proposal against the <i>Tasmanian Planning Scheme</i>.</p>

	If a permit is granted the documents submitted with the application (if relevant) become documents forming part of that permit. Any variations from the permit documentation will need to be assessed by Council against the <i>Tasmanian Planning Scheme</i> .
Description of the application during public notification We suggest Council continues to be generous in characterising the nature of the application as dwelling alterations, partial demolition and additions when only a 12sqm unroofed bathroom shell and 4 internal walls are to be retained. Retention of these elements cannot be guaranteed and yet the applicant is relying on them to avoid the scrutiny of a new build. Merging old and new built form is not best practice. There is no way to guarantee these small elements can survive unscathed post detachment from the existing dwelling. We say the retention of the bathroom shell and two walls is a ruse to lever proposed reductions to the non-primary setback to our adjoining property.	A development, whether it be for alterations and extensions to an existing dwelling or a new dwelling will be assessed against the same standards within the <i>Tasmanian Planning Scheme</i> . In this case, it is acknowledged that much of the existing dwelling will be demolished however the ground floor will retain the existing floor layout in terms having three square shaped rooms on either side of an entry hallway with bathroom towards the rear. Alterations, partial demolition and additions was seen as the best fit description.
Sewer Main Object to the encroachment into the sewer easement as it creates a legal minefield and liability to Council and the responsible sewer authority in the event maintenance works/excavation is required in the future. If this development is approved, who would be liable should the dwelling footings be damaged by public works in the future?	The site contains a Tas Water sewer main that runs just inwards from the northwestern boundary of the site. The main runs for the length of the site. The site plan submitted shows a 300 sewer pipe located by Michell Hodgetts Surveyors 0.98m from the northwestern boundary. The plan does not show the building located over this main.

ATTACHMENTS

- 1 [↓](#). Application Documents
- 2 [↓](#). Combined Representations

PLEASE QUOTE

Your Ref:

Our Ref: DA 2025/64

Enquiries: Planning Department

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320ABN: 29 846 979 690
Phone: (03) 6430 5700
Email: burnie@burnie.tas.gov.au
Web: www.burnie.tas.gov.auWe value your feedback on our service.
Tell us about it at www.burnie.tas.gov.au/feedback**NOTICE OF APPLICATION FOR LAND USE PERMIT**

(Section 57(3) Land Use Planning and Approvals Act 1993)

Advice to Adjoining Land Owner or Occupier

Application No: - DA 2025/64
Development Site: - 2 Morris Street COOEE
CT: 21960/3
Proposal: - Dwelling alterations, partial demolition and additions

Notice of the above application is served on you as an adjoining land owner or occupier.

The application may be viewed at -

**Burnie City Council Customer Services Counter
Ground Floor, City Offices,
80 Wilson Street, Burnie**

Between the hours of 8.45 am – 4.45 pm Monday to Friday inclusive (excluding public holidays) or on Council's website at www.burnie.tas.gov.au/permits

You are entitled to make representation in writing on any aspect of the proposal addressed to: -

**General Manager,
Burnie City Council,
PO Box 973, Burnie 7320**

or burnie@burnie.tas.gov.au by no later than 5.00 pm on **17 October 2025**. Council must have regard to any written representation received during the exhibition period when considering its decision on the application.

All persons who make representation will be notified within seven (7) days of the Council's decision. Any persons who made representation and is not satisfied with the Council decision may, under Section 61(5) of the *Land Use Planning and Approvals Act 1993*, lodge an appeal against that decision within fourteen (14) days of the date of that notice to: -

**The Tasmanian Civil and Administrative Tribunal,
GPO Box 1311,
HOBART TAS 7001.**

Should you have any enquiries regarding this development proposal, please do not hesitate to contact the Planning Department on (03) 6430 5700.

Troy McCarthy
PRINCIPAL PLANNER
Date of Notice: - **2 October 2025**

BURNIE CITY COUNCIL
PO Box 973, BURNIE, TASMANIA 7320.
Ph : (03) 6430 5700
Email : burnie@burnie.tas.gov.au

**Land Use Planning and Approvals Act 1993****Tasmanian Planning Scheme****PERMIT APPLICATION***Office use only*

Application No _____

Date Received _____

Permit Pathway - Permitted/Discretionary

Use or Development Site:

Street Address 2 MORRIS STREET, COOEE POINT

Certificate of
Title Reference CT 21960/3**Applicant**

First Name Starbox

Second
Name

Surname Architecture

Postal Address: LVL 1 21 BEST STREET
DEVONPORT

Phone No:

Mobile:

Email Address:

I/we consent for all giving of information and the serving of notices in relation to this application to be delivered electronically to the above email address?

YES



NO

**Applicants Signature:****Owner** (note – if more than one owner, all names must be indicated)

First Name Todd

Second Name

Surname Clifford

Postal Address:

Phone No:

Instruction for making a permit application**a) Use or development?**

The application must provide a full description of the proposed use and/or development and of the manner in which the use and/or development is to operate.

“Use” is the purpose or manner for which land is utilised. “Development” is any site works (including any change in natural condition or topography of land and the clearing or conversion of vegetation), and the construction, alteration, or removal of buildings, structures and signs, required in order to prepare a site for use or to change existing conditions within a site. Subdivision is development.

Clause 6.2 Tasmanian Planning Scheme provides the use classes by which all use or development must be described. Development must be categorised by reference to the use class it is to serve.

b) Required Information

Adequate statements, plans and specifications must be included within the permit application to address and demonstrate compliance with all applicable requirements of the planning scheme, including any site analysis, impact report and recommendation, and advice, consent or determination required from a State agency or utility entity.

The application must clearly identify the documents relied upon for determination.

Section 51(1AC) *Land Use Planning and Approvals Act 1993* provides that a permit application is not valid unless it includes all of the information required by a planning scheme. Clause 6.1 Tasmanian Planning Scheme prescribes the minimum information that is necessary in order to complete a valid permit application.

S54 *Land Use Planning and Approvals Act 1993* provides that the planning authority may require the applicant to supply further information before it considers a permit application. If the planning authority requires further information to more particularly address one or more of the applicable requirements of the Tasmanian Planning Scheme, the statutory period for determination of a permit application does not run until that information is answered to the satisfaction of the planning authority

c) Applicable Provisions and Standards

The permit application must be assessed against the applicable provisions and standards of the Tasmanian Planning Scheme. The application is to identify by reference the clauses it relies upon to demonstrate compliance. (eg *clause 8.4.3 (A1 – A4, and P5)*)

d) Discretionary Permits

If a permit is discretionary the permit application must be notified for a period of 14 days to allow opportunity for any interested person to consider the proposed use and/or development and to provide comment on the discretionary matter.

If a permit application relies on performance criteria to satisfy an applicable standard or is discretionary under another provision of the interim planning scheme, the permit is discretionary only with respect to that standard.

The Council must have regard to all representations received during the notification period on a discretionary matter when determining whether to grant or refuse a permit.

e) If the applicant is not the landowner

If the applicant is not the owner of the land in the use or development site, the applicant is required to notify all of the owners either prior to or within 7 days from the date of making the permit application.

The permit application must identify all of the landowners; and the applicant must sign the application form to acknowledge the obligation to advise such landowners that the permit application has been made.

If the site includes land owned or administered by the Burnie City Council or by a State government agency, the consent in writing from the Council or the Minister responsible for Crown land must be provided at the time of making the application.

f) Applicant declaration


It is an offence for a person to do any act that is contrary to a compliance requirement created under the section 63 *Land Use Planning and Approvals Act 1993*. The applicant is required to complete a declaration that the information given in the permit application is true and correct.

g) Payment of Fees

The Council is not required to take any action on the permit application until all the relevant fees have been paid.


Permit Information		(NB If insufficient space, please attach separate document)
Proposed Use:		
Use Class	Residential - Alterations and Additions of existing dwelling	
Documents included with the permit application to describe the Use		
2 MORRIS STREET COOEE POINT DA2		
Proposed Development		
Use class to which the development applies Residential		
Documents included with the permit application to describe the Development		
Provisions and Standards relied upon for grant of a Permit		

Value of use and/or development

Notification of Landowner/s
If land is not in applicant's ownership
I, STARBOX ARCHITECTURE, declare that the owner/each of the owners of the land has been notified of the intention to make this permit application.
Signature of Applicant  Date 09.05.2025

If the permit application involves land owned or administered by the BURNIE CITY COUNCIL
Burnie City Council consents to the making of this permit application.
General Manager (Signature) Date

If the permit application involves land owned or administered by the CROWN
I, the Minister responsible for the land, consent to the making of this permit application.
Minister (Signature) Date

Applicant Declaration
I, STARBOX ARCHITECTURE declare that the information I have given in this permit application to be true and correct to the best of my knowledge.
Signature of Applicant  Date 09.05.2025

Office use only

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

SEARCH OF TORRENS TITLE

VOLUME 21960	FOLIO 3
EDITION 4	DATE OF ISSUE 05-May-2022

SEARCH DATE : 14-Jan-2025

SEARCH TIME : 02.26 PM

DESCRIPTION OF LAND

City of BURNIE
Lot 3 on Diagram 21960
Being the land described in Conveyance No.30/4700
Derivation : Part of 50000A-0R-0P Gtd.to Van Diemens Land
Company
Derived from A24744

SCHEDULE 1

M930200 TRANSFER to TODD ANDREW CLIFFORD and NICOLA JAYNE
CLIFFORD Registered 05-May-2022 at noon

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
E286331 MORTGAGE to AMP Bank Limited Registered 05-May-2022
at 12.01 PM

UNREGISTERED DEALINGS AND NOTATIONS

No unregistered dealings or other notations



FOLIO PLAN

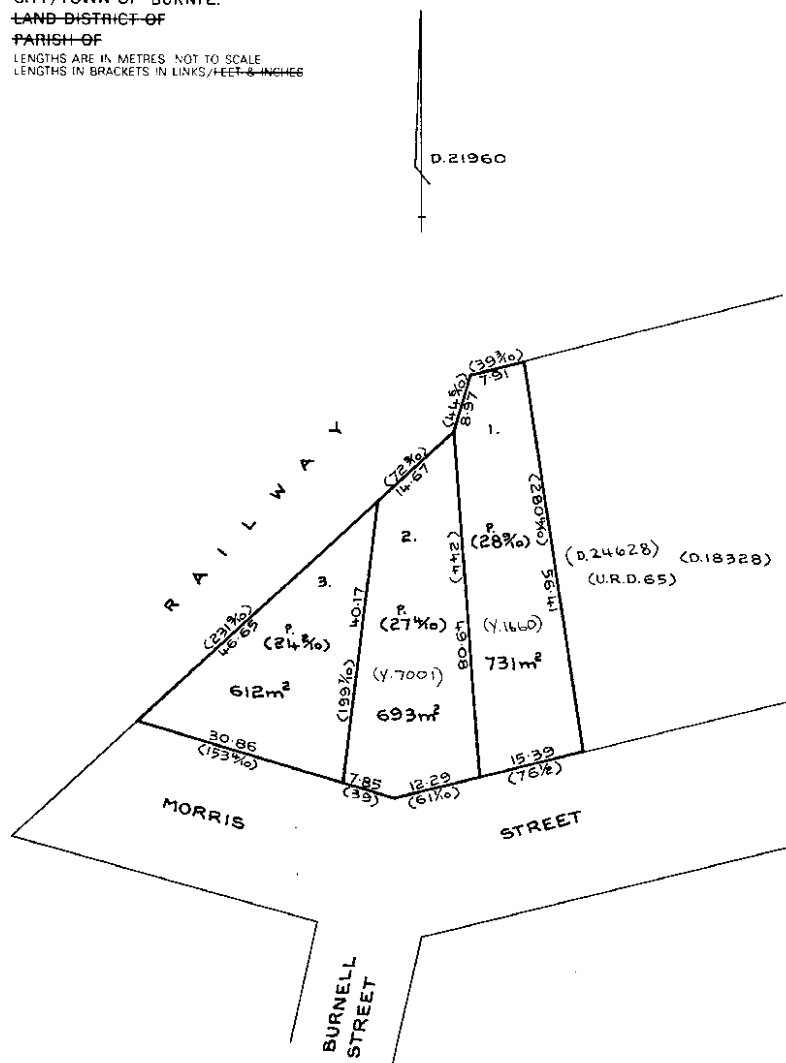
RECORDER OF TITLES

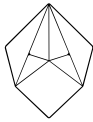
Issued Pursuant to the Land Titles Act 1980



APPROVED FROM: 29 NOV 1983 ACTING DEPUTY: <i>P. Broude</i> RECORDER OF TITLES	CONVERSION PLAN	REGISTERED NUMBER D.21960
FILE NUMBER Y.1660	GRANTEE PART OF 50,000 acres V. D. L. Company.	B. HILL. 28-11-83

SKETCH BY WAY OF ILLUSTRATION ONLY

CITY/TOWN OF BURNIE.
LAND-DISTRICT OF
PARISH OFLENGTHS ARE IN METRES NOT TO SCALE
LENGTHS IN BRACKETS IN LINKS/FEET & INCHES

**STARBOX**
ARCHITECTUREThursday 26th June 2025**RESPONSE TO PLANNING SCHEME**
2 Morris St, Cooe Point, TAS**Zone** General Residential**Clause 8.4.3 Site Coverage and Private Open Space For All Dwellings**

- P2 (a) The private open space proposed is directly adjacent to the rumpus room on the ground floor of the dwelling. There is a large sliding door connecting the two spaces. When the sliding door opens the private open space is perceived as an extension of the rumpus room.
- (b) The proposed private open space is located to the north and has ample amount of solar access. The allocated private open space has a operable fence running through it. This permeable and transparent fence is designed to be opened to enhance visual and sensory connections between the private open space and the surrounding coastline, beach, local vegetation and climate.

Clause 8.4.6 Privacy for all dwellings**P1 Elevated Outdoor Areas**

The balcony does not face or overlook any adjoining private residential properties. Instead, it directly overlooks Crown land that is uninhabited, undeveloped, and not zoned for residential use. As such, there is no risk of compromising the privacy, outlook, or sunlight access of neighbouring dwellings.

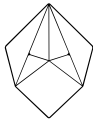
P2 Elevated Windows or Glazed Doors

The performance criteria relating to privacy aim to prevent unreasonable overlooking into sensitive areas such as private open space or habitable room windows of adjacent dwellings. Given that the balcony overlooks Crown land, and is categorised as a public reserve this intent is fully maintained. There are no dwellings or areas of private use within view, and therefore, no adverse privacy impacts are created.

P3 Shared Driveways or Parking Spaces

N/A

**ARCHITEAM**16 Rooke Street Devonport | info@starbox.net.au | www.starbox.net.au | (03) 6424 7736

**STARBOX**
ARCHITECTURE**Code C7.0 Natural Assets Code**

While the site is mapped under the Natural Assets Code, the proposed works do not occur near any defined watercourse, wetland, or coastline. The development footprint is sited away from any natural hydrological feature and does not intersect with environmentally sensitive areas. There will be no disturbance of water flow, no coastal modification, and no impact to riparian ecosystems.

A portion of the western elevation is purposefully dedicated as a garden, which has been designed to visually and ecologically give back to the surrounding coastal environment. This soft landscaping not only integrates the built form with the existing terrain but also contributes positively to local biodiversity and coastal character, acting as a buffer between development and the broader natural setting.

Due to the extent of the overlay on the property, the vast majority of proposed developments would fall within the mapped area.

The proposal meets the performance solution requirements by:

- **Avoiding encroachment on natural assets** such as water features or coastal hazards;
- **Respecting the visual landscape character** of the area through low-profile, non-dominant design; and
- **Maintaining ecological function** through appropriate planting and minimal disturbance.

**ARCHITEAM**

16 Rooke Street Devonport | info@starbox.net.au | www.starbox.net.au | (03) 6424 7736



Alterations + Additions

2 Morris Street, Coode Point, TAS

Starbox Architecture

10/110 Coode Street, Coode Point TAS 7246

08 6333 1111

www.starbox.net.au

General Notes

1. All work to be done in accordance with the latest edition of the Australian Standards (AS) and the Tasmanian Building Code (TBC).

2. All work to be done in accordance with the latest edition of the Australian Standards (AS) and the Tasmanian Building Code (TBC).

3. All work to be done in accordance with the latest edition of the Australian Standards (AS) and the Tasmanian Building Code (TBC).

4. All work to be done in accordance with the latest edition of the Australian Standards (AS) and the Tasmanian Building Code (TBC).

5. All work to be done in accordance with the latest edition of the Australian Standards (AS) and the Tasmanian Building Code (TBC).

Rev

Description

1

2

3

4

5

Issued

Chk

Date

1

2

3

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5

Project Title

2 Morris Street, Coode Point, TAS

Client

#Client Full Name

Status

Development Approval

Project Number

30025

Sheet Name

COVER PAGE

Drawing Number

AOO

Sheet Size

A1

Rev.

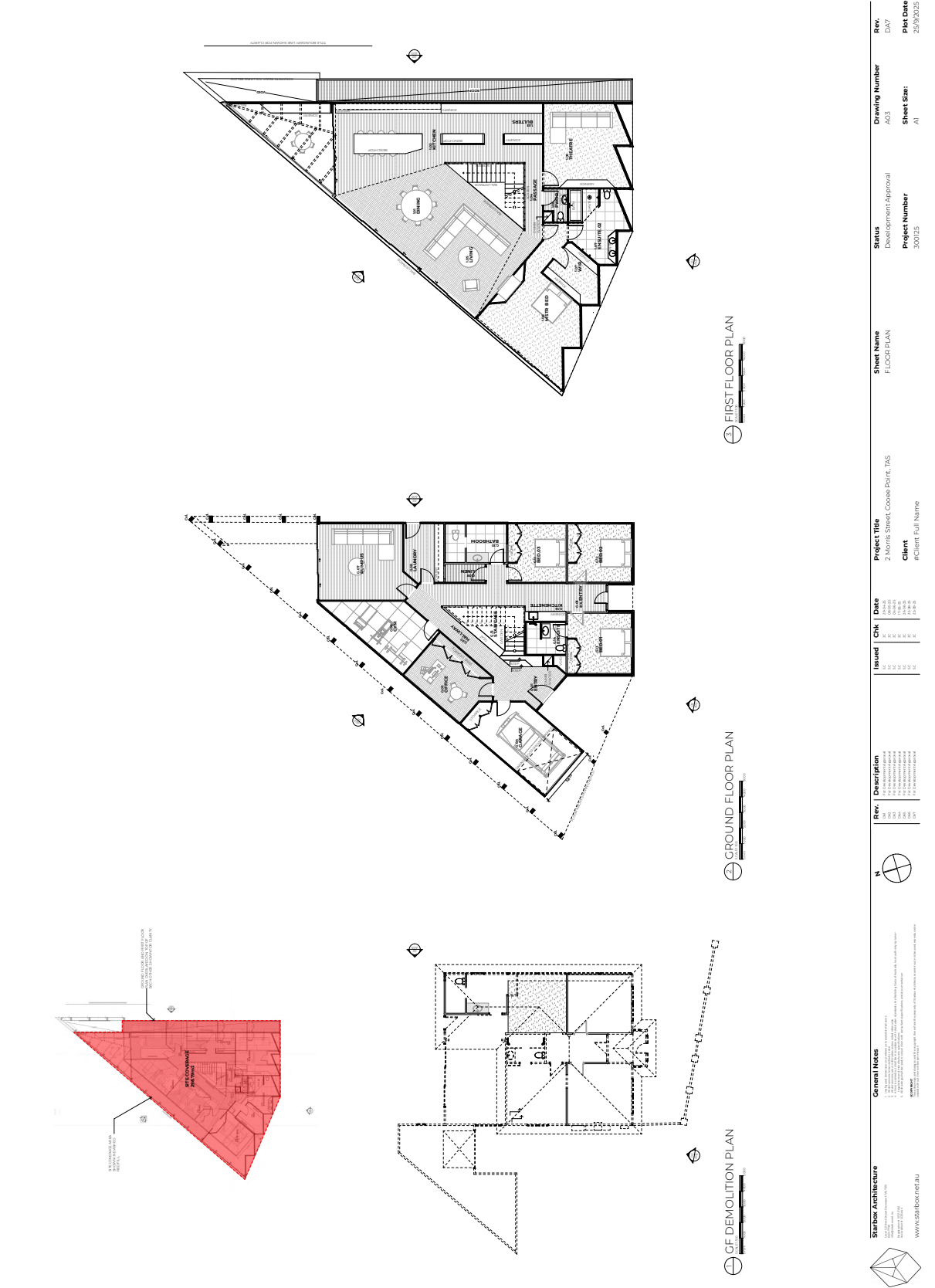
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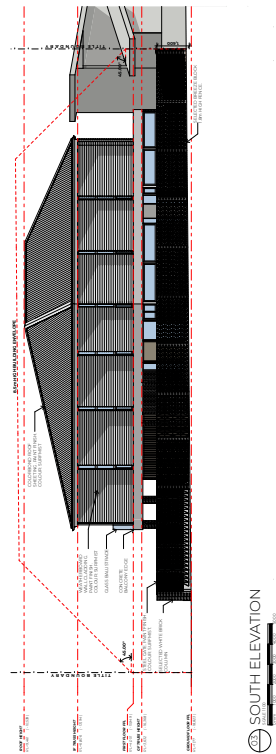
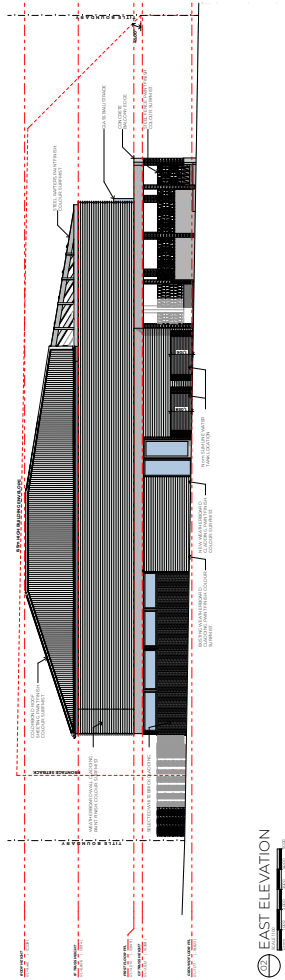
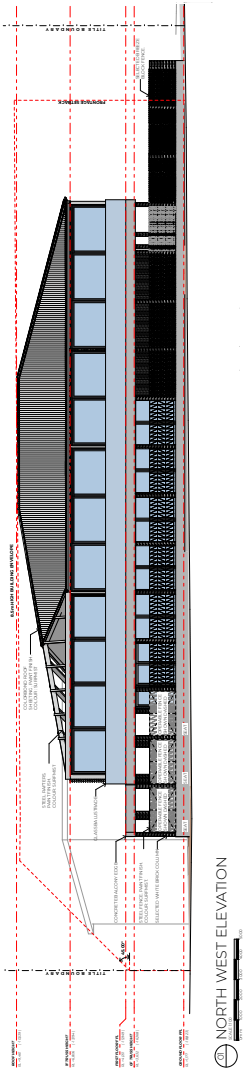
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04/06/25

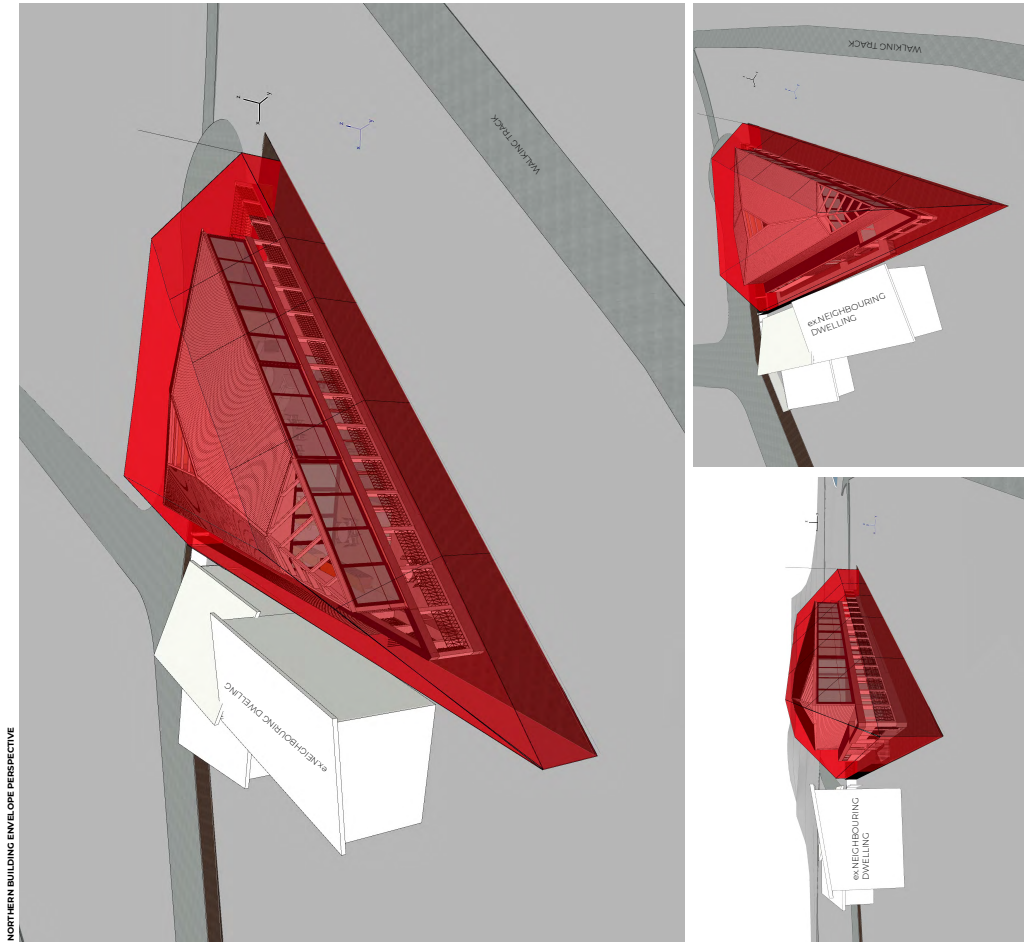
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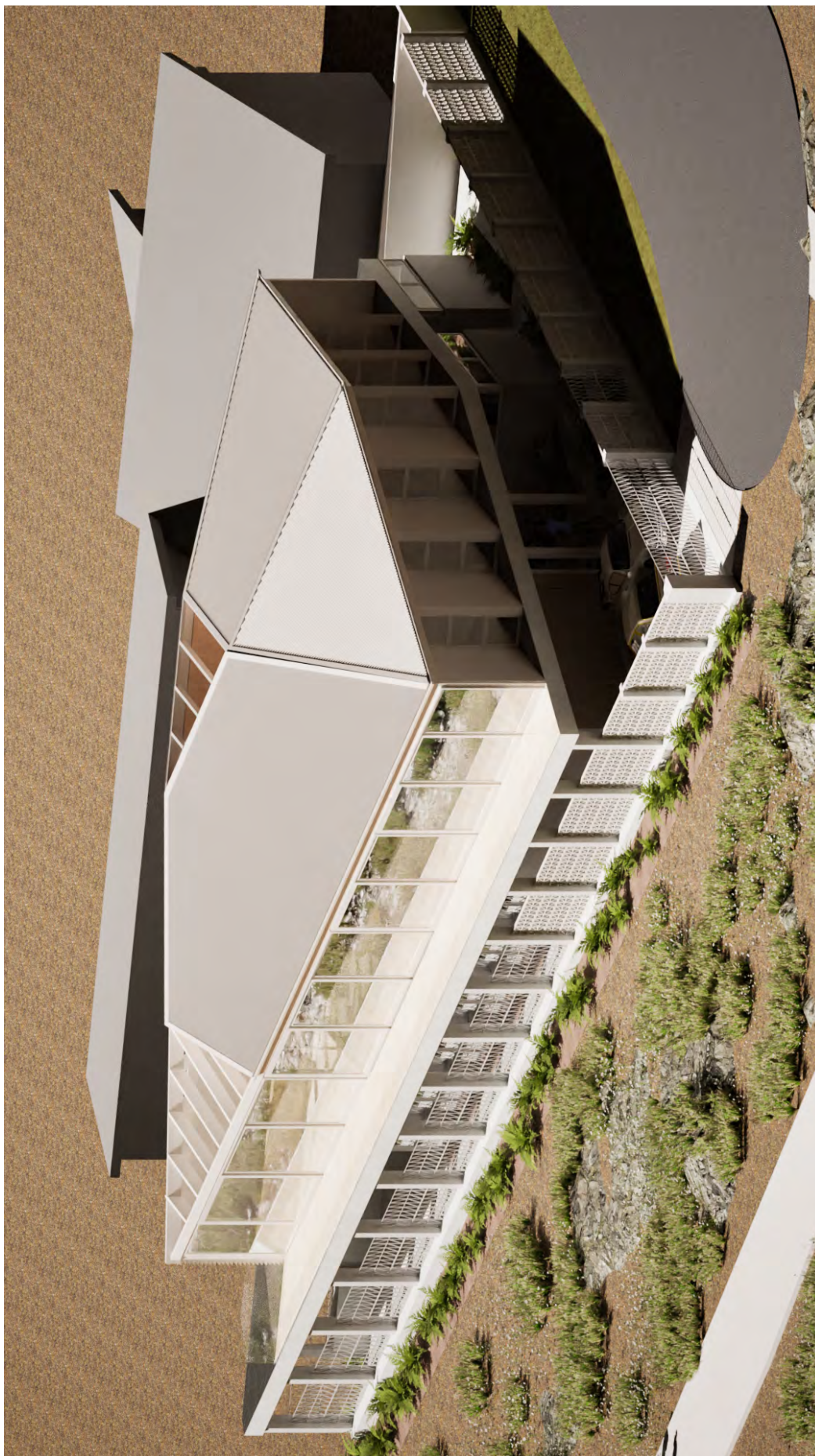
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1 Burnell Street
COOEE, 7320
12 October 2025

Mr Shane Crawford
General Manager
Burnie City Council
PO Box 973
BURNIE 7320
Email burnie@burnie.tas.gov.au

Dear Mr Crawford

**OBJECTION TO DA 2025/64 – 2 MORRIS STREET, COOEE
DWELLING ALTERATIONS, PARTIAL DEMOLITION AND ADDITIONS**

I write again to voice my objection to the proposed development of 2 Morris Street Cooe, in the strongest terms. It amazes me that having withdrawn the previous proposal, the applicant has made so little attempt to make the proposal more sympathetic to the beautiful coastline it adjoins, the active public space and the broader design style in the area.

This land is a focal point of Morris and Burnell Streets, where the residential area and the coastline meet. This means there needs to be clever design to link the natural and human world. Instead of a development that is sympathetic to these qualities, the applicant continues to propose a massive, bulky and imposing structure that respects neither the lowline coastal beauty of Cooe or the open, garden-based residential area.

The natural outlook from the street, that is the entitlement for all Burnie residents to enjoy, will be lost. I am not sure that Council appreciates in summer every afternoon we have visitors coming to enjoy this location, to access the beach, paddle, take photos and relax.

The design of this house seeks to own the beach view, with the balcony barely off the fenceline overlooking the public land, used by hundreds of walkers and riders using the pathway each day. To further this domination of the public space, on the ground level, the proposal includes an openable fence into the public land. It is unclear how this provides privacy to the minimal private open space on ground level for the applicant and how comfortable young families and older residents will feel being watched from this property. Now there is suitable open space for the existing house, across an expanse of both the public land and the backyard that will be lost to walls and fencing.

The proposal is too huge for this corner, presenting big walls of construction to the street, the pathway and the neighbouring house. It will be overwhelming in the extreme.

All along this side of Cooe residents are renovating and maintaining their homes and making the most of this beautiful location. This development, if Council approves it, will set the tone for an unfortunate change in the character of the area. This is something that as a long term resident, I do not support.

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It looks like it will be a mini-hotel, rather than a family home. If I have understood the plans correctly, it would seem that the size of this house and its future use is a relevant question for Council to look at.

With the four bedrooms as well as a gym and office proposed, it is unclear how the allocated single garage and two parking spots on a driveway is adequate to handle the needs of the property. Is this seriously enough? I ask because the size of the proposed house leaves no spare parking space for teens with P Plates in the future given the owners have both personal and work vehicles now. If parking cannot be provided on the land itself, then this will lead to further congestion in the head of the cul de sac at the end of Morris Street, and increase the danger for vehicles moving through the area.

It has already been my experience that the corner of Morris Street and Burnell Street attracts accidents and vehicle damage because of the parking in both streets. My car has been involved in a few incidents because of this. Any resident cars not able to be parked 2 Morris Street will most likely worsen the situation.

The proposal will also affect our local penguin colonies and other small animals that frequent our beautiful suburb. The fencing proposed does not support these animals moving naturally through this area. Regular members of our Church, clean the beach area of litter and care for the environment to support the coastal environment. The single strip of garden outside the fence makes no real benefit for nature to offset this building.

I do not know where the drainage from this project will go as this is not covered in the material on display. In the past I have had issues with water backing up from Morris Street into the intersection near my property. The new building has an extensive roof area and further driveways and entertainment areas that will increase the runoff and I can only imagine that this will get worse.

We are a small, peaceful community of families of all age brackets and welcome the opportunity for others to come and enjoy our little piece of paradise. But this development does not support these outcomes in any way, shape or form. Being substantially oversized, compared to the surrounding area will not lead to improvements in our area. Instead, it will make it busier and less peaceful.

By the building filling the western boundary of the land it will shade the public pathway in the morning throughout the year and the 8.5m roof height, will be the dominant feature on the landscape. We have other two storey homes along this stretch of the coast, but these are all well setback from their fencelines, respecting the privacy of the homeowners and giving the people walking and riding a sense of freedom.

Maximising the cover of the site will create a significant negative impact on the amenity and streetscape when we walk through the head of cul-de-sac to the pathway and along the pathway towards the town.

This house has been used for commercial purposes as an Airbnb rental since December 2023. Over that time, many people have stayed there from across the country. Nothing in the supporting report indicates that the owners of the property intend to live in it, and is my

understanding that the size of this house and the size of their family do not align. Is this a mini-motel development by stealth, where we see a subsequent application for another Air B'n'B?

I urge the Councils to refuse this application on the grounds that it is an overdevelopment of the land and does not respect the nature of the coast or the beauty of Cooeee. The commentary of the application describing the adjoining pathway and coastal reserve as "uninhabited, undeveloped" and not "near any defined coastline" shows that the applicants and the architect have no appreciation of our suburb where their proposal is set. We are not the Gold Coast or Northern Beaches of Sydney and we have a coastline and unique wildlife with the penguins that warrant more care.

Yours sincerely



Lady Delaila Doherty

4 Morris Street
COOEE, 7320

16 October 2025

Mr Shane Crawford
General Manager
Burnie City Council
PO Box 973
BURNIE 7320
Email burnie@burnie.tas.gov.au

Dear Mr Crawford

**OBJECTION TO DA 2025/64 – 2 MORRIS STREET, COOEE
DWELLING ALTERATIONS, PARTIAL DEMOLITION AND ADDITIONS**

We write to object to the above development as the owners and occupiers of the adjoining site, being 4 Morris Street, Cooe. We acknowledge that the applicant has made very modest changes to the proposal since the previous public notification in July.

We contend the proposal still represents an overdevelopment of the site using all acceptable solutions to their maximum extent. The proposed design shows no restraint with the proposed built form impacting a sewer easement and the required setbacks for a non-primary frontage to our neighbouring property. The scale and brutality of built form is at odds with the seaside landscape values of the area when compared with the development footprint on our adjoining lot and along the interface of other properties with Morris, Burnell and Turrung Streets.

This is an imposing structure that will dominate the vista from the adjoining ocean side public open space and the Morris Street streetscape. The excessive site coverage provides no opportunity for a natural softening of this brutal building.

Everything about this proposal screams excess. For example: the design includes a water front balcony sitting atop 14 columns extending the full length of the ocean side frontage (there is no dimension provided). We appreciate that the balcony extension on the first-floor conservatory has now been removed, inadvertently reducing the gross floor area to slip under the 50% site cover provision, so that direct views into our first-floor glassed living area and private backyard are now protected.

It is curious that this change did not lead to a reduction in the undercover patio space to provide some additional grassed area at ground level.

No changes have been made to sway our position that the proposal is in breach of the planning policy position outlined in Code C7.0 Natural Assets Code. This is discussed further in our submission.

We also submit that whilst meets most acceptable solutions for Clause 8.4.2 *Setbacks and building envelope for all dwellings* with millimetres to spare in the case of the side western side setback and building height, it does not achieve the Objectives of this Clause.

1 | Page Hausknecht submission against 2 Morris St

Specifically :

- Objective (a) - the scant setback from the eastern and western boundaries do not reflect the character of the Cooee neighbourhood.
- Objective (b) – there is no reasonable basis to form the view that the scale, bulk, massing and proportion of the proposed development is consistent with its surrounds. The only comparable building in footprint and form are the older units at 10 Morris. However this is a single storey structure that provides homes for significantly greater number of people with each unit having a designated open space area. 4 Burnell Street is the closest two storey house to the development site and it offsets the presence of the building height with a significant setback from the street. This proposal make no attempt to soften its impact in such a way. Located on the junction of the street and coastal pathway makes this design and form more damaging to the urban character of the area.
- Objective (c) - expects that *the separation between dwelling on adjoining properties to allow reasonable opportunity for daylight and sunlight to enter habitable rooms and private open space*. There has been no attempt to achieve this with this application and this development will result in the passive solar design of our home being severely compromised and the cost to live in our home will increase as the kitchen and living space on the ground level will be permanently cold and dark, whilst the upper lounge room will no longer function as a heat trap that has a pump to transfer heat to the ground floor area. This is discussed further below.
- Objective (d) – whilst we don't currently have solar energy installations, and are investigating solar panels and battery options, the upper lounge room will no longer function as a heat trap that has a pump to transfer heat to the ground floor area.

Further discussion on these matters is provided below in Attachment 1.

In terms of Clause 8.4.3 *Site Coverage and private open space for all dwellings*, again the objectives are not met with no provision for the play of young children, minimal landscaping and no space for any larger plantings to soften the impact of the structure and the private open space has little access to direct sunlight.

Whilst the applicant may argue that their children no longer need garden space, the building is expected to have a life of 50 years or more (many homes in this neighbourhood are 75years old) the space provision is for many generations of households.

It is also argued that in relation to Acceptable Solution A2 the provision of almost one quarter of the 24m² beyond the actual (non-boundary) fence of the site does not meet the definition of private open space. The definition refers to this being:

*means an outdoor area of the land or dwelling **for the exclusive use** of the occupants of the land or dwelling, excluding areas proposed or approved for vehicle access or vehicle parking.*

Whilst in a legal sense this land belong to the site, in a pragmatic sense the proposal will appear to co-opt the public land.

Further it is unclear from the plans if this will be part of the constructed patio or grassed area or some other treatment. Given this is the only private open space and all other parts of the land are long narrow linear spaces, this is not considered to achieve standard expected for A2.

Assessing the design against Clause 8.4.7 Frontage Fences for all dwellings, again it fails to achieve the Objective (c) being *is reasonably consistent with that on adjoining properties*. The nature of the construction, height and length exceed any of the established front fences in the neighbourhood.

Most properties in the surrounding area have low profile fences (less than 1m) that simply define the public and private space. In our case, the courtyard created by our fence reduces the impact of vehicle light intrusion into the home from vehicle moving through the intersection of Morris and Burnell Street. This is a practical reason for our front fence that covers part of our frontage.

It is unclear what practical purpose the proposed fence on the development of 2 Morris Street achieves that warrants such an imposing form. A less dominant fence would have been consistent with the character of the area and more compatible with a relaxed coastal theme.

In a practical sense it is likely that the fence will impede penguin movement through this site (I have video evidence of penguins nesting on 2 Morris Street from the land in December 2023 and we regularly have penguins moving through our front courtyard and can provide video and photos to confirm this). Likewise, we have seen pademelons and water rats move through both properties. Excluding these creatures for the ability to forage suggests that the applicants hold no value for the coastal environment this site occupies.

We would also like to highlight that the plans provided with the application remain inconsistent and unnecessarily complex to decipher. Common practices such as showing windows on the site plan are not observed, dimensions from boundaries are not fully marked and need to be extrapolated from other measurements, and renders have not all been updated to reflect the floor plan changes. Consequently, there is an unreasonable need to trust that what is being reviewed is what is being built.

The 3D renders at AOO, AO4, AO5 and AO6 do not align accurately with the site, ground floor and first floor plans provided. Whilst the 3D renders may have assisted in *selling* the design to the owners, they can play no role in the planning assessment **NOR SHOULD THEY FORM PART OF THE ENDORSED PLANS TO AVOID FUTURE DISPUTES**. They are distorted perspectives that do not align with plans AO2, AO3 and AO3 or existing site conditions. Plans AO2, AO3 and AO3 are either missing dimensions or they are illegible. ****please note there REMAINS an error with the numbering of the plans. There are two plans purporting to be AO3****.

The renders misrepresent the interface of this site with the Coastal zone protected by the Code 7.6.1. The statements with the application also dismiss the sensitivity of this area. Given more than half of the land is covered by this Code it is not appropriate to give the assessment of the proposal so little credence against these requirements.

An example of this is **Drawing A06 – External render** - This plan purports to represent the impact of the proposal viewed from beyond the site. We would suggest that the view point used is some 44m from the property boundary and more than 2m lower in height than the natural ground level for the site or the primary viewing area for the development on Morris Street or the Coastal Shared Path.

Drawing A06 – External render



Actual view from this position



*Actual view from Coastal Shared Path – note the new building will be the equivalent of
4.8 times the height of the current wooden fence*



In the application it states: (highlight added for emphasis):



STARBOX
ARCHITECTURE

Code C7.0 Natural Assets Code

While the site is mapped under the Natural Assets Code, the proposed works do not occur near any defined watercourse, wetland, or coastline. The development footprint is sited away from any natural hydrological feature and does not intersect with environmentally sensitive areas. There will be no disturbance of water flow, no coastal modification, and no impact to riparian ecosystems.

A portion of the western elevation is purposefully dedicated as a garden, which has been designed to visually and ecologically give back to the surrounding coastal environment. This soft landscaping not only integrates the built form with the existing terrain but also contributes positively to local biodiversity and coastal character, acting as a buffer between development and the broader natural setting.

Due to the extent of the overlay on the property, the vast majority of proposed developments would fall within the mapped area.

The proposal meets the performance solution requirements by:

- **Avoiding encroachment on natural assets** such as water features or coastal hazards;
- **Respecting the visual landscape character** of the area through low-profile, non-dominant design; and
- **Maintaining ecological function** through appropriate planting and minimal disturbance.

To be direct, this appears to be written from the perspective of someone who has not seen the site and assumes that Council staff assessing the proposal also haven't.

The minimal strip of 3m of landscaping along the western boundary has little prospect of either contributing to the ecology, or integrating the build form into the surround, if planted as shown. It will add to the existing planted area, but without significantly more detail on the native plants proposed and mature trees (which we are confident won't be wanted by the applicant to obstruct the view or allowed by Taswater due to the risk to the sewer line) this is not believable.

The construction is likely to encroach on the coastal zone to access part of the sites and the ultimate development seeks to usurp public space through the openable fence. There is no comparable built element in the immediate vicinity to warrant this bulky structure.

As noted elsewhere the land is a habitat for a range of creatures of note for the area. This will impact their movement, mating locations and foraging.

It is likely that run off from the site whilst construction is in progress will impact water quality in the coastal zone and there is insufficient area within the block to capture, provide for sediment to settle or control offsite impacts.

It is not considered that the building design responds to the size, shape, contours or context of the land. The report with the application states that the proposal meets the performance requirements by:

- *Respecting the visual landscape character of the area through low-profile, non dominant design.*

This assertion is not justified and a stroll along the Coastal Share Pathway from the truck wash to the new seating, sauna and public parking/toilets will highlight that this visually exposed site is proposed to be developed in a high impact and dominant manner without comparison in this established residential area.

Whilst it is appreciated that the applicant's job in representing their client is to put the best light on the proposal, the response to the Natural Assets Code takes *gilding the lily* to the extreme.

We suggest Council continues to be *generous* in characterising the nature of the application as *dwelling alterations, partial demolition and additions* when only a 12sqm *unroofed bathroom shell* and 4 internal walls are to be retained. Retention of these elements cannot be guaranteed and yet the applicant is relying on them to avoid the scrutiny of a new build. Merging old and new built form is not best practice. There is no way to guarantee these small elements can survive unscathed post detachment from the existing dwelling.

We say the retention of the bathroom shell and two walls is a ruse to lever proposed reductions to the non-primary setback to our adjoining property.

- New build requirement 3m setback to non-primary boundary.
- Existing bathroom setback 1.89m.

Conclusion

Based on the information above we:

- a) Object to the overdevelopment of the site, facilitated by the excessive site coverage, and suggest that the proposal stretches the maximum possible from the site to the extent that it does not achieve the Objectives of Clause 8.4.2 as the proposal will have a detrimental impact on:
 - i. the amenity and streetscape of the neighbourhood as a consequence of failing to respond to the frontage to the street. The proposal will be a massive blank wall on the upper storey, as all windows are orientated to the ocean and the excessive balcony along the western boundary intrudes onto the use by the many walkers and cyclists enjoying the Shared Coastal Pathway.

The encroachment into the side setbacks to our property on the eastern boundary are likewise unacceptable. The proposal lacks respect for the human scale of the area and the natural values of the locality;

- ii. our passive solar access, reducing the amenity of our home's primary habitable areas on both the ground and upper levels (split level at the rear of the lot) and leading to increased heating costs to warm our home between Autumn and Spring.
 - iii. Access to a view of the sky from our kitchen and living area on the ground level.
 - iv. stormwater runoff – as the site is more than 80% impervious surfaces, rainwater will either end up in our property, the road or public land in greater volumes and increased velocity creating an off-site hazard and reducing the amenity of our property and risking the landscape work done by Council adjoining this site.
- b) Object to the proposed use of the Shared Coastal Pathway for access, or adoption by the landowner of the segment of pathway and landscaped area adjoining their land.
- The fence should secure all of the Private Open Space for 2 Morris Street, not require the opening of the fence (not on the site boundary) or allow for movement of plant and equipment in through public land that is not a made road.
- c) Object to the way the Code 7.0 Natural Assets has been dismissed as irrelevant in relation to the site and overstatement of the alleged benefits that accrue from the proposal. It simply is not supportable to contend that this development avoids encroachment, respects the visual landscape character or maintains/enhances the ecological functions of the locality.
- d) Object to the proposal ignoring the presence of the penguin colony on Cooee Point and Cooee Beach when this site has had penguins nesting under the deck, as the owners are aware and have shared with us previously.
- e) Object to the poor quality of the plans and the use of distorted 3D renders. It is not the responsibility of adjoining owners, neighbours or Council staff to interpret and *guess* the applicants' intentions nor to be misled by distorted imaging by way of 3D renders.
- f) Object to the encroachment into the sewer easement as it creates a legal minefield and liability to Council and the responsible sewer authority in the event maintenance works/excavation is required in the future. If this development is approved, who would be liable should the dwelling footings be damaged by public works in the future?

This liability could be removed simply by the permit applicant observing the site constraint and building within the available footprint. This is further evidence of the overdevelopment of the site.

We say we will be adversely impacted by the close proximity of the dwelling to our boundary, the sheer bulk and scale of the built form and the domination of the structure in the context of our neighbourhood. The building screams excess from the fence treatment to the peak of the roof. It is unapologetically unresponsive to the site or its constraints.

To date, the permit applicant has had two favourable decisions. Firstly, the Council decision to accept the description of the development and secondly, the apparent approval by the sewer authority to construct the building over a sewer easement. We hope that the third decision results in refusal of the application.

We can be reached by email at the above address or by phoning Veronica on [REDACTED] if there are any issues that require clarification.

Yours sincerely



Anthony Hausknecht



Veronica Hausknecht

Attachment 1 – response to 8.4.3 Site coverage and private open space for all dwellings

In considering the site coverage we contend that the scale, mass and bulking of the building is not consistent with the streetscape as measured 100m from the property. As the image below shows, the Cooee residential area is a relaxed coastal neighbourhood where residential development comprises back yards and garden space being a predominant feature. Whilst in most cases, much of the frontage is developed, this is offset by open space in yards to create a green, open space where residents can play, relax and nature can intrude.

There are 5 double storey homes in the area shown with a red dot. Our objection is not to the double storey nature of the proposal, but the comparative overdevelopment of the land leaving no areas for play, relaxation, visual relief or natural processes like soakage of stormwater.

Location of 2 story houses within 100m of the applicant's site



It is unclear why there is a need for an openable fence in the north east corner of the fencing with a small area beyond the fence. It would be most unsuitable, and potentially unsafe for Shared Coastal Pathway users, if this opening was used to access this part of the land for works or services. The proposed opening also compromises the privacy of this space and should not be supported by Council.

It is unclear, with the extent of the proposed development how operational needs such as drainage from the site will be accommodated with the extent of development proposed. The site will have a very high runoff co-efficient and the 3,000l of tank storage will provide limited impact on the time of concentration of the runoff from the site. Given the majority of landscaped area for natural water absorption is at the front/south of the site and the tanks are located to the rear/north, overflow from the tanks is unlikely to be absorbed within the property.

This will result either in increased run off to our property and the public land to the north. These are not acceptable outcomes under the Urban Drainage Act. Whilst not directly a planning matter – there is a clear nexus between the site cover and setbacks proposed and run off/water management impacts on private and public land.

No provision appears to have been made for clothes drying and it is imagined this could be located within the Private Open Space or along the eastern building façade.

It is not clear how or why the breeze block wall is within the property boundary leaving landscaping associated with this property beyond the fenced site. This is most unusual and it is trusted that this will not lead to the land being abandoned by future residents and Council expected to maintain the additional area.

We do not believe the additions and alterations at 2 Morris St under application No DA2025/29 are appropriate to the planning scheme or are in keeping with community expectations. We have the following concerns:

- 1) The current building is not used as a residence but as short-term accommodation. It is believed the building will be used for these purposes after the additions and alterations which is essentially creating a two-storey hotel in a front-line coastal position, which is not within keeping of the current usage of the greater Cooee area.
- 2) **Clause 8.4.6 P3 Shared driveway or parking spaces** - The Morris St cul-de-sac is a popular parking area for people accessing Cooee beach and the coastal pathway, particularly in summer. Since starting operations as short-term accommodation, large vehicles which do not fit on the property have been parked on Morris St. Moving the location of the driveway at 2 Morris St and the increased traffic caused by the accommodation business will impact on parking in the cul-de-sac.
- 3) **Code C7.0 Natural Assets Code. Respecting the visual landscape character of the area through low profile, non-dominant design** – The proposed building will be two storeys with a long and imposing western façade which will dominate the landscape, in direct contravention to the code. The majority of dwellings in the area are single storey and of vastly smaller overall footprint, height and width. As the proposed dwelling is frontline coastal it (within 25m of the high tide level) the proposed design will dominate the natural landscape and set a precedent in the area.
- 4) **Code C7.0 Natural Assets Code. Maintaining ecological function** – The area of glazing on the western facing wall will increase from the current 1 window of approximately 1.0m x 1.2m to what appears to be 23 windows/glass panels of greater size. The total of the proposed glazed area will span two floors creating a large wall of glass which is a potential hazard to wildlife in two ways:
 1. The glazing and the direction it faces has the potential to cause bird strikes against the glass including migratory species of threatened and conservation significant birds which have been recorded within an 800m radius of the site (reference: Listmap). These include red capped plover (*Charadrius ruficapillus*) swift parrot (*Lathamus discolor*) and white throated needletail (*Hirundapus caudacutus*).
 - 2). The western boundary fence lies within 20m of a little penguin (*Eudyptula minor*) colony and the much-increased light emissions in terms of both intensity and area have the potential to disturb and disorientate the penguin colony.
- 5) **State Planning Provisions 15.4.1.** The proposed building does not meet the performance criteria of P2 (a), and (c) as it will cause unreasonable loss of amenity to neighbouring properties. The dwelling next door will suffer significant loss of light to due to the size and scale of the proposal.

The overall size and scale of the development is not appropriate in a frontline coastal position and a proposal on a much-reduced scale, being more sympathetic to the local environment, would be welcomed.

Stephen and Lisa Mott
10 Burnell St Cooee

14 Burnell Street
COOEE, 7320
12 October 2025

Mr Shane Crawford
General Manager
Burnie City Council
PO Box 973
BURNIE 7320
Email burnie@burnie.tas.gov.au

Dear Mr Crawford

**OBJECTION TO DA 2025/64 – 2 MORRIS STREET, COOEE
DWELLING ALTERATIONS, PARTIAL DEMOLITION AND ADDITIONS**

Yet again I write to object to the development of 2 Morris Street Cooe. After the previous application (DA2025/29) being withdrawn I had hoped the applicant had recognized that their proposal was out of character and an over-development of this site. Unfortunately, it seems this is not the case with the new proposal having only minor changes that make no attempt to enhance the current environment.

The new home is proposed to be significantly larger than the current house and I think it is reasonable to assume that with extra bedrooms and space, extra people and cars can be expected to go to and from the property every day. Over the time I have lived here I have seen significant changes in the level of traffic in the area and this land funnels up Burnell Street past my home.

Like many local residents, I regularly walk the beach path and beach, enjoying the privacy and clarity the scenery brings; feeling the water, sand and air and the wonder of this natural space. Now if this development fills the land at the end of Morris Street with a building that is four times the height of the current fence, it will impose itself on this precious public space and all those that love it.

I feel it will be uncomfortable for me, and many other residents to relax and commune with the earth and ocean here with people on the new balcony (that is almost on the property boundary) gawking on. This is a very exposed and overpowering proposal and I question how it will encourage locals to continue their existing practices for exercise, meditation and relaxation.

In my beach trips, I regularly clean up the washed in litter to make the area safer for the penguins, pademelons, water rats and other animals we share this space with. I fear that the construction of this property, further new fencing and no real yard will impact the land these animals rest and graze on.

- 1 -

This project presents a monument to the landowners, rather than reflecting the coastal beauty and functional homes of Cooeee. The houses in our suburb have patches of back yard for play, to soak the rain and private space. The proposed new building of glass and bright reflective surfaces will not sit in the coastal landscape; with its height and bulkiness it will monster over those moving past.

This house has been an Airbnb rental since December 2023, with solid use through the warmer months, both for tourists and workers in the area. My concerns now are that this new proposal is either to service larger groups of visitors or for a much larger household. I appreciate that this is not currently proposed, but then neither was the use for it now envisaged when the home was purchased.

I ask that Council refuse this proposal on the basis that it will not support the people and animals that call this area home or add to the natural assets that we, the residents, love.

Thank you for considering my concerns.

Yours sincerely



Paul Duncan-Smyth

MOTIONS ON NOTICE**AO174-25 MOTION ON NOTICE - SPORTS CENTRE COMBINED****FILE NO:** 15/5/2, 25/28353**PREVIOUS MIN:**

Councillor Ken Dorsey has given notice that he would move the following motion at this meeting:-

“THAT Council:

- 1) *Recognises the current inadequacy of indoor sports facilities across the three communities of Burnie, Wynyard and Somerset, which collectively support representative basketball teams and underage development programs competing between Latrobe and Smithton.***
- 2) *Acknowledges that:***
 - a) Each community currently maintains separate, aging indoor facilities that duplicate services and strain individual council budgets;***
 - b) The three communities share a combined population of approximately 31000 (2021 Census) residents that virtually are connected for a distance of 22 kilometres;***
 - c) All three communities’ field representative basketball teams and junior development squads in the same regional competition;***
 - d) Current facilities are inadequate for modern sporting standards, limiting participation growth and event hosting capabilities;***
 - e) Current facilities can be maintained as training facilities and numerous other uses;***
 - f) A collaborative regional approach would deliver superior outcomes while achieving economies of scale;***
- 3) *Resolves to initiate a feasibility study for the development of a centrally-located, purpose-built regional indoor multi-sport facility that would:***
 - a) Serve the combined needs of Burnie, Wynyard and Somerset;***
 - b) Provide competition-standard courts suitable for basketball, netball, futsal, volleyball, and other indoor sports;***
 - c) Include appropriate spectator seating, change rooms, and amenities;***
 - d) Offer training and development spaces for underage programs;***
 - e) Generate revenue through tournaments, events, and community bookings;***
 - f) Potentially host state and national level competitions;***
- 4) *Directs the General Manager to:***

-
- a) *Engage with sporting clubs, community groups, and stakeholders from all three communities to assess current and future needs;*
 - b) *Investigate potential central locations accessible to all three communities;*
 - c) *Prepare a comprehensive feasibility study including:*
 - i) *Projected capital costs and funding sources (including state and federal grants);*
 - ii) *Ongoing operational costs and revenue projections;*
 - iii) *Cost-benefit analysis comparing regional facility versus maintaining three separate facilities;*
 - iv) *Community consultation outcomes;*
 - v) *Governance structure options for multi-community facility management;*
 - vi) *Design specifications meeting Basketball Australia and relevant sporting body standards;*
 - d) *Explore partnership opportunities with state sporting organizations and neighbouring councils;*
 - e) *Report back to Council within [6 months] with findings and recommendations;*
- 5) *Commits to genuine consultation with the communities Burnie, Wynyard and Somerset throughout this process, ensuring all stakeholders have opportunity to contribute to the facility design and operational model; and*
- 6) *Utilises the funds allocated to fund a feasibility study and initial consultation process."*

COUNCILLOR'S COMMENTS

Background

The three communities of Burnie, Wynyard and Somerset on Tasmania's Northwest Coast share strong sporting connections, particularly through basketball. Each community fields representative teams and operates junior development programs that compete in the same regional competitions. However, current indoor facilities across the three towns are aging, undersized, and unable to meet growing demand or modern sporting standards.

Individual communities lack the population base and financial resources to independently develop 2 or 3 world-class facilities. A regional approach would pool resources, eliminate duplication, and deliver a facility that serves the entire area while positioning the Northwest Coast as a premier destination for indoor sporting events.

This motion represents an opportunity for genuine regional collaboration that benefits all three communities while providing superior facilities for current and future generations of athletes.

Financial Implications

Immediate: Feasibility study already budgeted

Future: Subject to feasibility study outcomes. Seek funding from:

- Tasmanian Government sporting infrastructure grants
- Federal government regional development funding
- Sport Australia facility funding programs
- Private sector partnerships
- Community fundraising initiatives

Operational costs would be offset by facility hire revenue, events, shared cost, and potential cost savings from closing or repurposing existing underutilized facilities.

Strategic Alignment

This motion aligns with:

- Council's Strategic Plan objectives for community health, wellbeing, and regional collaboration
- State Government priorities for sporting infrastructure and regional development
- Community aspirations for improved recreational facilities
- Economic development through sports tourism and events

GENERAL MANAGER'S COMMENTS

Council has an adopted Annual Plan action to "Complete a feasibility study for new multipurpose sports facility in line with grant deed requirements" and has secured funding to undertake this work.

A consultant has been engaged to undertake this work and collation of all previous studies has been undertaken and provided as background information.

In September, a meeting of all sporting clubs in Burnie was held. This project was outlined to the group and clubs were asked to express interest if they wished to participate in the discussions regarding the development of a new facility. It would be possible, and would be a sensible part of any feasibility study, to understand the needs and proposed developments in neighbouring areas to identify and analyse any potential opportunities.

Waratah Wynyard Council have created sporting master plans for both Somerset and Wynyard, with indoor facility additions and improvements planned for both areas.

In line with these points a suggested alternate motion is as follows:

Alternate Motion:

“THAT Council broaden the scope the of feasibility study underway in relation to an indoor multi-sports facility to include consideration of Burnie, Wynyard and Somerset and report back to Council within six months with findings and recommendations.”

MOTIONS ON NOTICE**AO175-25 MOTION ON NOTICE - COUNCIL AMALGAMATIONS**

FILE NO: 15/5/2, 25/28353
PREVIOUS MIN:

Councillor Ken Dorsey has given notice that he would move the following motion at this meeting:-

“THAT Council:

- 1) Supports the principle that structural reform of local government in Tasmania, including council amalgamations, requires a democratic mandate from the Tasmanian electorate;***
- 2) Directs the General Manager to write to the Premier and Minister for Local Government requesting that the Tasmanian Government include a binding referendum question on the ballot paper for the next ordinary council elections, asking Tasmanian electors whether they support the implementation of council amalgamations;***
- 3) Requests that if the referendum returns a majority vote in favour of amalgamations, the Tasmanian Government commit to:***
 - i) Utilising the Future of Local Government Review Final Report (October 2023) and its amalgamation frameworks as the basis for implementation.***
 - ii) Implementing the amalgamations by the following council election cycle.***
 - iii) Ensuring adequate transition funding and support for affected councils.***
- 4) Authorises the Mayor and General Manager to publicly advocate for this position and engage with other councils who may support this approach.”***

COUNCILLOR’S COMMENTS**Democratic Principle**

Council amalgamations fundamentally reshape local government across Tasmania. Such significant structural changes require direct voter approval, not just government or council decisions.

Scale and Sustainability

Tasmania has extraordinary levels of political representation:

- 29 councils with 260+ councillors
- 50 state MPs (35 House of Assembly, 15 Legislative Council)
- 17 federal representatives (5 MPs, 12 Senators)
- Total: 327+ elected representatives
- 1 representative for every 1,250 voters (408,000 enrolled voters)
- One of the highest representation ratios in the world

The cost to ratepayers

Maintaining 29 separate councils means ratepayer money funds:

- 29 CEOs and senior management teams
- Duplicated administrative systems
- 260+ councillor allowances and meeting costs (Burnie Wynyard more than \$500,000)
- Multiple governance structures doing the same work

This money could instead deliver:

- Better roads and infrastructure
- Improved community facilities
- Enhanced services
- Lower rates

Burnie City Council recognizes that appropriate scale and consolidation could deliver better value for ratepayers, but such significant structural reform requires clear electoral support.

Regional Context

The North-West region needs better infrastructure, improved services, and greater financial sustainability. Appropriate structural reform could deliver these outcomes.

Existing Framework

The Local Government Board's Future of Local Government Review (October 2023) already contains:

- Substantial research and consultation
- Amalgamation frameworks
- A blueprint for reducing 29 councils to 15

A referendum would determine whether this existing work should be implemented, avoiding duplication, delay and surety.

Timing

Holding the referendum at the council election is cost-effective and ensures maximum voter participation.

Context

Tasmania: 550,000 people, 29 councils, 327 total elected representatives serving just 408,000 voters.

Tasmania's structure is disproportionate. The resources consumed by this representation could deliver better infrastructure and services to communities.

Clear Decision

A statewide referendum provides clear direction and removes ongoing political uncertainty. Councils can then plan strategically based on the outcome.

Electoral Mandate

Any amalgamation process requires clear voter approval rather than proceeding without direct community endorsement.

Background Context

The Review: The Local Government Board completed the Future of Local Government Review in October 2023 with 37 recommendations, including a blueprint to reduce Tasmania's 29 councils to 15.

The Problem: Voluntary amalgamations and shared services have failed. No substantive progress has been made despite years of discussion.

The Solution: A referendum lets Tasmanian voters decide whether structural reform should proceed.

Regional Benefits: This motion aligns with Burnie's commitment to regional collaboration, including shared facilities and services in the North-West.

GENERAL MANAGER'S COMMENTS

Councils most recent motion relating to amalgamation was at the last meeting, when it was determined (Item AO153-25):

"That Council endorses future reforms that call for amalgamations and further reductions and greater accountability".

That said, Council officers nor Councillors have discussed the subject of amalgamation at workshop in recent times, so views on this motion as proposed are unclear.

In response to each of the specific points raised in the motion -

1. *To support the principle that amalgamations require a democratic mandate from the Tasmanian electorate*

The Tasmanian Government has made a commitment that council boundaries will not change unless there is support from individual councils and their communities. Many Tasmanian Councils currently oppose forced boundary changes. In these circumstances, the government recommend a voluntary reform program and have expressed a willingness to work with councils and communities that are open to discuss amalgamation or boundary changes.

2. *Include a binding referendum question on the ballot paper for the next ordinary council elections, asking Tasmanian electors whether they support the implementation of council amalgamations*

Refer to the previous point. The *Future of Local Government*¹ report commented:

The Board commissioned the University of Newcastle to research Tasmanian community sentiment on local government, including potential reform. The research involved a survey of a representative sample of 1000 Tasmanians (the largest of its type ever undertaken in Tasmania on this topic) and focus group sessions with 148 people across all nine of the Board's 'community catchments'.

Overall, there is strong community support for reducing the number of councils in Tasmania and for improving the way councils work together and with other levels of government. However, many community members are cautious about change, and want to better understand the benefits and costs of reform before fully committing to amalgamations in particular.

Key insights from the research included:

- *Boundary adjustment: The majority of Tasmanians support the boundary adjustment of council boundaries in principle, but require more detailed information at a local, granular level for that support to extend to their specific council.*
- *Representation and identity: Opposition to boundary adjustment is a view held with significantly higher conviction than those supporting structural reform. This is especially the case in regional areas where loss of representation, local identity, and tailored service delivery remain key concerns.*
- *Cost of living: Current cost of living pressures had a high degree of influence on prevailing attitudes toward council reforms, and the impact of reform on rates and charges was the most commonly voiced concern in the focus groups.*
- *Shared services: Support for shared services across council boundaries is high and unqualified. However, there is limited consensus as to which specific services (such as waste management, administration, procurement, etc.) could be shared across councils, provided at a regional level, or delivered by a statewide agency.*

¹ Final Report on the **Future of Local Government Review** – the Tasmanian Government commissioned the Local Government Board to undertake a Review of the role, function, and design of local government in Tasmania. The Final Report was released in October 2023 and is publicly available online.

3. *Requests that if the referendum returns a majority vote in favour of amalgamations, the Tasmanian Government commit to a series of actions*

The government have the ability to implement action now should they choose but having undertaken the Future of Local Government Review, have formed a position of preferring a voluntary approach for willing councils and communities.

More broadly, the Future of Local Government report states:

We believe there is broad support and agreement in the sector and broader community on what councils need to be able to do well, and what it looks like when they are succeeding. We have identified – through an extensive program of research, analysis, and engagement – clear evidence that some councils will struggle to be able to deliver for their communities in line with this vision unless we make significant changes to how our current system of local government is structured and funded.

In summary, we found:

- our current system of 29 councils has a significant and detrimental impact on the ability of those councils to attract and retain key staff, to uniformly manage assets well, and to deliver important regulatory functions;*
- concerning capability gaps across the sector, driven in part by workforce and skills shortages, leading to sub-standard delivery of important regulatory functions and highly uneven asset management practices. These gaps and challenges are being felt more acutely in smaller, rural councils and are exacerbated by many councils competing for the same staff; and*
- at a strategic level, the competition, fragmentation, and duplication of effort which naturally occurs across so many different councils hinders collaborative effort and outcomes when it comes to effectively managing regional and state-wide challenges.*

Arguably, broad community sentiment is already understood. What Burnie Council has not done in recent times is actively engaged with neighbouring Councils or its own community to explore opportunities for reform and improvement in this region. There may be appetite for a voluntary process however this is not well understood. Indeed, the Future of Local Government report is not clear regarding the preferred position for Burnie in any amalgamation process.

As this area is not identified as a current annual plan action, perhaps forming and understanding the local position is a more appropriate position to start. This will be required regardless, to determine a preferred and clear position for future consideration.

MOTIONS ON NOTICE**AO176-25 MOTION ON NOTICE - TOURISM PROMOTION AND AMENITY
ENHANCEMENT AT BURNIE AIRPORT****FILE NO:** 15/5/2, 25/28355**PREVIOUS MIN:**

Councillor Justin Grave has given notice that he would move the following motion at this meeting:-

“THAT Council:

- 1) Engages collaboratively with West by North West and the Burnie Airport Corporation to explore opportunities to enhance tourism information and visitor amenity at the Burnie Airport terminal;***
- 2) As part of considerations, investigate the installation of high-quality visual tourism displays, which may include:***
 - i) Photographic panels or digital displays showcasing key natural attractions such as Fern Glade, Guide Falls Reserve and the Emu Valley Rhododendron Garden;***
 - ii) Historic imagery from Council’s Winters Collection, including early aviation and Burnie Airport history, to celebrate the region’s heritage and sense of identity; and***
- 3) Receives a report back outlining:***
 - i) Design concepts and display options (physical or digital);***
 - ii) Cost estimates and potential funding or partnership contributions;***
 - iii) Opportunities to integrate local tourism branding and business promotion; and***
 - iv) A proposed timeline for staged implementation aligned with airport operational needs and visitor peak periods.”***

COUNCILLOR’S COMMENTS

Burnie Airport is a key entry point for visitors to our region.

By working with West by North West and the Airport Corporation, Council has an opportunity to create a welcoming gateway that reflects our natural attractions and local identity. Visual displays of Fern Glade, Guide Falls, the Rhodo Gardens and historic images from our Winters Collection would immediately showcase Burnie’s unique character. A

modest investment in signage, photography and amenity could significantly improve visitor perception and increase connection to our attractions, supporting local business and tourism outcomes.

GENERAL MANAGER'S COMMENTS

The intent of this motion is supported.

Obviously, as a part owner, any decision regarding alterations to the airport terminal, is subject to consultation with the Burnie Airport Corporation (BAC) and Board.

It is understood the BAC are interested in improvements as outlined in this motion and in recent months have reached out to Council officers to commence discussion in this regard.

If this motion is approved, officers will prepare a report for consideration, including discussion with relevant stakeholders.

MOTIONS ON NOTICE**AO177-25 MOTION ON NOTICE - PROPERTY CONDITION/SAFETY REPORT -
234-236 MOUNT STREET, BURNIE****FILE NO: 15/5/2, 25/28432****PREVIOUS MIN:**

Councillor Trent Aitken has given notice that he would move the following motion at this meeting:-

“THAT Council request a Property Condition/Safety report from the owners of the property located at 234-236 Mount Street Burnie, known to locals as the Bells for Bargains Building.”

COUNCILLOR’S COMMENTS

Recently a large section of the Build has fallen off and ended up on the footpath, this included windows and brickwork. It was lucky that no one was injured or died as it is a busy intersection in Burnie.

GENERAL MANAGER’S COMMENTS

Any compliance action taken against a property owner must be undertaken strictly in accordance with the relevant legal framework and within the limits of Council’s statutory powers.

It is the understanding of the Council officers that Council may only initiate action against a property owner after a formal dilapidated building report has been completed and the building is confirmed to be dilapidated. Where Council initiates this process, the preliminary assessment must be both undertaken and funded by Council.

Section 241 of the *Building Act 2016* outlines the legal requirements for commencing enforcement action in relation to dilapidated buildings. Given the technical complexity and subjectivity involved in assessing a building’s condition, it is critical that Council is guided by a suitably qualified and accredited expert.

To initiate this process, Council may engage an accredited building surveyor, at Council’s cost to undertake a building condition report. This report should assess the current state of the building and identify any associated public safety risks.

Based on the findings of the report, the General Manager, acting on advice from the Permit Authority, can determine whether the building meets the criteria to be classified as

dilapidated. If so, a dilapidated building notice will be issued, and the standard building compliance procedures will then be followed.

Given that Council does not have the authority to request the actions proposed in the original motion, and in consideration of the points outlined above, the following alternative motion is suggested:

Alternate Motion:

“THAT Council engage an accredited building surveyor, at Council’s cost, to undertake a building condition report to determine the state of the building located at 234-236 Mount Street Burnie, and any associated public safety risks.”

QUESTIONS WITHOUT NOTICE**AO178-25 COUNCILLOR QUESTIONS WITHOUT NOTICE****FILE NO: 15/5/5**

Councillor Questions without Notice

Questions without notice may be asked by councillors, in accordance with Regulation 34 of the *Local Government (Meeting Procedures) Regulations 2025*.

The Regulation provides that a councillor may ask a question of the chairperson, another councillor, or the general manager - Reg 34(1).

In asking a question without notice at a meeting, a councillor must not offer an argument or opinion, or draw any inference or make any imputations, except so far as may be necessary to explain the question - Reg 34(2).

The chairperson must not permit any debate of a question without notice, or its answer - Reg 34(3).

The chairperson, councillor or general manager who is asked a question without notice at a meeting may decline to answer the question - Reg 34(4).

The chairperson may refuse to accept a question without notice - Reg 33(3) - if it is:

- defamatory;
- contains offensive language;
- is unlawful;
- does not relate to the activities of the Council; or
- does not meet the requirements specified in Reg 33(2).

The chairperson must give reasons for the refusal - Reg 33(4).

The chairperson may require a councillor to put a question without notice in writing – Reg 34(5).

OFFICERS' REPORTS**AO179-25 PROPOSAL FOR DISPOSAL OF PUBLIC LAND - BRICKPORT ROAD
(CT 156273/40)**

FILE NO: 5/2/2, 3198526

PREVIOUS MIN:

1.0 RECOMMENDATION:***"THAT Council, by absolute majority:***

- 1) Determines its intention to dispose of a parcel of public land for the purpose of sale, located at Brickport Road, Burnie, CT Volume 156273 Folio 40; and***
- 2) Requires that a further report be presented to Council as to the outcome of the public submission process before proceeding to a sale process."***

2.0 EXECUTIVE SUMMARY**Purpose and Background**

At the Council meeting held on 23 September 2025, Council put forward a report recommending that Council determine its intention to dispose of four parcels of public land, two of which were located at Brickport Road (AO155-25). This public space has an additional third title which Council has identified at the below location which it intends to prepare for future sale:

- Brickport Road, Burnie, CT Volume 156273, Folio 40.

The proposed parcel is listed on Council's Public Land Register and is therefore subject to the legislative process for disposal of public land. The purpose of this report is to commence that process.

A map of the parcel is attached to this report.

Key Issues

Council must undertake a disposal of public land process which provides for a public notice period, and for any submissions to be considered before it can dispose.

The potential sale of this land presents an important opportunity for Council to facilitate additional residential development within the municipality and help address the growing demand for housing in Burnie.

3.0 SUPPORTING INFORMATION

Strategic Alignment

This report aligns with Council's Strategic Plan Goal 2: Making liveable local communities; and Goal 3: Making sustainably built and natural environments.

Legal

Section 20 (5) of the *Local Government Act 1993* provides that Council can acquire, hold, dispose of and otherwise deal with property.

Where land is specifically classified as public land on Council's Public Land Register, Section 178 of the *Local Government Act 1993* requires that Council must undertake a disposal of public land process. This provides for a public notice period, and for any submissions to be considered before it can dispose of the land to a potential purchaser.

Guidance on the processes for the disposal of public land is provided in the attached 'Sale of Public Land' information sheet produced by the Local Government Division.

Finance

The Act requires that a valuation for land by a registered valuer be obtained before Council disposes of public land. If the recommendation is passed Council will commence a commercial valuation for the land which will be provided to Council when available.

Sale of this land will increase cash reserves for Council, reduce ongoing operating maintenance costs and will provide an opportunity for future rating revenue.

Relation to Council Policy

There are no Council policies relating to this report.

Discussion

Burnie is currently experiencing significant demand for housing, including social and affordable housing options. The land identified in this report is already zoned as General Residential, making it well-suited to support the development of new homes.

Furthermore, with the introduction of major infrastructure projects such as the Marinus Link, the demand for housing in the Burnie area is expected to increase in the coming years.

The nominated land aligns with the Burnie Settlement Strategy, which provides key guidance for Council when considering the use of land. Relevant principles include:

- Promoting established settlement areas as the focus for growth and development.
- Facilitating choice and diversity in the location, form, and type of housing.

- Ensuring the sustainable use or development of land in accordance with its capability, to deliver the greatest economic and social benefit to the region's communities at minimal cost to natural values.

In addition, the Burnie City Council Playground Strategy highlights the need to consolidate open space, parks, and reserves to enable the development and enhancement of key recreational areas across the municipality.

The public submission process will provide the public with an opportunity to raise any concerns or objections, before a report is brought back to Council to make a decision on disposal.

Should the council make a future decision to dispose of the public land, the parcel will be subject to an open market process.

Health and Safety

There are no health and safety impacts resulting from this report.

Risk

Financial – If council does not proceed with the intention to sell, council will continue to incur operational costs for the maintenance of the identified land.

Strategic – The Cradle Coast Regional Land Use Strategy states that land supply within urban settlements should be sufficient to meet projected demand over a period of no less than 10 years and no more than 20 years.

Current population projections and dwelling demand scenarios indicate that the Burnie Local Government Area (LGA) has approximately 23 to 34 years of residential land supply. If Council does not actively consider the disposal of surplus, Council-owned residential land to reduce this figure, the rezoning of additional land to General Residential is likely to remain inconsistent with regional planning policy and, therefore, unlikely to be supported.

Environmental Sustainability

There are no environmental sustainability impacts to be considered in this report.

Consultation

In regard to the public land disposal process, the following steps need to occur:

- a) Council determines its intention to dispose of the subject land.
- b) Council's determination is published as a public notice, twice, in a local newspaper advising interested parties of Council's intention to dispose of the land.
- c) A notice is required to be placed at the property boundary advising of Council's intention to dispose of the land.

- d) Submissions are invited from interested parties in respect of Council's intention to dispose of the subject land.

Any submissions received within the specified time frame will be considered by Council following the completion of the statutory period.

The Act contains appeal provisions, should a person who lodges a submission, be aggrieved by the subsequent Council decision.

Conflict of Interest

No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

ATTACHMENTS

- 1 [↓](#). Parcel of Land CT 156273/40
- 2 [↓](#). Sale of Public Land Fact Sheet



SALE OF PUBLIC LAND

Information Sheet
21 April 2020

Introduction

The purpose of this information sheet is to provide guidance to councils on the process that must be followed when selling, exchanging or disposing of council owned public land.

As a general principle, decisions to sell, exchange or dispose of public land should be made in the best interests of the council and the community.

This information sheet does not cover the legislative requirements relating to the sale and disposal of land that is not public land, which is covered under section 177 of the *Local Government Act 1993* (the Act).

Power to deal with property

Under subsection 20(5) of the Act, a council is able to acquire, hold, dispose of and otherwise deal with property.

Furthermore, section 175 provides that a council may purchase or lease land for any purpose which it considers to be of benefit to the council or the community.

Legislative requirements

Section 178 of the Act requires a council to do certain things before selling, exchanging or disposing of public land owned by it.

In essence, the Act requires councils to notify the community regarding any proposal to sell public land and provide the community with an opportunity to have their views heard.

What is public land?

Section 177A of the Act provides that the following land owned by a council is public land:

- a public pier or public jetty;
- any land that provides health, recreation, amusement or sporting facilities for public use;
- any public park or garden;
- any land acquired under section 176 of the Act (land acquired by a council for prescribed purposes in accordance with the *Land Acquisition Act 1993*) for the purpose of establishing or extending public land;
- any land shown on a subdivision plan as public open space that is acquired by a council under the *Local Government (Building and Miscellaneous Provisions) Act 1993*;
- any other land that the council determines is public land; and
- any other prescribed land or class of land.

Under subsection 178(2), public land that is leased for any period by a council remains public land during that period.

Department of Premier and Cabinet
Local Government Division



Lease of public land for less than five years

Under section 179 of the Act, a council does not have to comply with the requirements of section 178 (sale, exchange and disposal of public land) if leasing public land for a period of up to five years.

Maintaining a public land register

Under subsection 177A(2) of the Act, the general manager of a council is to keep lists or maps of all public land within the municipal area and ensure that the lists/maps are available for public inspection at any time during normal business hours.

Deciding to sell public land – Open meeting and by absolute majority

Under subregulation 15(3)(c) of the *Local Government (Meeting Procedures) Regulations 2015* (the Regulations), a council must not close a meeting when it is considering proposals for the council to deal with public land under section 178 of the Act.

It is important that a council considers proposals relating to public land in open session so that the community has access to the relevant details associated with any proposal, including the decisions made by the council and the reasons behind those decisions.

As per subregulation 15(4) of the Regulations, the only instance in which a council may consider proposals to deal with public land in closed session is when it is considering a matter relating to actual or possible legal action taken by, or involving, the council.

Under subsection 178(3) of the Act, a resolution of a council to sell, lease, donate, exchange or otherwise dispose of public land is to be passed by an absolute majority¹.

Public notification of intent to sell public land and the public's right to object

Subsection 178(4) of the Act requires the general manager to notify the public of the council's intention to sell, lease, donate, exchange or otherwise dispose of public land.

In particular, the general manager is to:

- publish that intention on at least two separate occasions in a daily newspaper circulating in the municipal area;
- display a copy of the notice on any boundary of the public land that abuts a highway; and
- notify the public that objection to the proposed sale, lease, donation, exchange or disposal may be made to the general manager within 21 days of the date of the first publication.

Leading practice - Public notice of intent to sell public land

As leading practice, the public notice should be informative and enable the community to:

- clearly identify the land proposed for sale/exchange/disposal;
- understand the proposed sale/exchange/disposal process; and
- understand their right to object to the proposal and that any such objections will be duly considered by the council.

Furthermore, the public notice should include the following information:

- references to the relevant sections of the Act that the council is acting under (eg section 178 (sale, exchange and disposal of public land) and 178A (appeal);
- the specific location of the land (for example the street/property address and title details);
- a map/plan of the land;
- how the land/property is proposed to be sold, exchanged or disposed of;

1. 'absolute majority' – more than half the elected councillors, whether or not any of them are present at a council meeting'

- the timeframe for proposed sale/exchange/disposal of land;
- the timeframe for public objections and how such objections can be made; and
- how to contact the council for further information.

Considering public objections to proposed sale of public land

Subsection 178(4)(b) of the Act provides that a person may object to a council's intent to sell, lease, donate, exchange or otherwise dispose of public land.

Under subsection 178(6), a council must consider any such objections received and decide whether or not to take any action regarding the objection.

Furthermore, under subsection 178(6), within seven days of making a decision regarding an objection, a council must write to the objector to notify them of the council's decision as to whether any action will be taken.

This written notification must advise the person of the right to appeal against the council's decision under section 178A.

A person's right to appeal a council decision regarding an objection

Under section 178A of the Act, a person who lodged an objection regarding a council's intention to sell or exchange public land (under section 178) may appeal to the Resource Management and Planning Appeal Tribunal (RMPAT) against the council's decision regarding the objection made under subsection 178(6).

Such appeals must be made to RMPAT within 14 days after receiving written notification of the council's decision and must be in accordance with the *Resource Management and Planning Appeal Tribunal Act 1993* (RMPAT Act).

Furthermore, an appeal under section 178A may only be made on the ground that the decision of the council is not in the public interest in that:

- the community may suffer undue hardship due to the loss of access to, and the use of, the public land; or
- there is no similar facility available to the users of that facility.

Possible outcomes of RMPAT appeal

Under section 178B of the Act, upon hearing an appeal against a decision of a council regarding a public objection to a council's intent to sell/dispose of public land, RMPAT may:

- confirm the council's decision; or
- set aside the council's decision; or
- set aside the council's decision and
 - substitute it for another decision; or
 - remit the matter to the council for reconsideration.

As per subsection 178A(5), a decision of RMPAT on hearing an appeal is final and section 25 of the RMPAT Act, which provides that a party to an appeal before RMPAT may appeal to the Supreme Court on a question of law, does not apply.

What if no public objections are made?

Under subsection 178(5) of the Act, if the general manager does not receive any public objections under subsection 178(4) and an appeal under section 178A is not made, then the council may sell, lease, donate, exchange or otherwise dispose of public land in accordance with its public notification of intent under subsection 178(4).

**Instances in which a council must not
decide to take action to sell,
exchange and dispose of public land**

Under subsection 178(7) of the Act, a council must not decide to take any action under section 178 if:

- any public objection regarding the sale of public land is being considered by the council;
- an appeal under section 178A has not yet been determined by RMPAT; or
- RMPAT has made a determination regarding an appeal under subsection 178B(b) or (c).

Disclaimer: Information on legislation contained in this document is intended for information and general guidance only. Such information is not professional legal opinion.

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OFFICERS' REPORTS**AO180-25 FINANCE REPORT - SEPTEMBER 2025**

FILE NO: 4/18/2
PREVIOUS MIN:

1.0 RECOMMENDATION:

“THAT Council note the Finance Report, as attached.”

2.0 EXECUTIVE SUMMARY**Purpose and Background**

To provide a financial update, summarising the financial position of Council on a quarterly basis.

Key Issues

The financial reports included are:

- Summary Financial Statements
- Summary Capital Budget
- Investments
- Rates Summary
- Receivables Summary

Council's year to date performance against budget is overall favourable. There are several favourable and unfavourable variances against year to date budget, with revenue being higher than budgeted by \$628k, and expenditure being lower than budget by \$1.061m mostly due to timing variance. By year end Council is anticipated to finish within budget. A more detailed forecast will be provided as part of the next quarterly report.

Further commentary in the attached report.

3.0 SUPPORTING INFORMATION**Strategic Alignment**

This report aligns to Council's Strategic Plan Enabler: Financial Management.

Legal

Section 82 of the *Local Government Act 1993* requires every Council to adopt its Annual Plan and Budget Estimates no later than 31 August each year. These special purpose financial reports are prepared under *Australian Accounting Standards* and the *Local Government Act 1993*.

Finance

The attached reports include commentary on relevant financial impacts.

Relation to Council Policy

Council maintains the following relevant financial policies:

- Asset Management Policy
- Code for Tenders and Contracts
- Debt Management Policy
- Financial Hardship Policy
- Fraud and Corruption Control Policy
- Purchasing Policy
- Rates and Charges Policy
- Treasury Policy

All policies are publicly available online at www.burnie.tas.gov.au

Discussion

Refer to the attachments for detailed information on the following statements:

- Summary Financial Statements
- Summary Capital Budget
- Investments
- Rates Summary
- Receivables Summary

Budget Estimate variances under delegation

The General Manager has a delegation from Council to approve minor adjustments up to \$200,000 to any individual estimate item (including capital works) as deemed necessary during the 2025-26 financial year provided that the total of estimates remains unaltered (Item AO101-25, dated 24 June 2025).

There are no variance items exceeding \$200,000 for any individual estimate item, requiring a council decision.

Health and Safety

There are no health and safety impacts resulting from this report.

Risk

This Finance Report provides information relating to financial management. The commentary in attached reports will highlight any key financial risks.

Where financial risks exist, there can be flow on impacts to assets and infrastructure, service delivery, strategic and reputational risks. Conversely, sometimes financial resources are required in order to address and reduce other primary risks that emerge.

Council's Strategic Risk Register monitors the following key risk, which is described as follows:

Financial Sustainability	The ability of the Council to manage the financial sustainability of the Council and deliver the services expected by the community, by adequately managing the revenue base, maintenance and provision of renewal of infrastructure, and the regulatory roles.
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A number of controls are in place to manage this risk, and Council commits to undertaking a number of actions each year in the Register, which are aimed at further improving financial outcomes. The Strategic Risk Register is reviewed annually by Council.

Environmental Sustainability

There are no environmental sustainability impacts to be considered in this report.

Consultation

Relevant Council Officers have been consulted in the preparation of this report.

Conflict Of Interest

No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

ATTACHMENTS

1 [↓](#). Finance Report - September 2025



Finance Report

2025-26 | Quarter 1

SUMMARY FINANCIAL STATEMENTS

September 2025 YTD Comprehensive Income Statement

	Note	YTD Budget 2026 \$'000	YTD Actual 2026 \$'000	YTD Variance to Budget \$'000	Annual Budget 2026 \$'000
Recurrent Income					
Rates and charges	1	32,490	32,729	239	32,615
Statutory fees and fines	2	297	361	64	983
User fees	3	1,027	1,175	148	4,264
Operational Grants		487	485	(2)	3,984
Interest	4	142	389	247	443
Other income		258	259	1	778
Investment income		207	228	21	828
Reimbursements	5	143	53	(90)	300
Total recurrent income		35,051	35,679	628	44,194
Recurrent Expenses					
Employee benefits	6	3,024	2,626	(398)	12,086
Materials and services	7	3,880	3,491	(389)	14,734
Depreciation and amortisation	8	3,152	2,952	(200)	12,608
Impairment losses on receivables		-	-	-	34
Finance costs		-	-	-	270
Other expenses	9	1,268	1,193	(75)	4,241
Profit/(Loss) on disposal of assets		-	-	-	400
Total recurrent expenses		11,324	10,262	(1,061)	44,374
Operating surplus/(deficit) before capital items		23,728	25,417	1,689	(180)
Capital Items					
Capital Grants	10	5,823	-	(5,823)	23,291
Contributions - non-monetary assets		-	-	-	-
Share of Profit of Joint Venture		-	-	-	292
		5,823	-	(5,823)	23,582
Results from Continuing Operations		29,550	25,417	(5,823)	23,403
Other Comprehensive Income					
Net asset revaluation increment/(decrement)		-	-	-	17,459
Total Other Comprehensive Income		-	-	-	17,459
Total Comprehensive Result		29,550	25,417	(5,823)	40,862
Operating Margin		3.10	3.48		1.00
Consolidated underlying surplus/(deficit) related to performance					
Recurrent Income					
Total income from continuing operations					67,776
Less non operating income					
Capital grants					(23,291)
Contributions - non-monetary assets					-
Revenue from Constructed Assets					-
One off disposals					-
Adjustments to timing of Financial Assistance Grants					(63)
Recurrent Income			-		44,423
Recurrent Expenditure					44,374
Less non-operational expenditure					
Capital works completed on assets not owned by Council					-
Recurrent Expenditure			-		44,374
Adjusted Underlying surplus or (deficit)			-		49

OPERATING RESULT

Note 1 (Rates and charges) – Council has earned \$239,000 more than budget year to date to initial rate run revenue being higher than expected, and supplementary rates including service charges.

Note 2 (Statutory fees and fines) – Due to an increase in the size of recent development applications fee revenue has increased above budget for the period.

Note 3 (User fees) - The final stages of the Burnie Arts redevelopment has allowed for early usage of parts of the redeveloped site resulting in \$134,000 in revenue being earned in advance. This represents the bulk of the year to date variance.

Note 4 (Interest) - Council received at the end of 2024/2025 early payment for Government Funded Stage 1 Urban Plan redevelopment funding of \$8.650m in part contributing to a higher-than-expected cash account balance at the beginning of the year. As a result higher than budgeted interest of \$247,000 year to date.

Note 5 (Reimbursements) - Year to date reimbursements are below budget due to timing of insurance recoveries from community organisations.

Note 6 (Employee benefits) – Employee benefits are below budget predominantly due to vacancies and reductions in annual leave and long service leave provisions.

Note 7 (Materials and services) – Below budget mainly due to timing for expenditure in engaging contractors, seasonality of event sponsorship and strategic initiatives.

Note 8 (Depreciation) – Depreciation variance year to date due to the impact of carry-forward projects and a lower than forecast capitalisation in the prior year.

Note 9 (Other expenses) – Variance due to lower uptake of the rates early payment discount by \$66,000, and timing of Council utilities expenses.

Note 10 (Capital grants) – Below budget due to timing of expected revenue recognition from completed capital works projects.

STATEMENT OF FINANCIAL POSITION
Statement of Financial Position

	YTD Actual	Budget
	2026 \$'000	2025 \$'000
Assets		
Current assets		
Cash and cash equivalents	43,482	10,931
Trade and other receivables	15,999	3,759
Investments	-	-
Prepayments	833	633
Inventories	105	71
Contract Assets	305	1,325
Total current assets	60,724	16,720
Non-current assets		
Investment in water corporation	71,542	70,172
Investment in joint venture	3,424	3,358
Investment in subsidiaries	-	-
Property, infrastructure, plant and equipment	572,559	591,030
Right of Use Assets	19	18
Total non-current assets	647,543	664,578
Total assets	708,268	681,298
Liabilities		
Current liabilities		
Trade and other payables	4,891	7,012
Trust funds and deposits	106	78
Lease Liabilities	1	1
Contract liabilities	24,370	10,135
Provisions	2,073	2,390
Interest-bearing loans and borrowings	1,048	1,048
Total current liabilities	32,489	20,664
Non-current liabilities		
Provisions - NC	4,410	4,411
Lease Liabilities - NC	16	16
Interest-bearing loans and borrowings - NC	4,388	4,388
Total non-current liabilities	8,813	8,814
Total liabilities	41,302	29,478
Net Assets	666,966	651,819
Equity		
Reserves	326,767	335,471
Surplus / (deficit)	25,417	22,338
Accumulated surplus / (deficit)	314,783	294,010
Total Equity	666,966	651,819
CURRENT RATIO	1.87	0.81

STATEMENT OF CASHFLOWS
Burnie City Council
Estimated Statement of Cash Flows

	Actual YTD 2025	Revised Budget 2025
	\$'000	\$'000
Cash flows from operating activities		
Rates	20,617	30,769
Statutory fees and fines	355	970
User fees (inclusive of GST)	1,662	4,788
Operational Grants	481	2,615
Reimbursements (inclusive of GST)	59	283
Rents (inclusive of GST)	280	722
Other receipts (inclusive of GST)	6	248
Interest	350	808
Investment revenue from Water Corporation	228	828
Distributions from Joint Venture	83	61
Payments to employees	(2,944)	(11,404)
Payments to suppliers	(4,092)	(15,789)
Finance Costs paid	-	(258)
Other payments	(1,312)	(4,433)
Net GST refund / (payment)	309	1,289
Net cash provided by (used in) operating activities	16,082	11,497
Cash flows from investing activities		
Payments for property, infrastructure, plant and equipment	(2,856)	(40,435)
Proceeds from sale of property, plant, infrastructure and equ	-	-
Capital grants	(0)	25,957
Receipts from maturity of short term investments	-	-
Funds deposited as short term investments	-	-
Net cash provided by (used in) investing activities	(2,856)	(14,478)
Cash flows from financing activities		
Proceeds from trust funds and deposits	(273)	-
Repayment of Lease Liabilities	-	-
Repayment of interest bearing loans and borrowings	-	(1,029)
Net cash provided by (used in) financing activities	(273)	(1,029)
Net increase (decrease) in cash and cash equivalents	12,952	(4,010)
Cash and cash equivalents at the beginning of the financial year	30,530	14,941
Cash and cash equivalents at the end of the period	43,482	10,931

SUMMARY CAPITAL BUDGET

The summary capital budget provides a high level view of Council's financial spend against capital works budgets at the end of the reporting period. Year to date council has expended 18% of the annual capital works budget and is currently on track for completion at year end.

	Original Budget 2026	Actual- YTD- Sep 2025
Bridges & Culverts	501,924	2,556
Bridges	501,924	2,556
Buildings	2,316,282	729,933
Burnie Greens Complex	30,641	-
Burnie Arts & Function Centre	155,641	652,447
Sporting Grounds Admin	2,130,000	77,487
Furniture & Fittings	50,000	25,214
Governance and Strategic	50,000	25,214
ICT	555,793	37,608
IT Services	555,793	37,608
Parks and Reserves	4,968,288	368,015
Burnie Lawn Cemetery	6,412	-
Romaine Park	242,442	182,291
General Parks & Reserves	3,208,793	185,724
Hilder Parade	1,510,641	-
Stormwater	343,602	27,818
Stormwater	343,602	27,818
Transport	3,907,499	334,659
Rural Roads	801,977	5,236
Urban Roads	3,105,522	329,424
Waste	800,321	728,371
Waste Management Centre	800,321	728,371
Works and Depot Overhead	2,209,614	552,403
Works and Depot Overhead	2,209,614	552,403
Grand Total	15,653,322	2,806,577

INVESTMENTS

The schedule of Council's investments as at 30 September 2025 is below:

Investments Schedule as at 30/09/25						
<u>Institution</u>	<u>Term</u> <u>(days)</u>	<u>Annual Interest</u> <u>Rate</u>	<u>S&P Rating</u>	<u>Lodgement Date</u>	<u>Maturity</u>	<u>Amount</u>
CBA	At Call	3.50%	A1+			24,492,222
CBA	At Call	3.50%	A1+			2,147,936
						<u>26,640,158</u>
Investment Allocation by Credit Rating						
<u>Credit Rating</u>	<u>Policy Max Weighting</u>	<u>Investment %</u>	<u>Investment</u> <u>Amount</u>	<u>WAIR</u>		
A1+	100%	100%	\$26,640,158	3.50%		
A1	80%	0%	\$0	0.00%		
A2	50%	0%	\$0	0.00%		
		<u>100%</u>	<u>\$26,640,158</u>	<u>3.50%</u>		
Investment Allocation by Bank						
<u>Bank</u>	<u>Credit Rating</u>	<u>Policy Max Weighting</u>	<u>Investment %</u>	<u>Investment</u> <u>Amount</u>	<u>WAIR</u>	
CBA	A1+	100%	100%	\$26,640,158	3.50%	
			<u>100%</u>	<u>\$26,640,158</u>	<u>3.50%</u>	

Council's Treasury Management Policy CP-CBS-SG-038 sets the parameters for management of Council's investment portfolio.

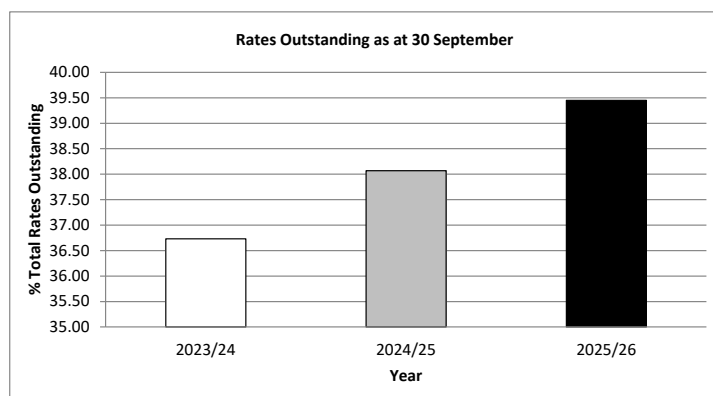
Funds are invested in a manner that allows Council to earn interest on community funds for as long as possible while retaining flexibility in accessing those funds for Council operations. The above investment excludes funds in Council's general working account.

The primary tool for deciding how much and how long to invest is the cash flow budget. A contingency of funds is retained in an interest bearing at call account to ensure funds are available to meet the Council's commitments.

RATES SUMMARY

Rates Outstanding as at 30/09/2025

	This Financial Year 30 Sep 2025		Last Financial Year 30 Sep 2024		Change
		\$		\$	\$
Arrears Brought Forward as at July 1	7.83%	2,687,852	7.93%	2,544,700	143,152
Credit Brought Forward	-3.35%	(1,149,306)	-3.33%	(1,067,267)	(82,040)
Legal Fees	0.00%	-	0.00%	681	(681)
Add Current Rates & Charges Levied	95.06%	32,620,774	95.25%	30,547,511	2,073,263
Supplementary Rates	0.32%	108,705	0.00%	43,986	108,705
Penalty	0.14%	48,052	0.14%	-	4,066
Gross Rates and Charges Demanded	100.00%	34,316,077	100.00%	32,069,611	2,246,466
Less: Rates & Charges Collected	56.97%	19,551,330	58.18%	18,656,611	894,719
Pension Remission	2.61%	895,239	2.69%	864,157	31,082
Residential Waste Remission	0.07%	24,976	0.08%	25,258	(282)
Hardship Interest Remission	0.00%	-	0.00%	-	-
Private Conservation	0.00%	665	0.00%	665	-
Misc Remissions	0.00%	-	0.00%	-	-
Services Remissions	0.00%	-	0.00%	-	-
Stormwater Remission	0.04%	13,058	0.04%	14,028	(970)
General Rate Remission	0.00%	-	0.00%	-	-
- Legal Fees	0.00%	-	0.00%	-	-
- Discounts	0.83%	285,588	0.92%	293,709	(8,121)
- Roundings/Adjustments	0.02%	6,175	0.02%	6,973	(798)
Sub Total	60.55%	20,777,030	61.93%	19,861,401	915,629
Unpaid Rates & Charges as at 30/09	39.45%	13,539,047	38.07%	12,208,210	1,330,837



The above rates analysis contains a summary of rating transaction movements for 2025-26 including levied, the total paid and the total unpaid at the end of each reporting period. Rates are levied in July each year. Outstanding rates as a percentage is marginally higher than the same time last year. As this corresponds with a reduction in the uptake of the early payment discount more rate payers have switched to instalments rather than upfront payment in full.

RECEIVABLES SUMMARY

The receivables analysis summarises all current amounts owed to the Council as at the end of the reporting period.

Receivables Analysis as at 30 September 2025

	<u>Total</u>	<u>Current - 30 Days</u>	<u>60 Days</u>	<u>90 Days</u>	<u>120+ Days</u>
Trade Debtors					
Sundry Debtors	100,328	41,519	5,378	7,774	45,657
Reserved Parking Spaces	5,720	(1,336)	270	115	6,672
Burnie Venues & Catering	40,230	24,481	4,114	7,886	3,748
Lease Debtors	2,193	2,193	-	-	-
ICT Debtors	116,664	63,494	-	21,391	31,779
Waste Debtors	128,933	125,308	46	74	3,505
Total Trade Debtors	394,068	255,659	9,808	37,240	91,362
Goods & Services Tax	145,860				
Infringements & Parking	701,208				
Other Receivables	413,396				
Rates & Charges	14,862,868				
Allowance for Impaired Debts	(518,863)				
Total Receivables	15,998,538				

Infringements & parking

	Sep-25		Sep-24		Movement	
	Count	Balance	Count	Balance	Count	Balance
Issued 2025	717	45,566				
Issued 2025	1291	105,699	854	51,652	437	54,047
Issued 2024	704	62,574	1,163	126,888	(459)	(64,314)
Issued 2023	638	56,141	765	72,787	(127)	(16,646)
Issued 2022	981	72,393	999	75,092	(18)	(2,698)
Issued 2021	685	47,459	698	49,883	(13)	(2,424)
Issued 2020	536	39,993	591	47,367	(55)	(7,375)
Issued 2019	503	34,884	526	39,094	(23)	(4,209)
Issued 2018	583	23,558	601	26,811	(18)	(3,253)
Issued 2017	1,532	21,890	1,536	23,655	(4)	(1,765)
Issued 2016	854	19,039	869	21,049	(15)	(2,010)
Issued 2015	410	21,608	423	23,010	(13)	(1,401)
Issued 2014	434	25,791	435	26,665	(1)	(874)
Issued 2013	529	30,235	555	32,114	(26)	(1,880)
Issued 2012	431	24,716	433	24,828	(2)	(112)
Issued 2011	439	25,025	443	25,607	(4)	(582)
Issued 2010	541	17,404	564	19,071	(23)	(1,667)
Issued Pre-2009	1,215	34,695	14,667	655,076	(13,452)	(620,380)
	12,306	708,669	26,122	1,340,647	(13,816)	(677,544)

In 2024-2025 Council wrote-off \$590,585 in uncollectable parking infringements. This represents the bulk of the movement in balance owed. Total receivables are in line with expectations for the time in the year.

OFFICERS' REPORTS**AO181-25 GOVERNANCE REPORT - SEPTEMBER 2025**

FILE NO: 4/18/2
PREVIOUS MIN:

1.0 RECOMMENDATION:

"THAT Council note the Governance Report, as presented."

2.0 EXECUTIVE SUMMARY**Purpose and Background**

To provide a governance update on matters relating to councillor activities and reportable governance information.

Key Issues

The governance items included in this report are:

- Council workshops
- Councillor attendance
- Meetings and events attended by the Mayor or their delegate
- Meetings and events attended by the General Manager
- Committees and working group meetings held
- Community support approved under delegation
- Use of the common seal
- Council meeting action list

3.0 SUPPORTING INFORMATION**Strategic Alignment**

This report aligns to Council's Strategic Plan Goal 5 – Making democratic and engaged communities.

Legal

There are no direct legal implications or legislative requirements for Council arising from the recommendation in this report.

Finance

There are no financial impacts resulting from this report.

Relation to Council Policy

There are no Council policies relating to this report.

Discussion

All listed matters are reported under *Detail* below.

Health and Safety

There are no health and safety impacts resulting from this report.

Risk

There are no identified risks that require consideration in relation to this report.

Environmental Sustainability

There are no environmental sustainability impacts to be considered in this report.

Consultation

Relevant Council Officers have been consulted in the preparation of this report.

Conflict Of Interest

No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

4.0 DETAIL**Council Workshops**

As required by regulation 10(3)(c) of the *Local Government (Meeting Procedures) Regulations 2025*, the General Manager advises that the following workshops were held since the last Council Meeting report:

Workshop	7 October 2025
Councillors in attendance	Mayor Brumby, Deputy Mayor Simpson, Cr Boyd, Cr Dorsey, Cr Grave, Cr Keygan, Cr Lynch, Cr Pease
Apologies	Cr Aitken
Items Discussed	Waste Management Policy, Draft Active Transport Presentation, General Manager Performance Review process determination

Workshop	14 October 2025
Councillors in attendance	Deputy Mayor Simpson, Cr Aitken, Cr Boyd, Cr Dorsey, Cr Grave, Cr Keygan, Cr Lynch, Cr Pease
Apologies	Mayor Brumby
Items Discussed	Unrecoverable Parking Infringements, Land Disposal-Transfer of Land, Business North West Presentation, Council Reporting – Snap Send Solve

Councillor Attendance

The following table indicates councillor attendances since the commencement of the current financial year:

Councillor Attendance 1 Jul 2025 to 19 Oct 2025	Ordinary Meetings (YTD: 3)	AGM / Special Meetings (YTD: 0)	Workshops (YTD: 7)	Meetings not attended under Leave of Absence
Mayor Teeny Brumby	2		5	
Deputy Mayor Giovanna Simpson	2		5	
Cr Trent Aitken	3		2	
Cr Alwyn Boyd	3		7	
Cr Ken Dorsey	3		6	
Cr Justin Grave	3		7	
Cr Amina Keygan	3		7	
Cr Chris Lynch	3		6	
Cr David Pease	3		7	

Meetings and events attended by the Mayor or their delegate

The Mayor advises that the following meetings, events or appointments were attended since the last Council Meeting report:

Description
Lifeline Tasmania
LGAT Mayor's Workshop
Citizenship Ceremony
Burnie Arts Tour with Anne Urquhart MP and Susan Templeman MP

Description
Launch of the Rosetree Café – Rhododendron Gardens
Burnie High School Musical - Once Upon a Time
TastroTalk - Eric Philips OAM - Astronaut - Presentation to Primary School students
Burnie Works
ABC Takeover Burnie - Red Carpet Cinema Screening
Early Childhood Education and Care Roundtable hosted by Anne Urquhart MP and Senator the Hon Jess Walsh
ABC Takeover – Judge on Panel
Burnie Youth Council Meeting
Burnie Emu Bay Lions Club – Judging Montello Primary School Artwork
ABC Takeover - Ideas Presentation
Cinema Screening - ABC Takeover Burnie
Cradle Coast Authority CEO - Philip Reid
Ivor Wiseman 100 th Birthday
Beacon - Emily Bridle
Dashain 2025 - Special Guest
SeaFM Radio Interview with Patty and Danni
ReCFIT - CEO Vanessa Pinto
Seniors Week Generations Connect - Fun Day
NW Arch Ministerial Media Event
Burnie Show and Agri-Expo – Opening of Show and Judging
TASART 2025 – Opening Night
Umina Park - Road Worlds for Seniors
Aboriginal Education Holiday Program
Safe City Roundtable
Bicentennial Committee
Ignite Annual Wellness Day - Mayor led morning swim
Indie School Burnie – School Assembly
Monday Mayor with Leon Compton ABC Radio
Youth Justice Acknowledgement Day BBQ
Secretary Department of State Growth
7BU Radio Interview with Leigh Kenworthy
Ambulance Tasmania North West Region - Awards and Recognition Ceremony
Burnie City Centre Urban Plan Working Group Meeting
Combined Churches
Burnie Musical Society - Opening Night - Come from Away
Opening Night 7BU: Celebrating 90 Years on the Air Exhibition
Emu Valley Rhododendron Garden Spring Festival
MakerX - STEM Hub - Cradle Coast Open Day

Meetings and events attended by the General Manager

The General Manager advises that the following meetings, events or appointments were attended since the last Council Meeting report:

Date	Description
15 September	Business North West
	Audit Tasmania – Performance Audit follow up review
16 September	Community member – via phone
17 September	Active Tasmania – South Burnie Bowls Club
18 September	Local Government Professionals Tasmania Conference
19 September	Local Government Professionals Tasmania Conference
22 September	Community member – via phone
23 September	Burnie Works
	Regional Development Australia – Tasmania – James McKee
24 September	ASU Organiser NW Tasmania
	Community Sporting Group Forum
25 September	LGBTQIA+ Working Group
26 September	SeaFM – New Years Eve event
	Department of State Growth regarding Ridgley Highway Corridor Plan

The General Manager was on leave from 27 September until 20 October.

Meetings and events attended by the Acting General Manager

The Acting General Manager advises that the following meetings, events or appointments were attended since the last Council Meeting report:

Date	Description
30 September	Active Transport Strategy Working Group
1 October	RecFIT
2 October	Resilience and Recovery - Department of Premier and Cabinet
6 October	Community member
7 October	@leisure Planners
	Review of Relief and Recovery Arrangements - Local Government Association of Tasmania
8 October	Safe City Round Table
10 October	North West General Managers meeting
	Anita Dow MP
13 October	Department of State Growth
	TasNetworks

Date	Description
15 October	TasPorts x Strait Link Lease Announcement
	Burnie Works
	Urban Plan Working Group
17 October	Office of Local Government

Committees and Working Group meetings held

The following council-appointed Committee or Working Group meetings have been held since the last Council Meeting report:

Date	Committee or Working Group
15 September	City Centre Urban Plan Working Group
25 September	Burnie Youth Council Special Committee
30 September	Active Transport Strategy Working Group
10 October	Burnie Bicentenary Committee
14 October	General Manager's Performance Review Special Committee

Community support approved under delegation

The following payments have been approved by the General Manager under delegation since the last Council Meeting report:

Amount	Recipient – Purpose
	No items to report

Delegation under Council resolution – AO084-22 dated 24 May 2022 (section 77 *Local Government Act 1993*)

Use of the Common Seal

The General Manager advises the following documents have been sealed since the last Council Meeting report:

Date	Document
1 July 2025	Contract between Tasmanian Government, Burnie City Council and Valuer: Provision of Statutory Valuation Services (NRET OVG 2024-09(c))
3 July 2025	Final Survey Plan and Schedule of Easements Misc 2025/5 – George Street, Ridgley
3 July 2025	Adhesion Order SD 2025/6 – 368-370 Mount Street, Upper Burnie
7 July 2025	Part 5 Agreement – 51 Clarkes Road, Upper Stowport
21 July 2025	Final Survey Plan and Schedule of Easements, and Part 5 Agreement, SD 2024/1363 – 57 Morris Road, Natone
21 July 2025	Final Survey Plan and Schedule of Easements, and Part 5 Agreement, SD

Date	Document
	2024/1361 – 57 Morris Road, Natone
6 August 2025	Final Survey Plan and Schedule of Easements – SD 2025/1385 – 121 Bass Highway, Cooe, 121A Bass Highway Cooe

Council Meeting action list

Actions lists from Council Meetings in Open Session are attached at the end of this report.

ATTACHMENTS

- 1 [↓](#). Open Session - All Actions - 23 September 2025
- 2 [↓](#). Open Session - Outstanding Actions up to August 2025

Council Meeting Action Report**All Actions for Open Session of 23 September 2025**

Date From: 23/09/2025
Date To: 23/09/2025
Printed: Thursday, 16 October 2025

Item Number	Report Title	Action	Department	Completed
AO152-25	MOTION ON NOTICE - REMOVAL OF DETERIORATING NORFOLK PINES - BURNIE WATERFRONT Council 23/09/2025	24 Sep 2025 Implement the removal of Norfolk Pines on Burnie Waterfront, per council resolution AO152-25.	Works and Services	
AO155-25	PROPOSAL FOR DISPOSAL OF PUBLIC LAND - 2 COOEE POINT ROAD (CT 117536/2), 13 AMANDA COURT (CT 54930/27), BRICKPORT ROAD (CT 121561/279 AND CT 153767/40) Council 23/09/2025	24 Sep 2025 Commence the submission process for disposal of public land parcels at Cooe Point, Amanda Court, and Brickport Road, per council resolution AO155-25 and bring a further report to council on the submissions received. 13 Oct 2025 Submission process commenced. Report to be brought back to November meeting.	Works and Services	
AO156-25	WEST PARK PRECINCT MASTERPLAN - PUBLIC CONSULTATION Council 23/09/2025	24 Sep 2025 Undertake a public consultation process for the West Park Precinct MasterPlan per council resolution AO156-25 and bring a further report to council after the consultation period has ended. 13 Oct 2025 Consultation process commenced.	Office of the General Manager	
AO157-25	WEST PARK CARETAKER HOUSE - DEMOLITION Council 23/09/2025	24 Sep 2025 Obtain necessary permits and undertake the demolition works of the West Park Caretaker House at West Park, per council resolution AO157-25.	Works and Services	
AO158-25	WEST PARK OVAL - SWITCHBOARD RENEWAL Council 23/09/2025	24 Sep 2025 Undertake switchboard renewal at West Park, per council resolution AO158-25.	Works and Services	
AO159-25	ADDITIONAL KERBSIDE COLLECTION - CIRCULAR ROAD Council 23/09/2025	24 Sep 2025 Incorporate the addition of Circular Road into In-zone Kerbside Collection and update relevant public maps and information, per council resolution AO159-25. 13 Oct 2025 Completed - Changes made to include Circular Road, maps updated on council and NWRRR websites.	Works and Services	13/10/2025
AO161-25	POLICY REVIEW - PURCHASING POLICY Council 23/09/2025	24 Sep 2025 Finalise the Purchasing Policy in the corporate document framework and publish, per council resolution AO161-25. 13 Oct 2025 Completed - Purchasing policy finalised and published.	Office of the General Manager	13/10/2025
AO164-25	BURNIE CITY COUNCIL AUDIT PANEL - UNCONFIRMED MINUTES OF MEETING HELD ON 6 AUGUST 2025 Council 23/09/2025	24 Sep 2025 Send acknowledgement to the Audit Panel Chair for letter dated 29 August 2025, and tabled to council AO164-25. 29 Sep 2025 Completed - Letter sent CM 25/26237.	Office of the General Manager	29/09/2025

Council Meeting Action Report**Outstanding Actions for Open Sessions up to August 2025**

Date From: 1/01/1900
Date To: 31/08/2025
Printed: Thursday, 16 October 2025

Item Number	Report Title	Action	Department	Completed
AO106-22	BY-LAW - INTENTION TO MAKE A NEW BY-LAW - DRAFT PUBLIC PLACES BY-LAW	<p>05 Jul 2022 Request the preparation of a Regulatory Impact Statement for the draft Public Places By-law and then progress the required Director's certification and public consultation process, before returning a report to Council, per AO106-22.</p> <p>06 Jul 2022 Preparation of RIS requested from legal practitioner on 6.7.22, doc 22/19343.</p> <p>17 Nov 2022 RIS received and being reviewed.</p> <p>11 Jan 2023 RIS finalised and sent to Director Local Government on 10.1.22 with draft by-law, seeking certification, prior to final stage of community consultation.</p> <p>13 Apr 2023 Public consultation information prepared, awaiting Certificate from Director Local Government.</p> <p>12 Sep 2023 Feedback received from DLG, being considered.</p> <p>17 Jan 2024 Revisions made incorporating feedback, and sent to legal advisor for review in Nov 2023.</p> <p>13 Feb 2024 Updated Public Places By-law and Regulatory Impact Statement re-submitted to Director Local Government 7 Feb 2024 for review and certification.</p> <p>11 Feb 2025 Further feedback received from DLG on 22 May 2024. Internal review of feedback and information provided to lawyer on 11 July 2024. Lawyer's feedback received 13 Aug 2024. Additional matters required to be addressed by DLG, proposed final amendments sent to Lawyer 31 Oct 2024. Awaiting response.</p> <p>17 Mar 2025 Response received from legal advisor, to be reviewed and re-sent to Director.</p> <p>15 Sep 2025 Certification of RIS received from Director of Local Government. Draft By-law to proceed to consultation as next stage.</p>	Office of the General Manager	

Council 28/06/2022

Council Meeting Action Report**Outstanding Actions for Open Sessions up to August 2025**

Date From: 1/01/1900
Date To: 31/08/2025
Printed: Thursday, 16 October 2025

Item Number	Report Title	Action	Department	Completed
AO102-23	INTENTION TO MAKE A NEW BY-LAW - HIGHWAYS BY-LAW	<p>28 Jun 2023 Progress the draft by-law to the next stage of legal review, preparation of Regulatory Impact Statement, and then submission to Director for review and certification (prior to consultation period), per Council resolution AO102-23.</p> <p>13 Jul 2023 Revised information sent to lawyer for review and preparation of Regulatory Impact Statement.</p> <p>13 Feb 2024 Proposed Highways By-law and Regulatory Impact Statement submitted to Director Local Government 7 Feb 2024 for review and certification.</p> <p>11 Feb 2025 Feedback received from DLG on 24 May 2024 requesting further information for Regulatory Impact Statement, in addition to feedback around By-law. Reviewing content, and preparing information to send to lawyer for consideration.</p>	Office of the General Manager	
Council 27/06/2023				
AO155-23	MOTION ON NOTICE - PRAYER AT COMMENCEMENT OF COUNCIL MEETINGS	<p>25 Oct 2023 Initiate an engagement with various faith leaders in the community with a view to establish an interfaith network that fosters social inclusion and collaborative community relationships, as per Council Resolution AO155-23.</p> <p>14 Oct 2024 Not started because of staff commitments to planned priorities.</p> <p>15 Nov 2024 On hold until commencement of new GM.</p> <p>11 Feb 2025 Meeting with General Manager and City Chaplain scheduled for 19 February 2025.</p> <p>17 Mar 2025 The General Manager has met with the City Chaplain and a meeting will be arranged with pastors in May (due to their availability).</p> <p>11 Apr 2025 Meeting held with Chair – Pastors of Burnie on 8 April 2025 with General Manager and Mayor.</p> <p>19 May Feasibility work is now underway.</p>	Office of the General Manager	
Council 24/10/2023				

Council Meeting Action Report**Outstanding Actions for Open Sessions up to August 2025**

Date From: 1/01/1900
 Date To: 31/08/2025
 Printed: Thursday, 16 October 2025

Item Number	Report Title	Action	Department	Completed
AO039-24	MOTION ON NOTICE - MULTIPURPOSE SPORTS CENTRE	<p>28 Mar 2024 Commence working with sporting groups to explore options for an indoor multi-sport facility, per Council resolution AO039-24.</p> <p>14 Oct 2024 To be started in 2025 Calander year.</p> <p>15 Nov 2024 No further update.</p> <p>15 Jan 2025 Funding has been secured to undertake this project. Work yet to commence.</p> <p>17 Mar 2025 Internal working group being established.</p> <p>11 Apr 2025 Awaiting proposed dates and times from Burnie Basketball Club for initial meeting.</p> <p>19 May 2025 Meeting held with Burnie Basketball Club. Officers now to commence work in line with grant deed.</p> <p>10 Jul 2025 RFQ expected to be released in August.</p> <p>12 Sep 2025 Quote received and engagement confirmed.</p>	Office of the General Manager	
Council 26/03/2024				
AO074-25	MOTION ON NOTICE - BEAUTIFICATION OF CONCRETE COLUMNS ON COASTAL PATHWAY	<p>29 May 2025 Undertake arrangements to engage local schools to beautify the concrete columns at Cooe Point Road and Coastal Pathway intersection, per council resolution AO074-25.</p> <p>30 May 2025 Contact has been made with Mikkayla Leckie of Burnie Works to ask if the Burnie Youth Council would be interested in organising this project.</p> <p>13 Jun 2025 To be discussed at the next Youth Council meeting.</p> <p>16 Jul 2025 Item discussed with Youth Council at their June meeting who will consider further works at their next meeting.</p> <p>05 Sep 2025 Council Officers are investigating methods for schools to decorate these columns without going out on site if possible.</p> <p>12 Sep 2025 Council Officers are arranging panels to be utilised for the schools to complete artwork on.</p>	Works and Services	
Council 27/05/2025				
AO137-25	MOTION ON NOTICE - BURNIE INN	<p>27 Aug 2025 Identify potential funding sources (including State and Federal grants) for conservation work; and consult with community, local historians and groups to ensure public input, as per Item AO137-25.</p> <p>05 Sep 2025 Council Officers will investigate for grant funding opportunities in the future.</p> <p>12 Sep 2025 Report received from Heritage Tasmania. Next steps are to be determined.</p>	Works and Services	
Council 26/08/2025				

Council Meeting Action Report**Outstanding Actions for Open Sessions up to August 2025**

Date From: 1/01/1900
Date To: 31/08/2025
Printed: Thursday, 16 October 2025

Item Number	Report Title	Action	Department	Completed
AO137-25	MOTION ON NOTICE - BURNIE INN	<p>27 Aug 2025 Prepare a report showing a costed preservation and maintenance plan, funding opportunities and recommended actions for long-term preservation, for Council's consideration by December, as per Item AO137-25.</p> <p>05 Sep 2025 Director Operations has written to Heritage Tasmania on 3 September 2025 to obtain further information.</p> <p>12 Sep 2025 Report received from Heritage Tasmania. Next steps are to be determined.</p>	Works and Services	
Council 26/08/2025				
AO137-25	MOTION ON NOTICE - BURNIE INN	<p>27 Aug 2025 Commission a heritage assessment and condition report of the Burnie Inn within three months, as per Item AO137-25.</p> <p>05 Sep 2025 Director Operations has written to Heritage Tasmania on 3 September 2025 to obtain further information.</p> <p>12 Sep 2025 Report received from Heritage Tasmania. Next steps are to be determined.</p>	Works and Services	
Council 26/08/2025				
AO142-25	CHRISTMAS PREPARATIONS 2025	<p>27 Aug 2025 Approve the purchase of new Christmas decorations and arrange customized street banners, as per Item AO142-25.</p> <p>05 Sep 2025 Christmas decorations have been ordered. Council Officers will contact schools to organise banners.</p> <p>12 Sep 2025 Council Officers have this in hand.</p> <p>13 Oct 2025 Preparations underway for school competition.</p>	Works and Services	
Council 26/08/2025				

OFFICERS' REPORTS**AO182-25 PROJECT DELIVERY REPORT - SEPTEMBER 2025**

FILE NO: 4/18/2
PREVIOUS MIN:

1.0 RECOMMENDATION:

"THAT Council note the information contained in the Project Delivery Report for the quarter ended 30 September 2025."

2.0 EXECUTIVE SUMMARY**Purpose and Background**

To provide an update to Council on the progress of major projects, the capital works program, annual plan actions, and related matters, on a quarterly basis.

Key Issues

The Project Delivery Update includes reports on:

- Annual Plan Progress Report
- Major Projects Update
- Contracts Awarded > \$100,000

3.0 SUPPORTING INFORMATION**Strategic Alignment**

This report aligns to Council's Strategic Plan Goal 1 Making an active town centre; Goal 2 Making liveable local communities; and Goal 3 Making sustainably built and natural environments.

Legal

Section 71 of the *Local Government Act 1993* requires that Council prepares an Annual Plan for each financial year which is consistent with the Strategic Plan and identifies the specific actions that Council commits to undertaking throughout the year. Section 82 of the Act requires that the Council adopt estimates of the Council's revenue and expenditure for the year (known as the Budget Estimates).

Finance

This report provides updates on projects that have been committed to within Council's Annual Plan and Budget Estimates.

Relation to Council Policy

Council maintains the following relevant financial policies:

- Code for Tenders and Contracts

Discussion

Refer to the attachments for the following reports:

- Annual Plan Progress Report
- Major Projects Update
- Contracts Awarded > \$100,000

More detailed capital works program updates will be provided in future iterations of this report.

Health and Safety

There are no health and safety impacts resulting from this report.

Risk

Council sets its annual plan initiatives and the capital works program, as part of its budget deliberations each year, ensuring that it has the budget resources to fund those activities.

The primary risk in this area is not meeting the planned works due to unforeseen reasons such as supply factors, people resources, timing delays, unforeseen contingencies, or cost escalation. Such delays can result in financial cost and asset renewal ratio impacts, flow on effects to interdependent projects, impacts to service delivery, and reputational risks within the community.

The purpose of this report is to provide periodical reporting on the status of council's committed program of works across the annual plan and capital program, in order to monitor progress and communicate delays or challenges as they arise.

Environmental Sustainability

There are no environmental sustainability impacts to be considered in this report.

Consultation

Relevant Council Officers have been consulted in the preparation of this report.

Conflict Of Interest






No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

ATTACHMENTS

- 1 [↓](#). Annual Plan Progress Report - September 2025
- 2 [↓](#). Major Projects Update - September 2025
- 3 [↓](#). Contracts Awarded > \$100,000 - September 2025










Annual Plan Progress Report 2025-26 | Quarter 1

2025-26 Annual Plan Actions	Progress Update	Status
Making an active town centre		
Complete year one actions from the agreed Urban Plan action list as developed by the Urban Plan Steering Committee	Appointment of Project Manager Urban Plan has been made. Working group established. Foreshore Renewal – next steps are design of rail infill, upgrade and expansion of playground, design and implementation of disability access improvements. Civic Plaza – next steps are detailed design of plaza redevelopment, and design for new inner city park. City Revitalisation – next steps are detailed design for retail heart city street scape, and on-street dining trial project.	 Commenced
Complete the Parking Strategy	Work has commenced internally, with full strategy to be completed in time for 2026-27 budget considerations.	 Commenced
A reintroduction of visitor information services to Burnie, located at Burnie Arts	Spirit Readiness grant has been submitted. Awaiting outcome.	 Commenced
Making liveable local communities		
Complete feasibility study for new multi-purpose sports facility in line with grant deed requirements	Collation of previous studies has been undertaken. Consultant has been engaged to produce report and concept designs.	 Commenced
Adopt the Active Transport Strategy and complete initial tasks in line with grant deed requirements	Well progressed with consultation stages and initial draft report. Final consultation undertaken and report to be provided to Council in October.	 Well progressed
Work with Cradle Coast Authority to finalise Coastal Pathway feasibility and design	Initial meeting held with CCA to pass over previous reports and information.	 Commenced
Progress the Playground Strategy by undertaking playground improvements at Shorewell Park and the Waterfront	Procurement documentation finalised, in readiness for design and construction tender for Shorewell Park Playground. Waterfront playground will be progressed as part of Urban Plan project.	 Commenced







Annual Plan Progress Report 2025-26 | Quarter 1

2025-26 Annual Plan Actions	Progress Update	Status
Oversee the delivery of South Burnie Bowls Club building redevelopment	Working group established, tender documents finalised and tender process closed. Submissions being assessed.	 Commenced
Making sustainably built and natural environments		
Commence and undertake year one actions of the Urban Rivers project	Grant Agreement signed. Land manager agreements being progressed. Full 3 year project involves improvement to catchments of Romaine Creek and Shorewell Creek through removing environmental weeds, revegetation with local native species, prevent stock access to waterways, and installation of gross pollutant traps.	 Commenced
Deliver waste improvements, including the introduction of food and organics collection	Communications campaign well progressed. New waste fees introduced with 2025-26 rates resolution. Consultation and amendment made with Circular Road residents. New FOGO and realigned waste and recycling services commenced in September as planned.	 Complete
Making culturally rich and vibrant communities		
Hold official opening of Burnie Arts	Construction of Stage 2 almost complete. Anticipating occupancy certificate in October. Planning underway for official opening to be held on 10 November 2025.	 Well progressed
Form a committee to progress Bicentennial event planning	Committee appointed and first meeting held.	 Complete
Making democratic and engaged communities		
Undertake projects in line with Council's Digital Transformation Strategy	Project list determined and actions underway. Current actions being pursued include Windows 11 transition, eServices platform take-up for 2025-26 rates payments; ongoing development of eForms, M365 Apps, and planning toward Civica Altitude transition.	 Ongoing
Review Council's events support and associated processes	Yet to commence. Will be considered within the new Community Services roles being re-established.	 Not started



Annual Plan Progress Report

2025-26 | Quarter 1

2025-26 Annual Plan Actions	Progress Update	Status
Commence facility signage review and upgrade program	Facility audit completed. Planning and recommendations being worked on.	 Commenced
Progress actions in line with Council's Asset Management Improvement Plan	Asset Management Plans adopted by Council in July 2025. Actions within plans are progressing.	 Commenced
Explore and initiate dedicated asset disposal program	Public land disposal process commenced for three parcels of vacant land. Demolition of caretakers' house at West Park Oval to be progressed. Planning in progress to identify next round for Council consideration.	 Commenced
Adopt West Park Master Plan	Working group meeting held in August 2025. Draft plan tabled to Council in September 2025 and made available for public consultation. Feedback and further report to be brought back to Council.	 Well progressed

Status Key

 Not started	 Commenced	 In progress	 Well progressed	 Complete	 Ongoing	 Deferred
--	--	--	--	--	--	---



Major Projects Update

2025-26 | Quarter 1

Burnie Arts – Plaza Redevelopment

\$1,350,000

Project Summary

The redeveloped plaza and landscaped areas will provide places for entertainment and relaxation. Through initiatives in the Urban Plan, it will also have better connections to the waterfront and shopping areas.

Strategic Alignment

This project forms part of the Burnie City Centre Urban Plan, available online at burnie.tas.gov.au

Funding

This project has been allocated \$1.35M from Council's 2025-26 capital budget.

Progress to Date

Detailed designs for the redevelopment have been completed by the consultant.

Next Steps

The contractor Vos Construction and Joinery will be relocating the site sheds into Little Alexander Street and fencing off the construction zone in the car park to the west of the Museum.

Planned Completion

30 June 2026



Shorewell Park Playground Redevelopment

\$1,500,000

Project Summary

The Burnie City Council is upgrading the playground at Shorewell Park to create a safe, inclusive, and engaging space for children and families.

Delivered as a design and construct contract, the project invites contractors to propose creative designs aligned with Council's Playground Strategy. The upgrade will include modern play equipment, improved landscaping, and supporting amenities.

Strategic Alignment

This project is a deliverable of the Burnie City Council Playground Strategy, available online at burnie.tas.gov.au

Funding

This project is subject to a total funding of \$1.4M from the Tasmanian Government (inc \$400K via LGAT).

Progress to Date

Tender for the design and construction of the playground has been developed ready for advertising.

Next Steps

Upon completion of the tender being advertised, officers will evaluate the submission with an aim to bring the recommendation to council at the December meeting.

Planned Completion

29 May 2026



South Burnie Bowls Club

\$2,130,000

Project Summary

South Burnie Bowls Club has received funding from the State Government for the redevelopment of the clubrooms located at 2 Hales Street South Burnie.

Strategic Alignment

This funding was sought by the South Burnie Bowls Club and provided via Council to deliver. The project aligns with Council's Strategic Plan Goal 4 Making culturally rich and vibrant communities.

Funding

This project has received funding of \$2.13M from the Tasmanian Government.

Progress to Date

Detailed designs for the redevelopment have been completed by JD2 Consulting and tenders have been called.

Next Steps

Upon completion of the tender being advertised, officers will evaluate the submission and aim to bring the recommendation to Council at the October meeting.

Planned Completion

29 May 2026



Foreshore Playground

\$1,500,000

Project Summary

Council is upgrading the playground located on the Foreshore as a part of its \$17.3M Urban Plan Project. It aims to replace and expand upon the current foreshore play area to help create a safe, inclusive, and engaging space for children and families.

Strategic Alignment

This project forms part of the Burnie City Centre Urban Plan, available online at burnie.tas.gov.au

Funding

This project has received funding from the Tasmanian Government as part of its funding toward the Urban Plan.

Progress to Date

The consultation process has been completed as part of the urban plan development.

Next Steps

Officers have met with the Urban Plan working group and are now progressing towards a detailed design.

Planned Completion

30 June 2026





Contracts Awarded > \$100,000 2025-26 | Quarter 1

The following table lists contracts awarded during the above quarter, with a value exceeding \$100,000 ex GST. Council's Code for Tenders and Contracts Policy is available online at www.burnie.tas.gov.au

All contracts awarded throughout the financial year which exceed \$100,000, are reported in Council's Annual Report.

Contract/ Project Number	Contract / Project Description	Successful Contractor Registered Business Name and Address	Awarded Date	Initial Term / Duration	Period of Options for Extending	Value of Contract (ex GST)
	No contracts awarded during this period					

OFFICERS' REPORTS**AO183-25 OPERATIONAL ACTIVITY REPORT - SEPTEMBER 2025**

FILE NO: 4/18/2
PREVIOUS MIN:

1.0 RECOMMENDATION:

“THAT Council note the Operational Activity Report, as attached.”

2.0 EXECUTIVE SUMMARY**Purpose and Background**

To provide an activity update on various Council operations, on a quarterly basis.

Key Issues

The operational activity reports included are:

- Planning Report
- Building Report
- Environmental Health Report
- Operational Maintenance Report
- Waste Report
- Community and Recreation Facilities Report

3.0 SUPPORTING INFORMATION**Strategic Alignment**

This report aligns to Council’s Strategic Plan Goal 2 Making liveable local communities, and Goal 3 Making sustainably built and natural environments.

Legal

Planning, building and environmental health information provided in this report, are subject to the following relevant legislation:

- *Land Use Planning and Appeals Act 1993* and associated regulations
- *Building Act 2016* and associated regulations
- *Occupational Licencing Act 2005*

- *Environmental Management and Pollution Control Act 1994* and associated regulations
- *Food Act 2003* and associated regulations
- *Public Health Act 1997* and associated regulations

In relation to waste services, Council has the Waste Collection and Waste Management By-law, no 2 of 2017.

Finance

There are no financial impacts resulting from this report.

Relation to Council Policy

Council maintains the following relevant operational policies:

- Public Places Use Agreement Policy
- Waste Collection and Management Services Policy

Discussion

Refer to the attachments for detailed information on the following items:

- Planning Report
- Building Report
- Environmental Health Report
- Operational Maintenance Report
- Waste Report
- Community and Recreation Facilities Report

Health and Safety

There are no health and safety impacts resulting from this report.

Risk

There are no identified risks that require consideration in relation to this report.

Environmental Sustainability

There are no environmental sustainability impacts to be considered in this report.

Consultation

Relevant Council Officers have been consulted in the preparation of this report.

Conflict Of Interest

No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

ATTACHMENTS

- 1 [↓](#). Planning Report - September 2025
- 2 [↓](#). Building Report - September 2025
- 3 [↓](#). Environmental Health Report - September 2025
- 4 [↓](#). Operational Maintenance Report - September 2025
- 5 [↓](#). Waste Report - September 2025
- 6 [↓](#). Community and Recreation Facilities Report - September 2025



Planning Report

2025-26 | Quarter 1

Key Points

- Development application submitted in Q1 2025 were consistent with the period in 2024.
- Average days to determine an application were consistently lower than the statutory requirements.

Summary of Land Use and Development Applications

Land Use and Development Applications	Jul 24	Aug 24	Sep 24	Oct 24	Nov 24	Dec 24	Jan 25	Feb 25	Mar 25	Apr 25	May 25	Jun 25	July 25	Aug 25	Sept 25
Permitted Use and Development lodged	2	10	0	2	6	2	1	0	2	2	1	2	2	1	1
Discretionary Use and Development lodged	9	6	3	5	8	4	8	8	7	2	4	2	9	8	8
Subdivisions lodged	1	2	4	4	1	4	1	2	2	1	1	3	3	0	3
Total Applications Lodged	12	18	7	11	15	10	10	10	11	5	6	7	14	9	12

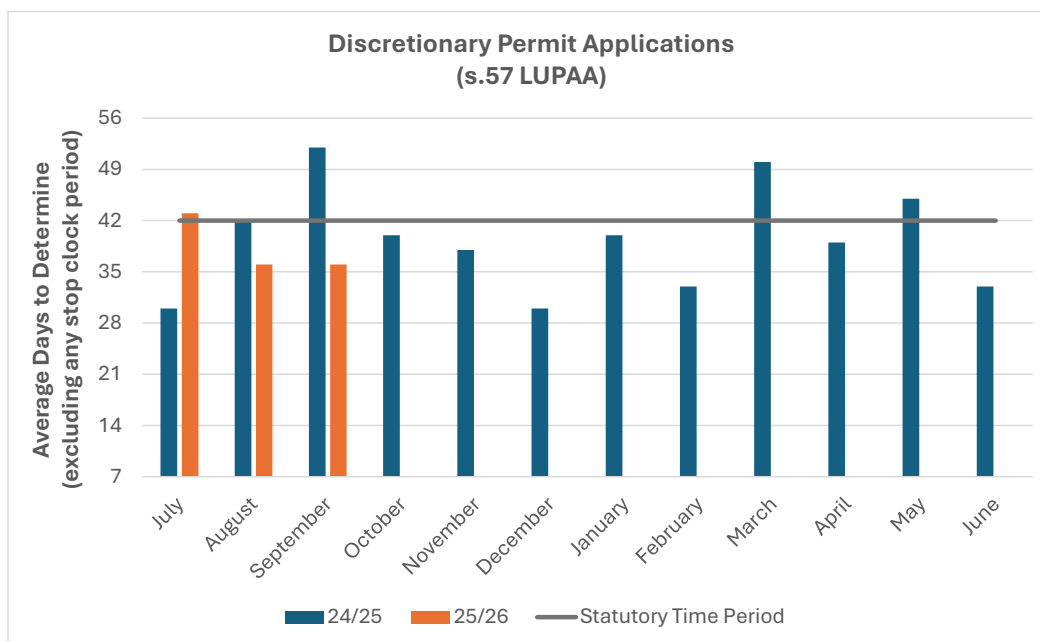
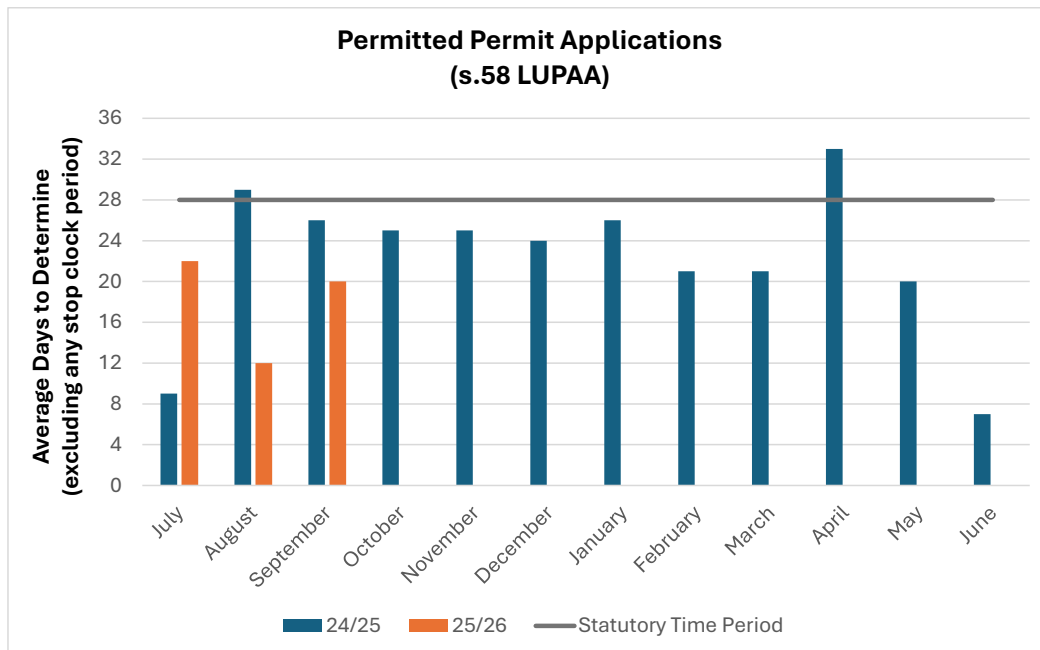
Land Use and Development Application Outcomes	Jul 24	Aug 24	Sep 24	Oct 24	Nov 24	Dec 24	Jan 25	Feb 25	Mar 25	Apr 25	May 25	Jun 25	July 25	Aug 25	Sept 25
Determined by Delegation	7	9	14	6	6	10	9	7	5	9	7	5	2	9	7
Determined by Council	0	1	1	0	1	0	0	0	2	0	0	2	0	0	1
Withdrawn	0	0	2	0	1	0	1	1	1	0	0	1	1	1	0
Applications cancelled by Planning Authority	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1
Consent Decisions	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0
Application Approved by Tasmanian Planning Commission	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Applications Appealed and Approved by Tasmanian Civil and Administrative Tribunal	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0

Planning Scheme Amendments	Jul 24	Aug 24	Sep 24	Oct 24	Nov 24	Dec 24	Jan 25	Feb 25	Mar 25	Apr 25	May 25	Jun 25	July 25	Aug 25	Sept 25
Amendment requests	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Amendment Approved	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0
Amendment Refused	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Approval Detail

Permit No.	Type of Development	Location	Approval	Decision Date
2025/19	Dwelling - Single	712 West Mooreville Road WEST MOOREVILLE TAS 7321	Approved Under Delegation	24/07/2025
2025/47	Wall Sign	Smith Street WIVENHOE TAS 7320	Approved Under Delegation	05/08/2025
2025/40	Dwelling - Single	3 Charlton Close HEYBRIDGE TAS 7316	Approved Under Delegation	05/08/2025
2025/33	Change of Use to Visitor Accommodation	210 Mount Street UPPER BURNIE TAS 7320	Approved Under Delegation	05/08/2025
2025/9	Extension of Waste Storage Facility	80 Wilson Street BURNIE TAS 7320	Approved Under Delegation	11/08/2025
2025/41	Dwelling - Single	784 Circular Road EAST RIDGLEY TAS 7321	Approved Under Delegation	18/08/2025
2025/37	Storage Shed	1419 Ridgley Highway HIGHCLERE TAS 7321	Approved Under Delegation	19/08/2025
2025/43	Consulting Rooms/Health Centre	16 Mooreville Road PARK GROVE TAS 7320	Approved Under Delegation	22/08/2025
2025/49	Sauna	2 North Terrace BURNIE TAS 7320	Approved Under Delegation	28/08/2025
2025/51	Visitor Accommodation	Unit 2 21 Uplands Place PARK GROVE TAS 7320	Approved Under Delegation	01/09/2025
2025/53	(Outbuilding) Shed	135 West Mooreville Road EAST CAM TAS 7321	Approved Under Delegation	02/09/2025
2025/24	Dwelling - Multiple	Unit 2 14 Gray Street UPPER BURNIE TAS 7320	Approved Under Delegation	11/09/2025
2025/52	Dwelling - Single	24 Grant Street North HAVENVIEW TAS 7320	Approved Under Delegation	11/09/2025
2025/57	Chicken egg production building	1648 Oonah Road TEWKESBURY TAS 7321	Approved Under Delegation	16/09/2025
2024/54	Dwelling - Single	11 Bissett Place HEYBRIDGE TAS 7316	Approved Under Delegation	25/09/2025
2025/1381	Subdivision to create two (2) lots	1 Madeline Drive MOOREVILLE TAS 7320	Approved Under Delegation	09/07/2025
2025/1382	Subdivision to create two (2) lots	9 Besser Crescent CAMDALE TAS 7320	Approved Under Delegation	21/08/2025
2025/1384	Subdivision to create two (2) lots	143 Natone Road STOWPORT TAS 7321	Approved by Council	23/09/2025
2025/1385	Subdivision and consolidation	121 Bass Highway COOEE TAS 7320	Approved Under Delegation	17/09/2025

Determination Timeframes



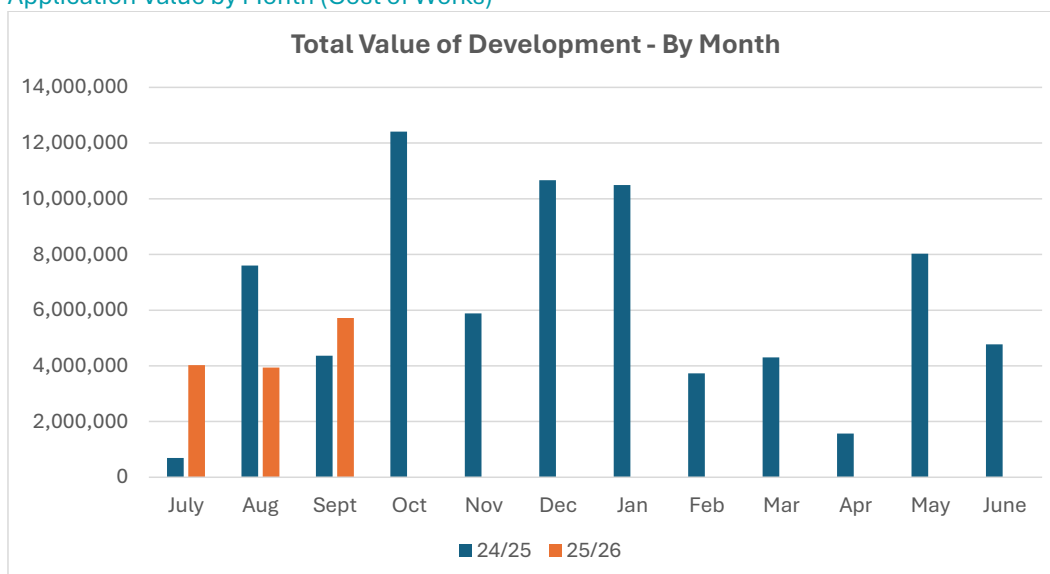


Building Report 2025-26 | Quarter 1

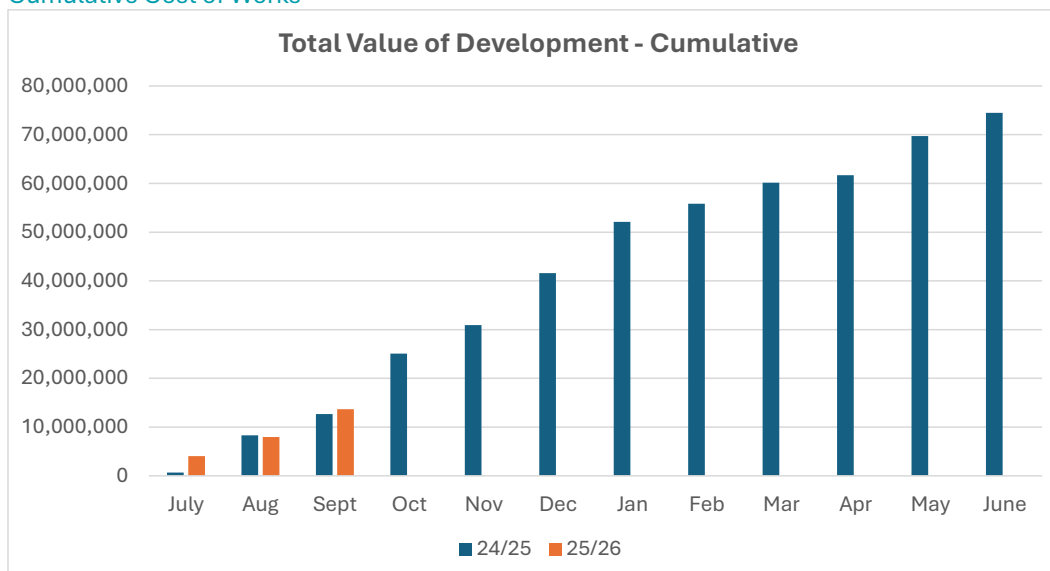
Key Point

- Q1 consolidated building revenue estimates were approximately 8% higher than the same period last year (13,693,344 vs 12,660,162).

Application Value by Month (Cost of Works)



Cumulative Cost of Works



Detail

	Year	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	June
Notifiable Plumbing	2024	4	7	8	8	2	1	4	6	6	6	4	5
	2025	11	5	5									
Notifiable Building	2024	6	8	9	11	16	10	7	13	14	11	11	10
	2025	12	13	11									
Permit Plumbing	2024	4	1	2	1	3	1	2	2	4	3	4	3
	2025	4	1	3									
Permit Building	2024	0	2	2	4	3	2	2	0	1	3	7	0
	2025	3	1	2									
Substantial Compliance	2024	0	0	1	0	1	1	0	0	0	0	0	1
	2025	1	1	0									
Notifiable Demolition	2024	2	1	0	0	0	0	0	0	0	0	1	0
	2025	2	1	0									
Permit Demolition	2024	0	0	0	0	0	1	0	0	0	0	0	0
	2025	1	0	0									
Permit Refused	2024	0	0	0	0	0	0	0	0	0	0	0	0
	2025	0	0	0									



Environmental Health Report

2025-26 | Quarter 1

Key Points

- Environmental Health activity remains consistent, with no emerging issues identified during the first quarter of the financial year.

Detail

Environmental Enquiries / Investigations

Type	Number of Enquiries / Investigations			
	Jul	Aug	Sept	YTD Total
Environmental Queries	0	0	0	0
Air	2	1	4	7
Water	1	1	2	4
Noise	4	3	3	10
Solid Waste	0	0	1	1
Other	4	6	0	10

YTD is measured from 1 July each year.

Environmental Sampling

Type	Number of Samples Taken			
	Jul	Aug	Sept	YTD Total
Beach Water Samples (summer months only)	0	0	0	0
Environmental Samples (new)	0	0	0	0
Public Swimming Pool samples	5	5	8	18

YTD is measured from 1 July each year.

Inspections

Type	Number of Samples Taken			
	Jul	Aug	Sept	YTD Total
Food Premises Inspections	5	9	3	17

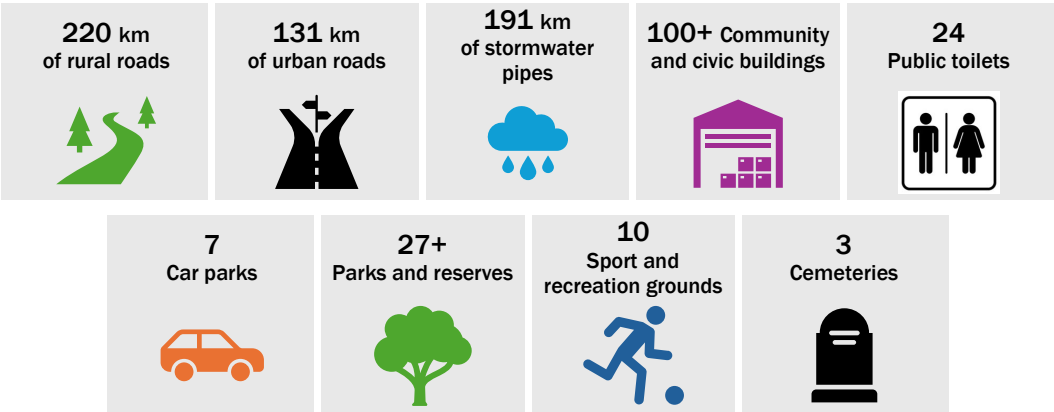
YTD is measured from 1 July each year.



Operational Maintenance Report - Highlights 2025-26 | Quarter 1

Council undertakes continual maintenance activities across its assets including the sealed and unsealed road network, the stormwater network, community and civic buildings, car parks, parks and reserves, sporting grounds, and cemeteries.

Snapshot of assets maintained by Council



Below are some maintenance highlights featured from the past quarter.

Civil Construction and Stormwater

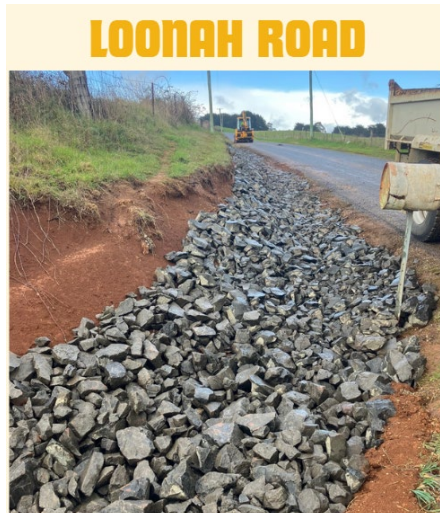
Grading of Unsealed Roads

Grading of gravel roads was undertaken in Highclere, West Ridgely and Tewkesbury.



Loonah Road – Natone

Rock lining works were undertaken in Loonah Road, Natone.

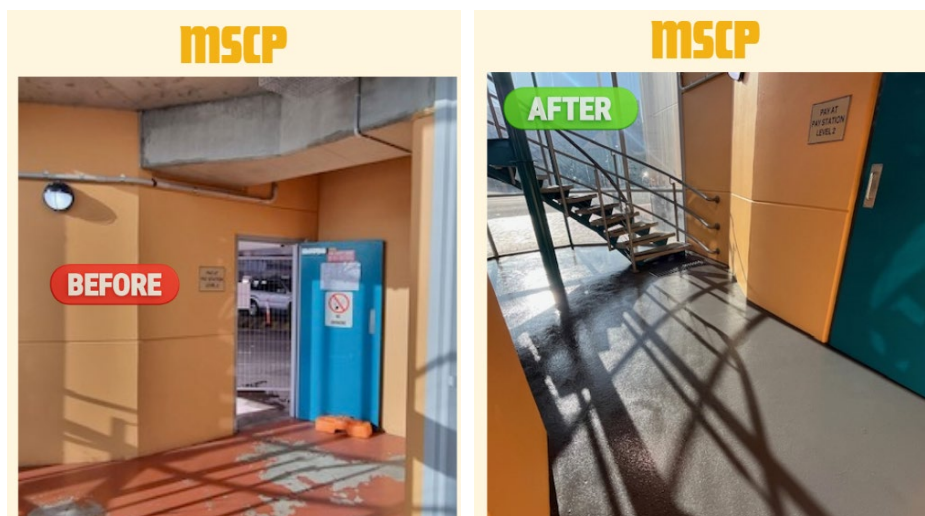
**Buildings****Ridgley Community Hall**

Garden beds at the Ridgley Community Hall were created, with bark and planting work to revitalize the surroundings.



Multi-Storey Car Park and Basalt Column Car Park

Stairwells at the multi-storey car park were cleaned and painted.



Pressure cleaning of the Multi-Storey Car Park was undertaken resulting in a clean fresh surface. Pond cleaning was undertaken in the Basalt Column Car Park.



Burnie Aquatic Centre

The new drive on pressure cleaner has also been cleaning at the Aquatic Centre to improve the outdoor pavement surfaces for patrons.






**Parks, Reserves, Sporting Grounds and Cemeteries****Burnie Park**

Matting to walking areas was replaced at Burnie Park.



Vandalism

Over the past quarter, the following vandalism occurred.

Category		Cost
 Graffiti		\$800
 Public toilet Damage		\$10,700
 Outside Furniture Damage		\$0
 Broken Glass/Windows		\$0
 Miscellaneous: Car ran into electrical box; 2 x fire extinguishers stolen by smashing glass to access key to unlock cabinets		\$1,300
Total for this quarter		\$12,800

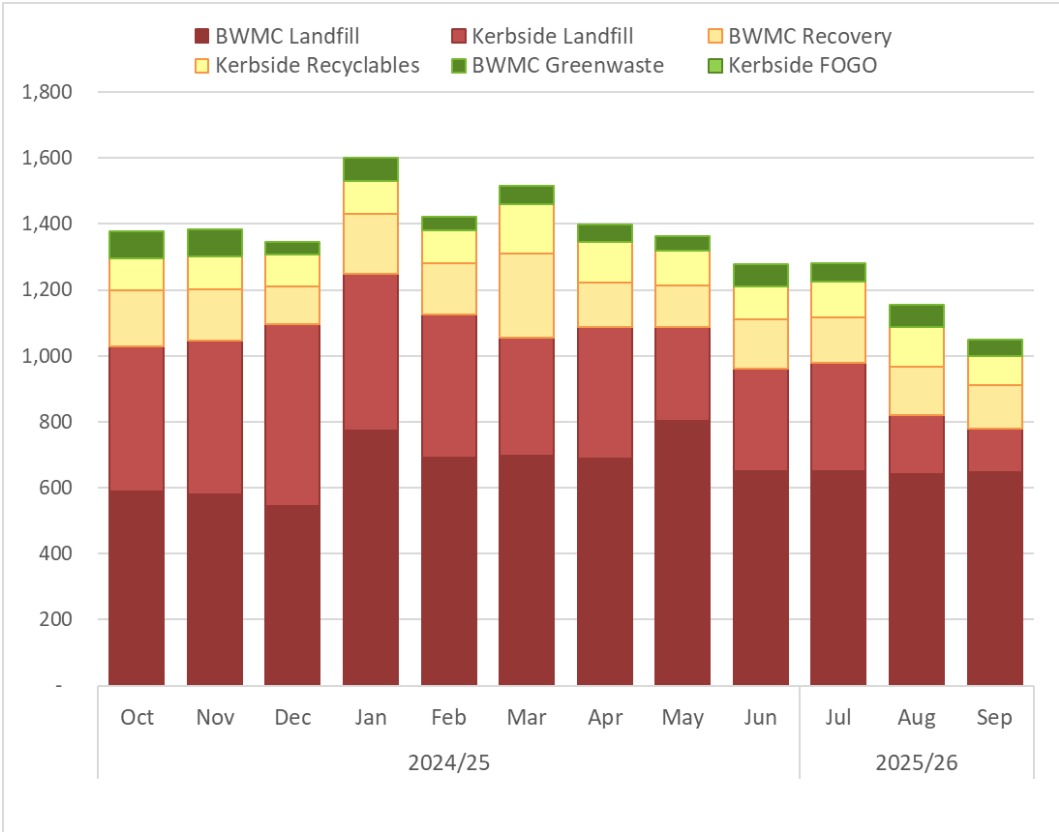


Waste Report

2025-26 | Quarter 1

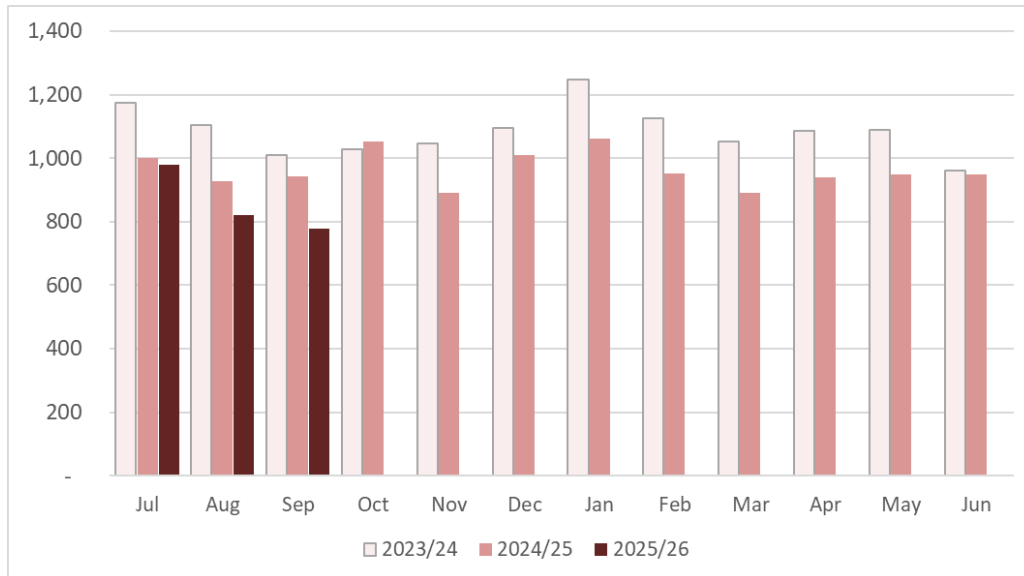
Waste Totals – Past 12 Months

Commentary: Diversion from landfill is sitting at around 25% of the total waste stream. Introduction of FOGO and fortnightly landfill collection in October will see some movement in this diversion.

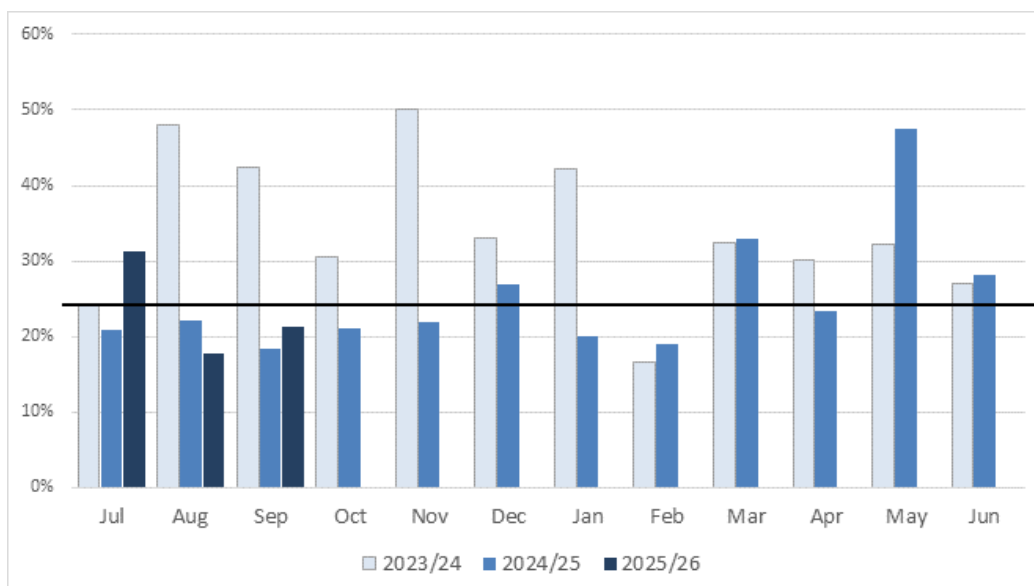


Goal 1: Reducing Landfill Waste

Commentary: Overall Waste to Landfill continues its downward trend over the past quarter.

**Goal 2: Recoverables % diverted away from Landfill**

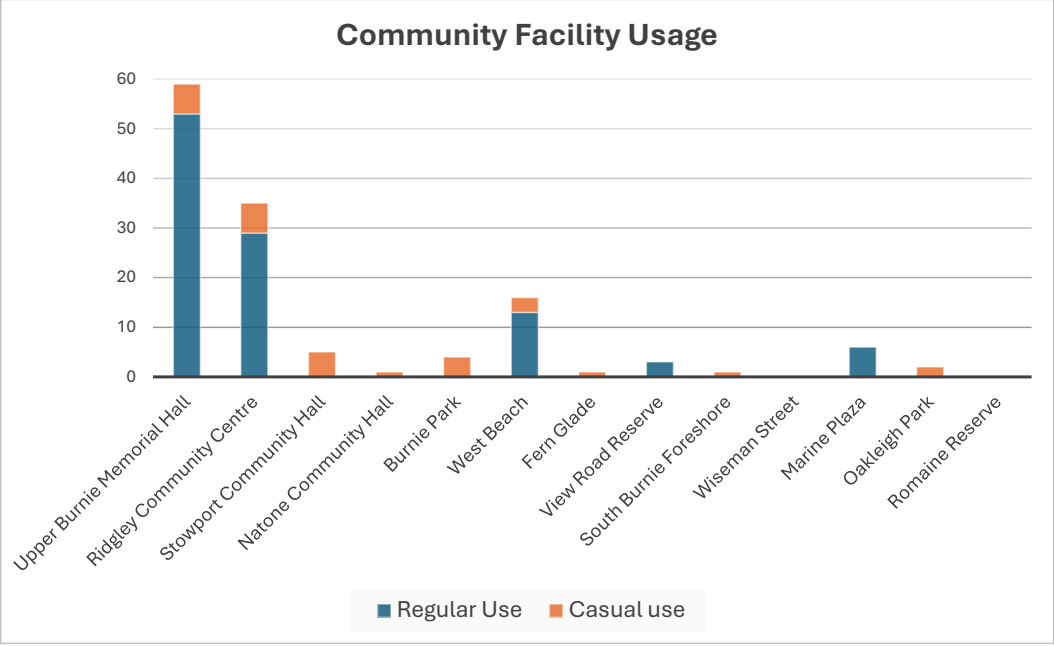
This graph includes recoverable materials such as FOGO, Recyclables, Cardboard, Greenwaste, Steel, Cardboard.



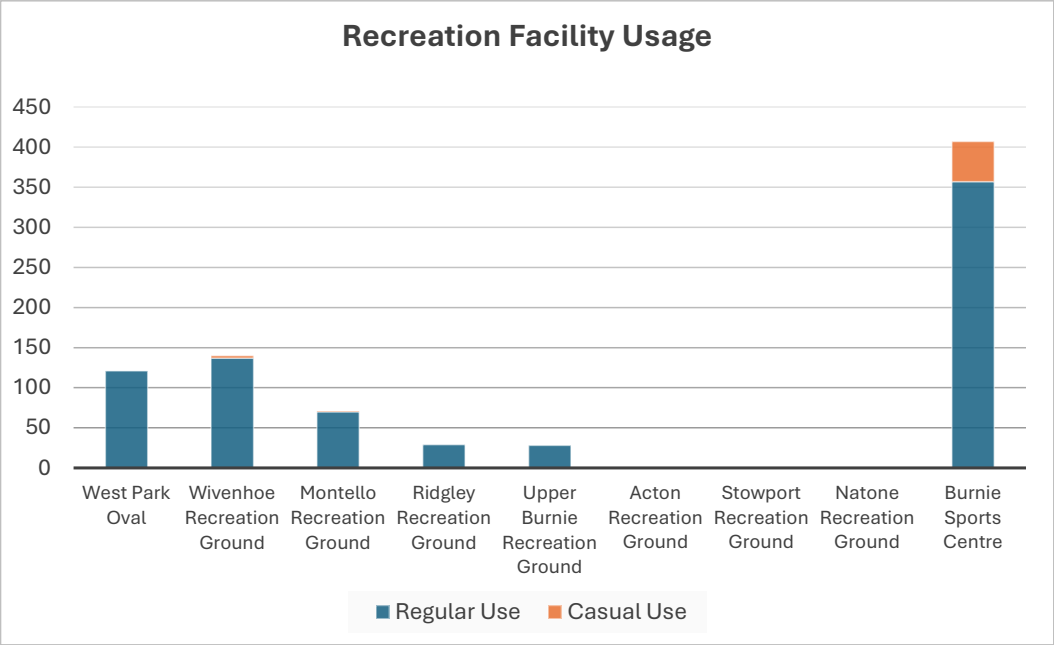


Community and Recreation Facilities Report 2025-26 | Quarter 1

The below graph shows the Community Facility Usage, per occasion, for the quarter. Usages are organised activities only, and does not include open public use.



The below Recreation Facilities, per occasion, can be subject to seasonal use.



OFFICERS' REPORTS**AO184-25 COMMUNITY AND ENGAGEMENT REPORT - SEPTEMBER 2025**

FILE NO: 4/18/2
PREVIOUS MIN:

1.0 RECOMMENDATION:

“THAT Council note the Community and Engagement Report, as attached.”

2.0 EXECUTIVE SUMMARY**Purpose and Background**

To provide an update on matters relating to community services and engagement, on a quarterly basis.

Key Issues

This report will be expanded in future with the commencement of new positions in community services roles.

The community and engagement reports included are:

- Snap Send Solve Report
- Communications Report

3.0 SUPPORTING INFORMATION**Strategic Alignment**

This report aligns to Council’s Strategic Plan Goal 2 Making liveable local communities, Goal 4 Making culturally rich and vibrant communities, and Goal 5 Making democratic and engaged communities; together with the Strategic Plan Enabler, Community Consultation.

Legal

Section 339F of the *Local Government Act 1993* requires that all Councils maintain a Customer Service Charter, and to annually report the number and nature of complaints received under it, which Council does through its Annual Report.

There are no other direct legal implications or legislative requirements for Council arising from the recommendation in this report.

Finance

There are no financial impacts resulting from this report.

Relation to Council Policy

Council maintains the following relevant community and engagement policies:

- Customer Service Charter
- Social Media Policy

All policies are publicly available online at www.burnie.tas.gov.au

Discussion

Refer to the attachments for detailed information on the following items:

- Snap Send Solve Report
- Communications Report

This report will be developed in time with additional information relating to Burnie Arts, and community events conducted by Council.

Health and Safety

There are no health and safety impacts resulting from this report.

Risk

Councils exist to provide services to the community that they serve. The primary risk category relating to this report is in relation to reputation.

Council's Strategic Risk Register monitors the following key risk, which is described as follows:

Community Confidence	The ability of the Council to create, maintain and develop positive community confidence and relationships including good communication and consultation, managing stakeholder risks, ensuring transparency and sound decision making, and proactive engagement.
----------------------	--

A number of controls are in place to manage this risk, and Council commits to undertaking a number of actions each year in the Register, which are aimed at further improving financial outcomes. The Strategic Risk Register is reviewed annually by Council.

Environmental Sustainability

There are no environmental sustainability impacts to be considered in this report.

Consultation

This report provides information on various communication and consultation efforts with the community.

Conflict Of Interest

No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

ATTACHMENTS

- 1 [↓](#). Communications Report - September 2025
- 2 [↓](#). Snap Send Solve Report - September 2025

Burnie City Council Communications Quarterly Report (Q3) July - September 2025



Burnie website

This reports outlines the tracking of data through web analytics. This quarterly overview of our data captured is also crossed referenced with external sources including Burnie City Council Social media.

Councils webpage provides our users with essential information, eServices and updates of Local Government requirements.



Discover Burnie app

The Discover Burnie app 'e-guides' tourists and locals alike, helping to discover - and rediscover - what Burnie has to offer.

This report highlights top content and usage, giving Council an overview of what Tourists and locals are searching for, and doing each quarter.



Burnie Connect

Acknowledging that not everybody accesses social media, the idea was conceived for Burnie Connect - an electronic newsletter delivered to the community and stakeholders by email, which was launched in May 2024.



Facebook

3.065 Billion active users

In Q1 of 2025, Facebook remains the world's largest social media platform with 3.065 billion active users according to Statista. Most of us view social media as fundamental to everyday life, broadening the range of available possibilities to facilitate interaction through a new set of capacities to enhance and promote effective communication.



Instagram

2 Billion active users

In 2025, Instagram is estimated to have over 2 billion monthly active users globally.

Burnie City Council launched an Instagram page in late 2023 to connect with a different demographic in Burnie.

The strategy includes enhancing Burnie's Tourism by connecting with a younger audience that are already present on the platform.



Community Consultations and Meetings

Meetings, which include spontaneous pop-up sites, are held to provide the community with more information on a subject and to enable them to pose questions to Council staff. The purpose of these sessions is to equip the community with the necessary information to make informed decisions about their opinions on the issue being discussed.



Webcast views

Councils livestreaming from monthly Council meetings. Both live and archived views are noted

Burnie website report

This reports outlines the tracking data through web analytics. This quarterly overview of our data captured is also cross referenced with external sources including Burnie City Council Social media.

Councils webpage provides our users with essential information and updates of events, grant opportunities, road updates, projects, local news, rates, registrations and compliance forms.

TERMS *A quick breakdown*

Active Users Users who have initiated at least one session during the date range.	New Users The number of first-time users during the selected date range.	Pageviews per User Total number of Sessions within the date range. A session is the period time a user is actively engaged with our website. All usage data (Screen Views, Events, Ecommerce, etc.) is associated with a session.	Number of Sessions per User The average number of Sessions per user
PCT engaged The engagement rate is the percentage of engaged sessions on our website. The bounce rate is the opposite of the engagement rate. The bounce rate is the percentage of sessions that were not engaged.	Page Views Page views are the total number of pages viewed. Repeated views of a single page are counted.	Engagement Time Average session duration refers to the average time users spend on our website during a single session. It is calculated by dividing the total duration of all sessions by the number of sessions during a specific period.	
		Event Count An event count is the total number of times specific user actions, or "events," occur on a website. These actions can include button clicks, form submissions, video plays, or file downloads, and tracking them provides insight into user engagement and site performance	

- In the **third** quarter of 2025, we have seen an increase in Event Count at 341,551.
- Mobile device use remains the majority method by which users access Council's content, which requires Council to design our content to be user friendly on the mobile screen
- The Event count per active user (38.3K) divides down to 8.91
- Waste Service changes were the top theme this quarter with 1,190 views across four pages:
 - Waste Changes and Summary (2k)
 - Waste and Recovery Service details (1.8k)
 - Let's get ready to FOGO (958)
 - Waste Changes - Bin collection dates (229)



Snap shot

Active Users - 38.3K Up 3.1% from Q2	Event Count 341,551	PCT Engage 51% (Q2) 43%	Page Views 114,181 (Q2) 133,000
New Users - 35.5K Up 1.5% from Q2			Engagement time 00:01:07 Up 18.8% from Q2
Pageviews per user - 3.0 Up 7.1% from Q2			

Burnie website trends

News	Views
1. Waste Changes Summary Burnie City Council	2K
2. Waste and Recovery Service Details Burnie City Council	1.8K
3. Let's get ready to FOGO Burnie City Council	958
4. Draft Active Transport Strategy - Round 2 Burnie City Council	415
5. Burnie LGBTQIA+ Advisory Group Burnie City Council	355
6. Commercial Operators for Burnie Cruise Visits Burnie City Council	326
7. Rates and Charges 2025-26 Burnie City Council	252
8. Waste Changes - Bin Collection Dates Burnie City Council	229
9. Annual Plan and Budget Estimates 2025-26 Burnie City Council	223
10. Circular Road Extension Consideration Burnie City Council	178
Grand total	9.7K

Events	Views
1. The Burnie Show & Agri-Expo Burnie City Council	1.5K
2. Tasmanian State Election Burnie City Council	982
3. School Holiday Activities Burnie City Council	591
4. Community Noticeboard Burnie City Council	534
5. Book Market : Burnie Emu Bay Lions Club Burnie City Council	317
6. Don't miss the Bus - Breastsreen Tasmania Burnie City Council	264
7. Out of the Shadows - Suicide Awareness Walk Burnie City Council	228
Grand total	11.4K

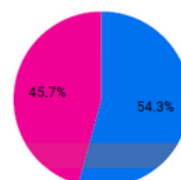
Downloads	Page title	#
Council Agenda - 22 July 2025(PDF, 17MB)	Council Meetings Burnie City Council	631
Executive Manager Community Services(PDF, 1.5MB)	Executive Manager Community Services & Communities Burnie City Council	610
DA-2025-49-Public-Exhibition-Documents_R_2025-49	2 North Terrace BURNIE - CT: 147506/1 Burnie City Council	540
Community Activation Officer(PDF, 233KB)	Executive Manager Community Services & Communities Burnie City Council	512
Waste Fees(PDF, 445KB)	Burnie Waste Management Centre Burnie City Council	444
Position Description(PDF, 252KB)	Executive Manager Works and Infrastructure Burnie City Council	404
Grand total		20K

Projects and Works	Views
Projects and Works Burnie City Council	814
Burnie Courts Complex Relocation Project Burnie City Council	387
State and Local Investment Burnie City Council	260
Burnie City Centre Urban Plan Burnie City Council	221
Burnie Coastal Pathway Burnie City Council	182
Grand total	2.7K

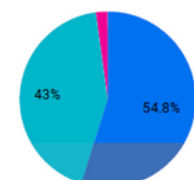
Jobs	Views
Careers at Council Burnie City Council	3,336
Executive Manager Community Services & Communities Burnie City Council	745
Food and Beverage Attendant Burnie City Council	735
Collections Officer Burnie City Council	586
Executive Manager Works and Infrastructure Burnie City Council	456
Grand total	8,093

Permit applications on exhibition	Views
2 North Terrace BURNIE - CT: 147506/1 Burnie City Council	394
Permit applications on exhibition Burnie City Council	156
1 & 3 Atkins Drive ROMAINE - CT: 161069/2, 162223/0 Burnie City Council	141
PARK GROVE, 16 Mooreville Road - CT: 184237/1 Burnie City Council	118
121 and 121A Bass Highway COOEE - CT: 17795/1, 17796/1 Burnie City Council	104
Grand total	2,336

Portion of visitors returning



What device are people using?



new returning mobile desktop tablet smart tv

	Live	Archived
January	89	87
February	289	210
March	322	160
April	240	247
May	201	219
June	153	1037*
July	452	1135
August	503	381
September	188	261

*The archived views for June are the highest since Council implemented livestreaming.

Burnie City Council
Monthly Council Meetings

Live stream views

Burnie website - Digital Services

Growing our digital services - making it easier for our community to access services from anywhere, anytime and on any device.



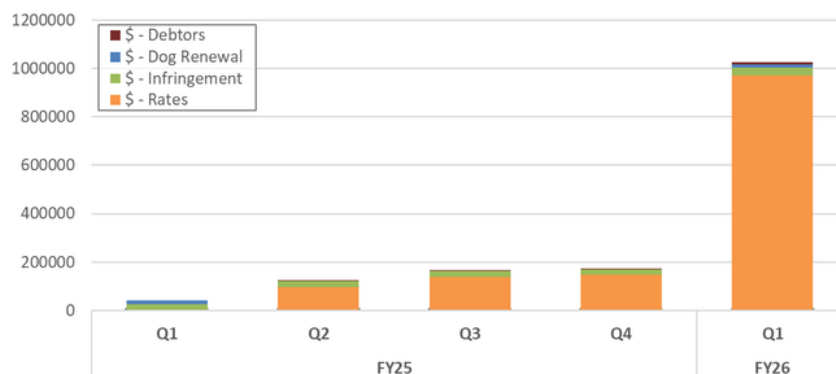
Snap shot (over the past 12 months since launching eServices)

Total \$
Income:
\$1.5M

Total #
Payments:
5146

Total #
Customers:
4180

eServices - # payments and \$ amounts per month



More customers are now using eServices to both check their rates balances and to make rates payments. Many like the convenience of paying for more than one property in a single transaction, while others prefer fortnightly payments to make costs easier to manage. Some ratepayers now pay all their instalments online, and a few pay for several properties at once. Increasingly larger payments made show people's growing trust in the online system, with limits set only by their credit cards. Online payments for infringements are steady each quarter. Dog renewals were marginally lower this year than last year.

There is expected to be an increase in Debtor Payments as the implementation of electronic invoices will now direct more customers to eServices for payment.

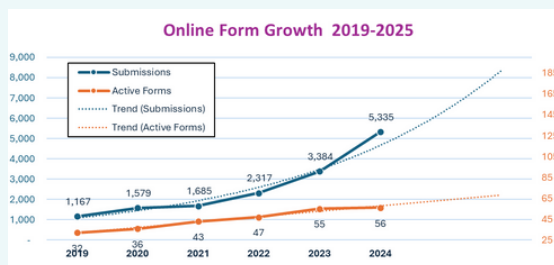


Snap shot online forms

60
Online
Forms

5000 Submissions
growing at a rate of 1000
submissions each year.

Online Form Growth



Top 10 submissions

Top 10 Submissions		2023	2024	2025 (%)
1	Cooee Point Camping Request	1104	1167	605
2	Accounts Receivable - Invoice Request Form			311
3	Food Business Rego/Renewal	153	171	151
4	Rates - Change of Delivery Address	98	119	115
5	Draft West Park Precinct Masterplan - 2025			106
6	Enquiry - Venues	76	102	104
7	Building Information Request	70	72	91
8	Event Submit	96	92	82
9	Volunteer Registration		51	77
10	Cruise Ship - Volunteer Availability	29	90	75

Discover Burnie App

From 1 July - 30 Sep 2025, the Discover Burnie app had 1,233 views from 459 visitors.

The highest viewed page of the quarter
Visit the Little Penguins

Of 12 users asked for feedback on the guide, 100% said they 'loved' the guide.

(Q2)
1,207 views from 502 visitors.

(Q2)
Visit the Little Penguins

(Q1)
2,804 views from 1,095 visitors.

(Q1)
Platypus spotting

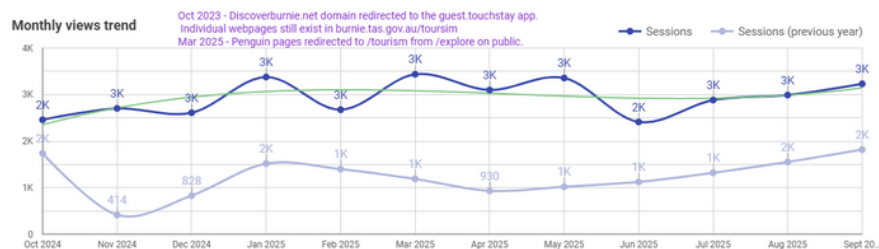
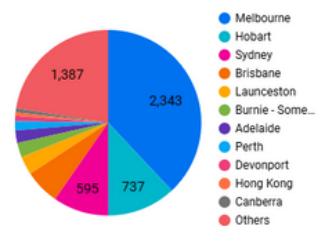
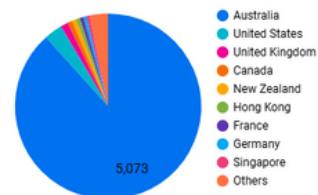
Page title	Jul 2025	Jun 2025	May 2025	Apr 2025
Visit the Little Penguins Discover Burnie	302	734	1K	1.3K
Top Of The Town Hotel Motel Discover Burnie	62	172	216	156
Platypus Spotting in Burnie Discover Burnie	46	131	163	252
Where to see platypus in Tasmania Discover Burnie	36	185	205	160
Burnie Central Townhouse Hotel Discover Burnie	59	175	169	180

April - May - June

Most popular pages

Page title	Jul 2025	Aug 2025	Sept 2025
Visit the Little Penguins Discover Burnie	921	863	1K
Top Of The Town Hotel Motel Discover Burnie	248	172	207
Burnie Central Townhouse Hotel Discover Burnie	222	196	139
The Makers Technology and Innovation Hub Discover ...	180	135	192
Platypus Spotting in Burnie Discover Burnie	178	158	156
Activites in and around Burnie Discover Burnie	136	164	160
Where to see platypus in Tasmania Discover Burnie	122	136	128
Burnie Holiday Caravan Park Discover Burnie	98	94	182
Weather in Burnie Discover Burnie	133	132	126
What's on Discover Burnie	120	98	162
Top 10 best things to do in Burnie Discover Burnie	121	106	122
Fishing Spots around Burnie Discover Burnie	58	148	126
Wild on West Dinosaur Park Discover Burnie	108	94	114
Self Drive Day Trips Discover Burnie	104	76	126

Where are they from?



Page title	Oct 2024	Nov 2024	Dec 2024	Jan 2025	Feb 2025	Mar 2025	Apr 2025	May 2025	Jun 2025	Jul 2025	Aug 2025	Sept 2025	Oct 2025	G...
Visit the Little Penguins...	35	46	26	48	72	669	953	708	504	645	585	736	415	5,442
Platypus Spotting in Bu...	416	547	384	496	361	346	213	123	100	119	128	104	65	3,402
Where to see platypus L...	81	176	218	257	201	115	119	157	135	85	95	93	101	1,833
Burnie Central Townho...	49	98	117	117	112	192	126	139	125	142	145	99	70	1,531
Top Of The Town Hotel ...	55	83	69	58	109	129	120	152	124	192	122	153	53	1,419
The Makers Technolog...	86	97	103	92	117	128	141	67	90	116	89	135	62	1,323
What's on Discover Bur...	130	152	167	112	75	126	94	53	45	61	67	104	78	1,264
Activites in and around ...	147	135	63	66	35	83	71	65	87	101	114	131	79	1,177
Self Drive Day Trips Dis...	103	91	71	203	140	86	76	65	61	70	49	92	50	1,157
Weather in Burnie Disc...	52	83	89	62	95	67	76	104	130	110	105	92	81	1,146

Burnie INSTAGRAM

Strategy

The Instagram strategy seeks to enhance the council's human connection more effectively than we can on Facebook, sharing the narratives of our region and our community. Our goal is to provide valuable content by embracing creativity, and empowering the council with a stronger voice and distinctiveness. Demonstrate that the council is engaged with the community, actively present, and embodies a sense of 'the people'.



Snap shot

Total Views - **25.8K** (up 50.6%)Total Reach - **2.4K** (up 43%)Total Content Interactions - **314** (down 6.3%)Followers - **432** (down 25%)

Goals

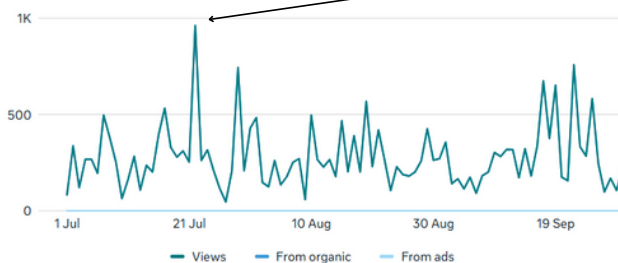
- Use Instagram reels as a fresh approach to draw in audience from not just Burnie area, but AUS wide
- Engage a younger demographic through launching an Instagram account
- Build an online community through Instagram
- Promote events and rebuild culture in the city around community events through having coverage of key events on Instagram
- Personalise the council through the Instagram account

110

Total Content

455Page Visits
(up 6.6%)

Top content by views

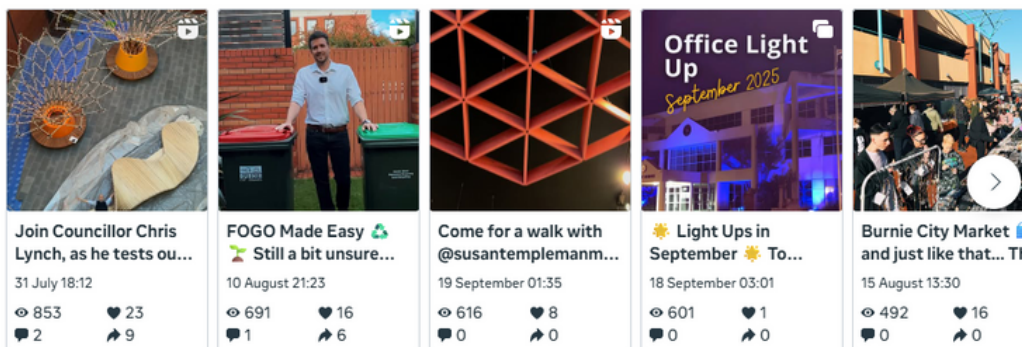


Views breakdown
1 Jul - 30 Sep

Category	Views	Change
Total	25,766	↑ 50.6%
From organic	25,766	↑ 50.6%
From ads	0	0%



Top content on Councils Instagram page is **tourism based**. Compared to our Facebook page which tracks higher views and engagements on local government messaging.



Burnie Facebook

Strategy

For the Burnie City Council, we are using Facebook to reach the community. This is a great platform for engaging with our community in a way that is more formal, and professional but still personal.

Facebook is also a more forum-based platform, which is why we use it to post all of the relevant information to the community that they need to know. Then when someone wants to know something specific, they can come and search on our Facebook page.

We post short-form content that is engaging and easy to read and then link through to the relevant website pages where the reader can find more in-depth information when necessary.

We include a range of video-based content because this is the way people are consuming social media content. The videos are value-adding to the reader, rather than just information based. This stops the page from getting stale and over time will build more positivity within the community as they consume more of this positive form of content

Goals

- Increase awareness of the Burnie City brand and Council services
- Maintain effective communication and support for community and on behalf of stakeholders
- Promote investment opportunities and liveability in the Burnie municipality
- Build a positive community brand
- Increase the council's ability to gain feedback and engage with the community



Snap shot

Followers - **12,628** (up 34%)

Published Content
156

Total Views - **1.4M**
(up 24.9%)

3-sec views
53.8K
(down 18.3%)

1-min views
7.4K
(up 18.5%)

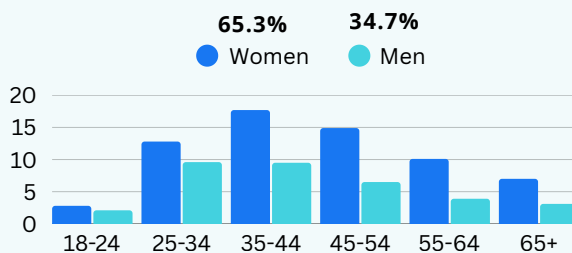
Total Content Interactions - **8.7K** (0%)

Page Visits - **47.8K** (up 21.5%)

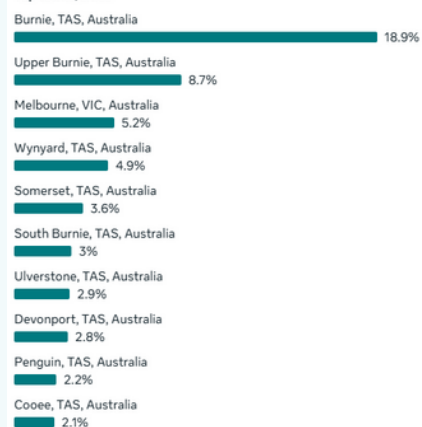
3K
Link Clicks
(up 64.8%)

Audience age and gender

In the current quarter, we had an increase of female followers 55+

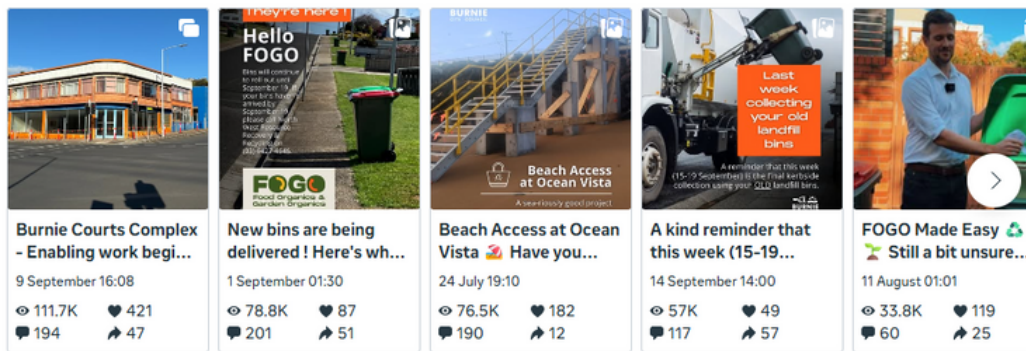


Top towns/cities

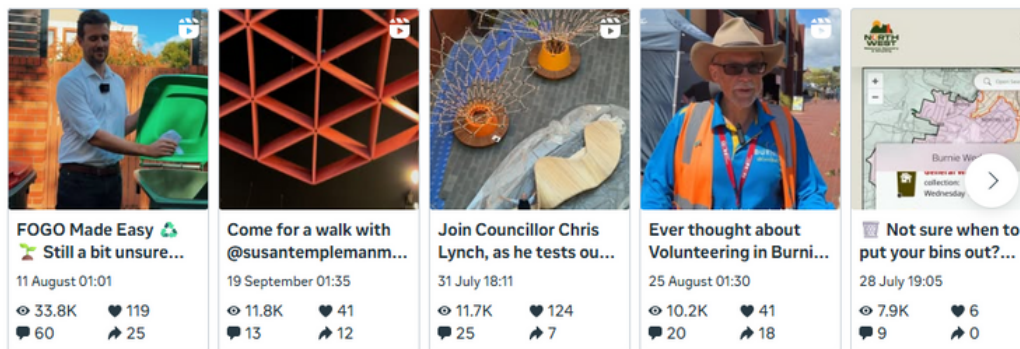


Burnie Facebook *Top Performers*

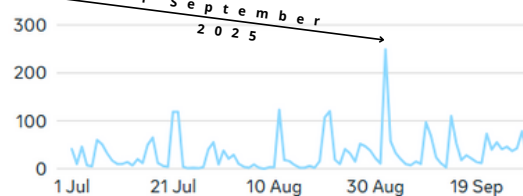
Top CONTENT by views



Top REELS by views

375
Link Clicks

Link Clicks ⓘ



Waste Service Changes - Community Campaign

Published Social media content since launching campaign (March 2025)

**Published**
28 March 2025**Stagnant Post**

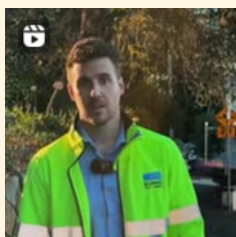
Content on the introduction of FOGO

Views - 19.5K
Interactions - 287
Link clicks - 123**Published**
24 April 2025**Video Content**

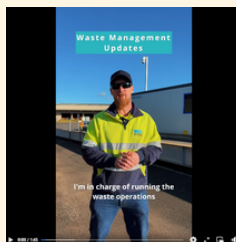
Content on FOGO Alliance

Views - 13K
Interactions - 92
Link clicks - 50**Published**
25 June 2025**Video Content**

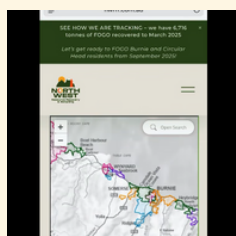
Content on APBE 2025-26

Views - 7.3K
Interactions - 65
Link clicks - 60**Published**
26 June 2025**Video Post**

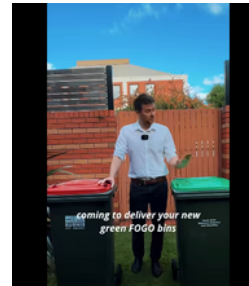
Content on Waste Service Change details

Views - 20K
Interactions - 236
Link clicks - 258**Published**
1 July 2025**Video Post**

Content on Tip voucher changes and BWMC loop

Views - 25K
Interactions - 286
Link clicks - 47**Published**
29 July 2025**Video Post**

Content on NWRRR interactive collection map

Views - 8K
Interactions - 19
Link clicks - 121**Published**
11 August 2025**Video Content**

Content on the Go's and No's of FOGO

Views - 33.8K
Interactions - 237
Link clicks - 172**Published**
28 August 2025**Video Content**

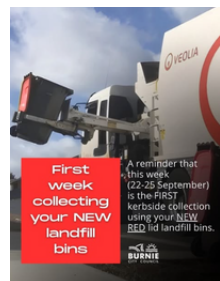
Content on Bins arriving in Burnie

Views - 6.8K
Interactions - 83
Link clicks - 77**Published**
1 September 2025**Stagnant Post**

Content on bins being delivered

Views - 79K
Interactions - 432
Link clicks - 373**Published**
1 September 2025**Stagnant Post**

Content on last collection of old landfill bin

Views - 57K
Interactions - 255
Link clicks - 184**Published**
22 September 2025**Stagnant Post**

Content on first collection of NEW landfill bin

Views - 30K
Interactions - 99
Link clicks - 105

Waste Service Changes - Community Campaign

Published
29 September 2025

Stagnant Post

Content on FOGO
collection weekViews - 33K
Interactions - 123
Link clicks - 87Published
5 October 2025

Stagnant Post

Content on Landfill and
Recycling weekViews - 9.3K
Interactions - 61
Link clicks - 27Published
22 September 2025

Stagnant Post

Content on last collection
of old landfill binViews - 30K
Interactions - 99
Link clicks - 105

Total campaign data

Views - 371.7K

Interactions - 2,380

Link clicks - 1,789

Further advertising:

- Website pages (Waste Service Summary, Waste Service Details, and updated BWMC pages)
- Direct emails to Outer Commercial business
- Letter on bins and post office boxes (no letter boxes available on properties)
- Posters with QR codes
- Letter with rate notices
- Burnie Connect newsletter
- Radio Advertising 7BU and Sea FM
- Primary School Visits

Summarised
Community
Feedback Q3

- **Service inclusion:** Residents in East and West Ridgley questioned why some areas were classified as Inzone or Outzone, seeking access to kerbside collection.
- **Access and suitability:** Concerns raised by residents on kerbside and roadside presentation, with steep or long driveways, mobility issues, or limited space for bin storage.
- **Cost and charging:** Confusion regarding service charges for the Landfill Levy, Municipal Waste, and Kerbside collection fee. Frequent feedback about the kerbside collection fee being charged, even if people don't want the bins or they are not used, with requests for opt-out or reduced fees.
- **Green waste and landfill allowance:** There are requests for a clearer explanation of what the municipal waste charge includes, especially since there are no vouchers available, and 100 kg of waste can be disposed of for free. Additionally, greater flexibility is requested for those bringing green waste to the Waste Management Centre to utilize the free 100 kg provision.
- **Collection timing and frequency:** Questions and complaints about missed or early collections, and adjustment challenges moving to fortnightly alternating schedules.
- **Bin size and additional capacity:** Requests for extra bins, particularly for households with medical, infant, or care needs.
- **Positive feedback:** Recognition from those who have been looking forward to FOGO, plus support for including Circular Road after the community survey process.

Active Transport Strategy - Community Consultation

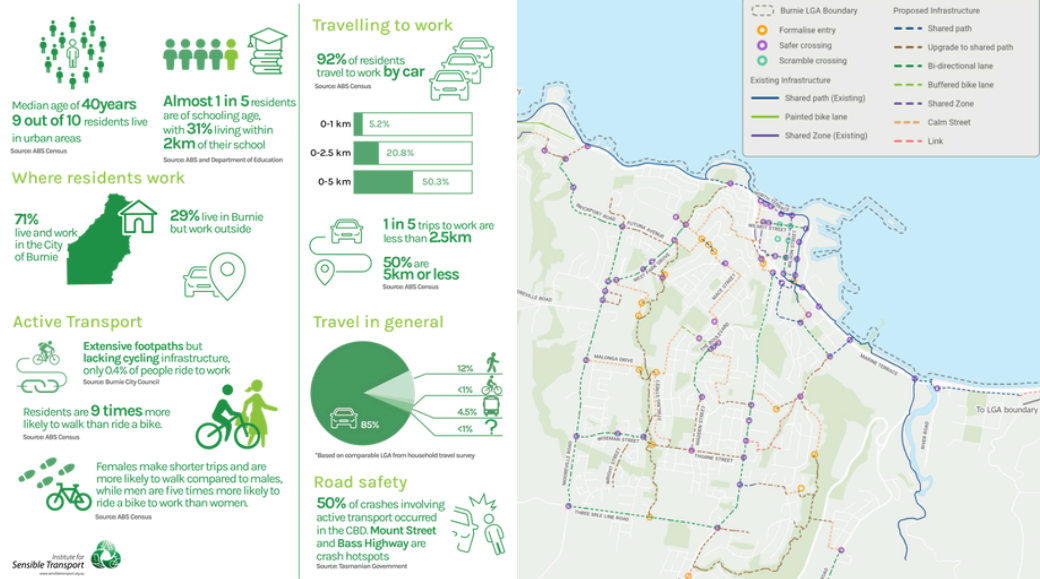
Round two of the Community Consultation on the Draft Active Transport Strategy (ATS) commenced on 17 July 2025, asking the community for their feedback on the Draft Strategy. This involved:

- Pop-Ups
- Online Surveys
- Professional stakeholder workshops
- Active transport working group
- Burnie Youth Council

One of the major key finding was:

Over 90% of community said they want improved walking, wheeling and riding infrastructure and supportive programs.

Burnie Active Transport Strategy Transport and Demographics



Changes were made to the Draft ATS after listening to feedback provide by key stakeholders and the community:

General updates

- Added tabs to the issues and opportunities graphics from Cradle Coast Authority feedback
- A glossary of key terms has been included

New actions added

- Investigate developing street design standards to ensure low traffic streets, both existing and in new developments, are designed to support walking, wheeling and riding.
- Develop a secure bike parking program in the CBD that provides weather-protected and monitored facilities.
- Include active transport routes and paths in Council's operational maintenance schedule to ensure networks are regularly cleared of debris and kept in good condition.
- Support and encourage workplace travel behaviour initiatives that encourage walking, wheeling and riding.
- Investigate and promote crowdsourced safety reporting tools (e.g. Bike Spot) to identify unsafe locations on the active transport network.
- Investigate opportunities to provide resting areas along key active transport routes, particularly in areas with steep topography, including seats and shaded areas to support people of all ages and abilities.

The final ATS report will be provided to the Council Meeting in October for endorsement.

Burnie Connect

Acknowledging that not everybody accesses social media, the idea was conceived for **Burnie Connect** - an electronic newsletter delivered to the community and stakeholders by email, which was launched in May 2024.

For email newsletters, an average open rate by recipients is between 15% -25%.

The third edition of Burnie Connect was distributed on 2 October 2024 to 455 subscribers, with an open rate of 73%.

The fourth edition of Burnie Connect was distributed on 20 December 2024 to 477 subscribers, with an open rate of 57%. This rate fall is attributable to the delivery of the newsletter close to the holiday period.

The fifth edition was distributed on 28 February 2025 to 481 subscribers and received an open rate of 68%.

The sixth edition was distributed on 23 May 2025 to 487 subscribers and received an open rate of 46.2%.

With the success of the previous editions and feedback from the community, Council has increased the occurrence of Burnie Connect - with publishing two editions this quarter.

July
edition



Snap shot

490
Subscribers

74.7%
Open rate

79
Link Clicks

0.41%
Unsubscribe
Rate



September
edition



Snap shot

493
Subscribers

71.7%
Open rate

96
Link Clicks

0.2%
Unsubscribe
Rate

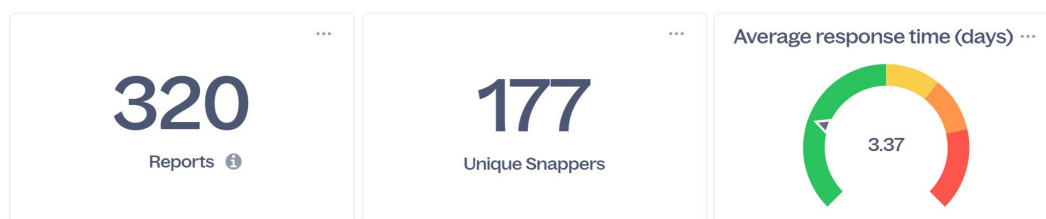




Snap Send Solve Reporting 2025-26 | Quarter 1

Snap Send Solve is a tool available throughout Australia that allows anyone to report an issue that needs attention. The issue is directed to the relevant authority, such as Council. Council's Customer Service Charter requires that all Snap Send Solve requests have a response provided within ten working days. The Customer Service Charter is available online at burnie.tas.gov.au

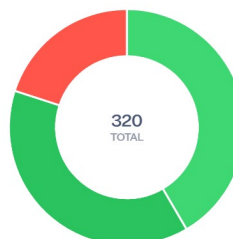
Summary



SLA Response Times

SLA means Service Level Agreement and refers to Council's Customer Service Charter. Response times are measured when a request is moved from 'viewed' to either 'in-progress' or 'closed'.

Within 14d Closed SLA	41.6%
Within 14d IP SLA	38.4%
Outside 14d SLA	20.0%

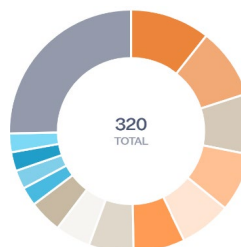


Operations Summary Reporting

The following chart is a breakdown of the reports sent into Snap Send Solve from the community. It comprises the incident types and average times to resolve a report by Council officers.

Burnie – Incident Type Pie Chart

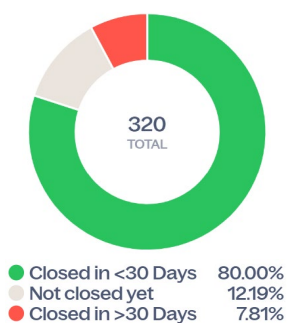
Pavement	10.63%
Animal - Deceased	9.38%
Facility - General Request	8.13%
Overgrown Vegetation	7.81%
Park - General Request	6.88%
Street Gutters / Storm Water	6.88%
Poles and Signage	5.94%
Parking - Illegal	4.69%
Fallen Tree	4.38%
Pothole	2.50%
Rubbish and Bins - General	2.50%
Trees - General	2.50%
Vandalism - General	2.50%
Other	25.31%



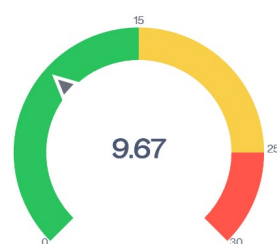
The above category 'Other' is made up of the following types of reports: Public toilet cleaning, facility maintenance, road repairs, waste and litter, infrastructure repairs, abandoned vehicles, illegal parking and environmental health issues.

Time Taken to Close Reports

Incidents Closed - Days

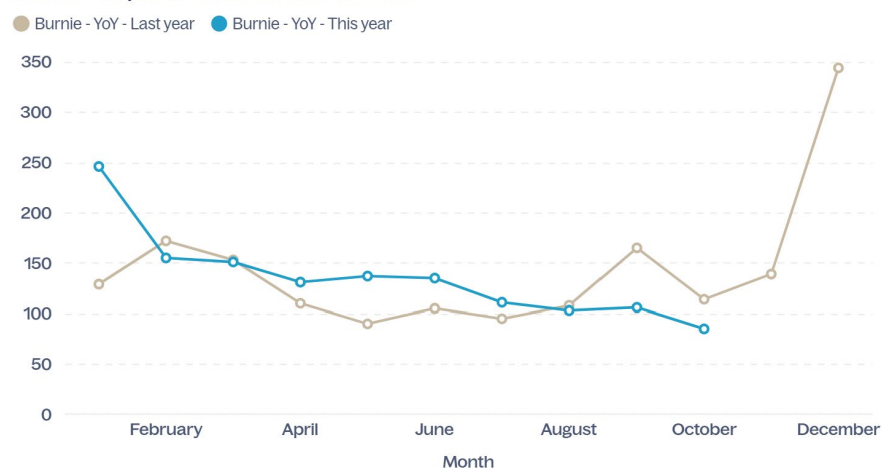


Average Time to Close



Number of Reports Year on Year

Burnie - Reports received Year on Year



OFFICERS' REPORTS**AO185-25 COMMUNICATIONS JOURNAL - OCTOBER 2025**

FILE NO: 2/17/3
PREVIOUS MIN:

1.0 RECOMMENDATION:

"THAT Council note the information contained in the Communications Journal for October 2025 as listed."

2.0 EXECUTIVE SUMMARY**Purpose and Background**

The purpose of the Communications Journal section of the Agenda is to provide Council with relevant general communication items received that need to be brought to the attention of Councillors.

Items contained in this monthly report are generally for noting.

3.0 CORRESPONDENCE FOR NOTING

The following correspondence is attached for noting:

3.1 Letter to the Premier of Tasmania, The Honourable Jeremy Rockliff MP

Correspondence to the Premier reinforcing the need for Government's ongoing support in Burnie during budget deliberations for the following:

- Burnie Court Facilities
- Urban Plan
- Spirit Preparedness Fund
- Emu Valley Rhododendron Garden

3.2 Letter from Local Government Association of Tasmania (LGAT) Chief Executive Officer (CEO), Mr Dion Lester

Correspondence relates to the LGAT resolution: Pensioner Rates Remissions motion that was put forward by Burnie City Council and passed at the LGAT General Meeting in 2024 and response from the Department of Treasury and Finance.

ATTACHMENTS

- 1 [↓](#). Premier of Tasmania - Burnie City Council Election Priorities
- 2 [↓](#). LGAT - Pensioner Rates Remissions

PLEASE QUOTE

Your Ref:

Our Ref:

25/26169

Enquiries: Office of the General Manager

egasupport@burnie.tas.gov.au

(03) 6430 5705

80 Wilson Street, Burnie Tasmania
PO Box 973, Burnie TAS 7320

ABN: 29 846 979 690

Phone: (03) 6430 5700

Email: burnie@burnie.tas.gov.auWeb: www.burnie.tas.gov.auWe value your feedback on our service.
Tell us about it at burnie.tas.gov.au/feedback

1 October 2025

The Honourable Jeremy Rockliff MP
Premier of Tasmania
Level 11
15 Murray Street
HOBART TAS 7001

Email: jeremy.rockliff@parliament.tas.gov.au*A hard copy will not be sent unless requested*

Dear Premier

Burnie City Council – Community Projects of Interest

Firstly, congratulations once again on your return to parliament. We look forward to working with you in the term ahead on projects that provide mutual benefit.

As government budget deliberations continue, it is important that we reinforce the need for your ongoing support in Burnie in the following areas:

Burnie Court Facilities

It is great to see demolition underway on the site earmarked for the new Court Facility in Burnie. We encourage the government to continue with the development as soon as possible to provide a much needed boost to the legal fraternity and service provision in this part of the state. We also indicate our desire for the government to demolish the existing court site and provide the land to Council for development of community open space.

Urban Plan

We are grateful for the financial support provided to Council for progression of the Urban Plan. Updates and progress reports will be provided as required. As mentioned at our Cabinet meeting, we strongly support the movement of Health and other government services into the CBD to provide much needed space for development at other sites, whether this be at Parkside or the NW Community Hospital site.

- 2 -

Spirit Preparedness Fund

Council waits with anticipation for the outcomes of the Spirit Preparedness Fund application process. This program, which offers funding to support tourism businesses and councils in preparing for increased visitor numbers from the new Spirit of Tasmania vessels, is a positive initiative that needs to be supported. Council is seeking funding to establish visitor services in Burnie, much needed for travellers on both the Spirits and cruise ships alike.

Emu Vally Rhododendron Garden

We were extremely pleased to hear that in the draft budget ongoing financial support had been allocated to the Emu Valley Rhododendron Gardens. These Gardens are one of the premier tourist destinations in the North West and an important asset for the community. We look forward to this financial support being ratified in the upcoming budgets.

Again, thank you for your ongoing support.

Yours sincerely



Teeny Brumby
MAYOR

Yours sincerely



Shane Crawford
GENERAL MANAGER



20 October 2025

Our Ref: DL | LR

Shane Crawford
Chief Executive Officer
Burnie City Council

Sent via email: scrawford@burnie.tas.gov.au

Dear Shane,

LGAT resolution: Pensioner Rates Remissions

I write to you in relation to the pensioner rates remissions motion passed at the Local Government Association of Tasmania (LGAT) General Meeting in July 2024. The motion put forward by Burnie City Council was moved by Flinders Council and seconded by George Town Council. It sought:

That LGAT calls upon the State Government to amend its Pensioner Rates Remission Guidelines to benefit pensioners who receive an eligible card within a financial year, post the 1st July with the benefit being prorated from the date of issue.

In acting on this resolution, we wrote to the Secretary of the Department of Treasury and Finance in August 2024 and again in March 2025 with further information.

In their response Treasury and Finance indicated that the Government has previously considered a pro-rata system for the Pensioner Rates Remission Scheme to be unfeasible due to the impact on legislation and systems, and increases to the administrative complexity and costs for councils and Government. Further, to ensure consistent treatment for taxpayers, implementing a pro rata approach would require that consideration be given to removing recipients from the scheme who become ineligible partway through the year, with councils potentially required to recoup funds from recipients who become ineligible partway through the year.

Given these factors, the Government maintained that use of a specific cut-off date continues to be considered the most appropriate practical means to determine eligibility in a way that balances the need to ensure consistent treatment for taxpayers and administrative simplicity.

I have attached Treasury and Finance's most recent correspondence on this matter, which outlines in further detail their rationale.

As a result of the Government's response this resolution will be closed. We will continue to monitor State Government policy and legislative processes for opportunities to raise this matter. If you have any questions, please do not hesitate to contact me.

Yours sincerely



Dion Lester

CHIEF EXECUTIVE OFFICER

CC: Flinders Council General Manager, Warren Groves

(warren.groves@flinders.tas.gov.au)

Department of Treasury and Finance

The Treasury Building
21 Murray Street HOBART TAS 7000
GPO Box 147 HOBART TAS 7001 Australia
Telephone (03) 6166 4444 Facsimile (03) 6173 0219
Email secretary@treasury.tas.gov.au Web www.treasury.tas.gov.au



Doc reference 25/49151

Dion Lester
Chief Executive Officer
Local Government Association Tasmania
via email: reception@lgat.tas.gov.au

Dear Mr Lester

I am writing in response to your email of 12 March 2025 regarding the resolution passed by Tasmania's councils requesting the State Government change the Pensioner Rates Remission to a pro rata system.

As you are aware, the use of a cut-off date means that some individuals need to wait until the next 1 July to receive the Pensioner Rates Remission. I acknowledge that this can cause frustration for some individuals entering the scheme, particularly in the context of ongoing cost of living pressures faced by many in our community.

While I note that this issue is being raised through local councils as a matter of importance, my understanding is that the Pensioner Rates Remission scheme meets the needs of most recipients. The Pensioner Rates Remission is not exclusively for aged pensioners but applies to people in a wide range of situations. In 2023-24 almost 55 000 Tasmanians benefitted from this support. By way of contrast, since 1 January 2022, Treasury has received only five ministerial correspondence requests requesting that the scheme applies on a pro rata system.

Furthermore, while a transition to a pro rata system may benefit a small number of individuals entering the scheme each year, such a change would adversely affect an unknown number of people who become ineligible for the scheme through the course of the year. For example, recipients of the Pensioner Rates Remission can become ineligible for the scheme if their income circumstances change such that they no longer hold a health care card. If a pro rata scheme is adopted, such recipients would have their final year of the Pensioner Rates Remission proportionally reduced to recognise the date that eligibility criteria were no longer met. It is unclear whether the change would overall increase or reduce support provided to Tasmanians by the Government.

As noted in my letter of 19 August 2024, a pro rata system for the Pensioner Rates Remission scheme is considered unfeasible due to the legislative and system changes required and the associated increase in administrative burden. At a minimum, this would include:

- drafting legislation with the Office of Parliamentary Counsel;
- Parliament enacting changes to amend the *Local Government (Rates and Charges Remissions) Act 1991*;
- Parliament enacting consequential amendments, as required, to related legislation;

2

- a review of the *Local Government (Rates and Charges Remissions) Regulations 2024* to determine any consequential changes necessitated by the statutory amendments;
- the State Revenue Office to develop an appropriate IT solution to administer Pensioner Rates Remissions on a pro rata basis. The complex changes required are expected to be accompanied by broader systems updates, testing and internal training;
- Local Governments implementing similar IT systems changes to complement and adapt to the new IT solution adopted by the State Revenue Office. While yet to be scoped, implementing these changes is likely to collectively amount to a substantial impost on councils given the range of IT systems used across Tasmania's 29 councils;
- increased administrative efforts to move from the current batch process used to check eligibility at certain points each year to the more manually intensive ad hoc eligibility checks required of a pro rata system;
- increased resources to ensure suitably trained personnel are available in the State Revenue Office to administer the new system; and
- further administrative resourcing for the State Revenue Office and Local Government to manage the additional manual eligibility checking that would be needed under a pro rata system to identify and remove recipients who become ineligible partway through the year due to changes of circumstance, and recoup the funds paid out to these individuals.

Overall, implementing the changes required to transition to a pro rata system would increase regulation and administration processes at significant costs to both the State and Local Governments. In this respect, the proposal also does not align with the Government's priority of reducing red tape and administrative burden.

The Pensioner Rates Remission is a valuable support to almost one in ten people in our community and, as such, I recognise that this issue is important to councils in Tasmania. I hope that this letter has provided some further clarity regarding why the use of a specific cut-off date is considered to be the most appropriate practical means to determine eligibility in a way that balances the need to ensure consistent treatment for taxpayers and administrative simplicity.

Yours sincerely



for Gary Swain
Secretary

21 March 2025

OFFICERS' REPORTS**AO186-25 NEW POLICY - WASTE COLLECTION AND MANAGEMENT SERVICES
POLICY CP-CCS-CG-076**

FILE NO: 4/14/2

PREVIOUS MIN:

1.0 RECOMMENDATION:*"THAT Council:*

- 1) Endorse the new Waste Collection and Management Services Policy, as presented; and*
- 2) Rescind the Burnie Waste Management Strategy 2018–2023 and instead adopt:*
 - i) the Cradle Coast Waste Management Group Strategy 2023–2028 (for which Council endorsed support at its 27 September 2022 Council Meeting, Agenda Item AO173-22); together with*
 - ii) the Tasmanian Waste and Resource Recovery Strategy 2023–2026 and the National Waste Policy 2018 as Burnie's guiding strategic waste management framework."*

2.0 EXECUTIVE SUMMARY**Purpose and Background**

The purpose of this report is to consider the introduction of a new Waste Collection and Management Services Policy.

Council has historically relied on the Waste Collection and Management By-law to regulate kerbside waste collection and management services, with a focus on compliance and enforcement.

Recent service changes, including the introduction of FOGO, adjustments to commercial waste arrangements, and the transition from in-house to contractor delivery, make this an appropriate time to introduce a Waste Policy.

The Policy specifies service levels, roles, and responsibilities, providing clarity for the community, contractors, and Council. The policy provides a level of operational clarity, in support of the by-law.

Aligning Council's planning with the Cradle Coast Waste Management Group Strategy 2023–2028, the Tasmanian Waste and Resource Recovery Strategy 2023–2026 and the National Waste Policy 2018 will ensure that Burnie's approach is consistent with regional, state and national frameworks, reflecting best practice and supporting broader sustainability objectives.

Key Issues

Waste management is a core local government responsibility under the *Local Government Act 1993* to protect community health and safety.

The By-law ensures enforceability and compliance, while the new Waste Policy outlines operational standards, roles, and responsibilities.

The Policy promotes transparency, equity, and consistency in service delivery across residents, businesses, and community facilities.

The Policy strengthens contract management by setting clear service standards and performance expectations for contractors.

Together, the By-law and Policy establish a framework that supports Council's waste management goals, ensures accountability, and provides a smooth transition to contracted services. The By-law will be reviewed and updated as a separate process as the current by-law approaches expiry.

3.0 SUPPORTING INFORMATION

Strategic Alignment

This report aligns with Council's Strategic Plan Making a Better Burnie 2044 Goal 2 – Making liveable local communities, Goal 3 – Making sustainably built and natural environments, and the Enabler, Digital Transformation, to improve service delivery and efficiency.

The regional, state and national strategies provide the overarching framework, ensuring Burnie's waste management planning remains aligned with broader sustainability objectives, best practice, and legislative requirements.

Legal

Under the *Local Government Act 1993*, councils are responsible for providing waste management services that safeguard public health and safety.

Council currently has a Waste Collection and Waste Management By-law, No 2 of 2017. The By-law continues to ensure compliance and enforceability, while the supporting Waste Policy provides additional operational clarity.

Finance

There are no immediate financial implications arising from this report. Waste service charges will continue to be set annually through Council's annual budget process.

Relation to Council Policy

The introduction of a Waste Policy creates a new framework that complements the Waste By-law and the Rates and Charges Policy, ensuring consistency in service delivery.

Discussion

Council's approach has historically relied on regulation through the By-law. The new Waste Policy enhances transparency by detailing service levels, conditions, and responsibilities, while the By-law ensures compliance and enforceability.

This combined approach supports equitable charging, operational efficiency, and community clarity, while enabling stronger oversight of contractor performance.

Adopting the regional, state, and national strategies as the guiding framework ensures Burnie's waste management planning remains aligned with broader sustainability objectives, contemporary best practice, and legislative requirements.

Council adopted its rates and charges on 26 June 2025 under Item AO101-25, which includes the charge for the year for waste as part of the rating system.

Distinct from this, Council also adopted its Fees and Charges for 2025-26 on 26 June 2025 under Item AO101-25. Since that time, it has been identified that a separate service fee is appropriate and necessary for additional waste collection services to accommodate households with extenuating circumstances.

Council has delegated power to the General Manager under section 205 of the *Local Government Act 1993* to set fees for items not contained in Council's adopted Fees and Charges Schedule (Item AO084-22). It is therefore proposed that the following fees be set accordingly, for the remainder of 2025-26, and included into the Fees and Charges Schedule for Council review in 2026-27.

- Large households (6+ persons): Full set of three bins – \$28 inc GST per month (\$336 inc GST per annum)
- Short-term medical needs: Single landfill bin – \$20 inc GST per month (up to \$240 inc GST per annum)

Health and Safety

Waste management is a core local government responsibility due to its direct impact on community health and safety. Effective collection and disposal minimises risks from odour, pests, contamination, and illegal dumping. The By-law and new Waste Policy reinforce Council's commitment to these outcomes.

Risk

The introduction of a Waste Policy reduces risks associated with inconsistent service delivery, contractor performance, and community expectations by providing clear operational guidance.

Environmental Sustainability

The Policy supports Council's goals to reduce reliance on landfill, increase recycling, and divert organics, aligning with broader national, state and regional sustainability strategies.

Consultation


Relevant Council Officers, contractors, regional waste group, and external advisors have been consulted in the preparation of the attached draft policy. Councillors have reviewed the policy in a workshop.

Conflict Of Interest

No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

ATTACHMENTS

1 [↓](#). Draft Policy (New) - Waste Collection and Management Services

 BURNIE CITY COUNCIL	COUNCIL POLICY	
	Waste Collection and Management Services (DRAFT)	
	Approved By: Council	Document Code: CP-CCS-CG-076
	Doc Controller: Executive Manager Corporate Governance	Version: 1.0
	File: 4/14/2	Approved Date: TBC
		Next Review Date: + 4 years

1 PURPOSE

This Policy sets out Council's approach to delivering waste services across the Burnie municipality. It is guided by core principles that promote environmental responsibility, regional efficiency, clarity in service delivery, competitive neutrality, and the fostering of innovation and circular economy practices.

2 OBJECTIVE

The aims of this Policy are to:

- Apply charges and services consistently across ratepayers and service users, on a transparent, full cost-recovery basis
- Ensure waste services are viable, affordable, reliable, and sustainable, reaching as many users as practicable at the lowest practical cost.
- Encourage and support responsible waste management behaviours within the community.
- Support and align with regional, state, and national waste management strategies and initiatives.

3 SCOPE

This Policy applies to all rated property types within the Burnie municipality. It covers kerbside collections for general waste, recycling, and food organics and garden organics (FOGO); waste services for new developments; public place waste infrastructure; and waste services for Council-owned and operated facilities, Council-leased facilities, events, and the Waste Management Centre.

The Policy sets out service levels, conditions, applicable fees and charges, and the responsibilities of service users, Council, and contracted service providers.


4 POLICY

4.1 Roles and Responsibilities

Council is responsible for providing and managing waste services, infrastructure, customer support, and community education on waste management.

Service Users are responsible for using waste services correctly, ensuring bins are properly maintained, and preventing contamination of waste streams.

Contractors are responsible for delivering waste services in accordance with contractual requirements, service standards, and performance expectations.

 BURNIE CITY COUNCIL	COUNCIL POLICY	
	Waste Collection and Management Services (DRAFT)	
Approved By:	Council Executive Manager Corporate Governance	Document Code: CP-CCS-CG-076
Doc Controller:		Version: 1.0
File: 4/14/2		Approved Date: TBC
		Next Review Date: + 4 years

4.2 Waste Principles

Council is committed to delivering effective, economical, and sustainable waste services guided by the following principles:

- Environmental Responsibility: Reduce landfill waste and emissions by maximising recycling and diverting organics through FOGO.
- Efficiency and Cost Management: Support a regionally aligned collection system to control rising costs through shared services and operational efficiencies.
- Equity and Transparency: Maintain clear service boundaries and a consistent, cost-recovery model to ensure fair access and transparent service delivery.
- Competitive Neutrality: Support a fair market by providing any additional services on a fee-for-service basis, ensuring private operators are not disadvantaged, while also fostering innovation and promoting circular economy opportunities

These principles support Council's goals to reduce landfill, improve recycling, minimise contamination, and provide accessible, affordable services for the community.

4.3 Service Rates and Charges

Council sets waste management fees through its annual rates resolution, adopted as part of the Annual Plan and Budget Estimates in accordance with the Local Government Act 1993. Waste charges are applied in three components:

4.3.1. The Municipal Waste Charge


The municipal waste charge is implemented to cover the costs associated with various municipal waste services. These services include the maintenance of public bins, providing dog deposit bags, cleaning the Central Business District (CBD), managing waste at public events, addressing illegal dumping, street sweeping, and expenses related to the Waste Management Centre and landfill rehabilitation.

This charge is paid by all ratepayers within the municipal area.

4.3.2. The Landfill Levy

The Landfill Levy was established in response to the State Government's landfill levy, as part of the *Waste and Resource Recovery Act 2022*.

The Council recovers a portion of the costs associated with the landfill levy incurred through its waste transfer and public waste management operations by implementing this service rate charge across all properties.

 BURNIE CITY COUNCIL	COUNCIL POLICY	
	Waste Collection and Management Services (DRAFT)	
	Approved By: Council	Document Code: CP-CCS-CG-076
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	File: 4/14/2	Approved Date: TBC
		Next Review Date: + 4 years

4.3.3. Kerbside Collection Service Charge

All properties within the kerbside collection area, as defined by the Waste Collection Service Area Maps, are required to pay the Kerbside Collection Charge based on their zone.

This charge applies regardless of occupancy status or whether the service is actively used.

The charge covers collection, processing, and management costs and is designed to support landfill reduction while ensuring the service operates on a full cost-recovery basis.

Charges commence from the date Council receives the Certificate of Occupancy, unless an earlier service start date is agreed.

Opting out is not permitted within the collection area, as service viability relies on a minimum number of participating properties.

Fee remissions may be granted where kerbside collection is impractical (e.g., insufficient presentation space or lack of access), in line with Council's Rates and Charges Policy.

4.4 Kerbside Collection Services

Council has established three kerbside service zones, balancing cost efficiency with service coverage across the municipality. (Refer to the published Kerbside Collection zone map).

These zones are:


- CBD – Transitional waste services area;
- Inzone – Standard fortnightly 3-bin collection area;
- Outzone – Areas with no Council-provided kerbside waste services.

4.4.1. Serviced Inzone

All property types within the designated Inzone are entitled to access the standard kerbside collection service and are subject to a Kerbside Collection Charge, which is determined annually as part of Council's budget process.

The standard fortnightly kerbside collection service includes a 240-litre bin for each waste stream:

- Green lid – Food Organics and Garden Organics (FOGO)
- Yellow lid – Comingled Recycling
- Red lid – General waste to landfill



BURNIE
CITY COUNCIL

COUNCIL POLICY

Waste Collection and Management Services (DRAFT)

Approved By:

Council

Doc Controller:

Executive Manager
Corporate Governance

File:

4/14/2

Document Code:

CP-CCS-CG-076

Version:

1.0

Approved Date:

TBC

Next Review Date:

+ 4 years

4.4.2. Burnie Commercial Business District (CBD)

Due to limited storage and presentation space for bins, the CBD zone, which is bordered by the Bass Highway, will have a modified kerbside collection service during the transitional period.

This adjustment will be in place while the CBD Urban Plan is being developed, which will explore future options for waste service delivery.

Current services are available to CBD properties include:

- Waste – 240L weekly collection on a Tuesday
- Cardboard – <1m³ neatly stacked, weekly collection on a Tuesday evening

Provision of bins will be the responsibility of the business during this transition phase

4.4.3. Municipal Waste Services

Public Place Recycling

Council provides commingled recycling bins in high-traffic areas like shopping precincts and parks, serviced under the waste contract. Council will continue expanding public recycling where effective and may explore organics bins in community spaces in the future.

Community Recreation Reserves

Council will provide free public place garbage and recycling services to recreation reserves and ovals for litter management. The litter bins will be fixed, and it is the responsibility of the Committee of Management to ensure all bins are removed from the stands and placed out for collection.

Community Halls and Buildings


Will be allocated a waste service per space according to the service zone provision, the cost of which will be included in the lease fee.

4.4.4. Additional Waste Services

Council generally does not provide additional kerbside waste services beyond the standard offering, with such services typically available through private waste operators who can provide more flexibility than the 3-bin fortnightly collection through Council.

Properties within the CBD may apply to Council or private operators for additional collections. These services attract additional fees per pickup, invoiced monthly, and reviewed annually.

For residential properties with special waste needs, additional waste services should be sought through private arrangements in the first instance. In some extenuating circumstances, the Council may consider additional services on a case-by-case basis, at the General Manager's discretion, and these services will be invoiced and reviewed annually.

 BURNIE CITY COUNCIL	COUNCIL POLICY	
	Waste Collection and Management Services (DRAFT)	
Approved By:	Council Executive Manager Corporate Governance	Document Code: CP-CCS-CG-076
Doc Controller:		Version: 1.0
File:	4/14/2	Approved Date: TBC
		Next Review Date: + 4 years

4.4.5. Remissions

Properties outside the defined boundary will not receive Council kerbside collection services; residents are encouraged to arrange for private collection with a commercial waste operator or take their waste to the waste management centre.

Unoccupied Vacant Land and properties outside of the collection service zone will not be charged the fee for kerbside collection services.

A waiver for the Waste Collection Charge may be granted to properties that meet the eligibility criteria outlined in the Council's Rates and Charges Policy. This includes cases where there is insufficient space for collection either at the kerbside or within a multi-property development, or where access to the service is not provided due to safety concerns.

4.4.6. New Services

For new builds, or new land uses, property owners must notify Council upon receiving their Certificate of Occupancy, or land use approval, to arrange bin delivery. Kerbside collection charges will automatically apply from the date of occupancy, as recorded by Council.


Property owners may also choose to apply for the service before occupancy, in which case the waste service charge will be applied from the elected start date and reflected on rates accordingly.

4.4.7. Extensions to Kerbside Collection Routes.

Council may extend collection routes to service approved subdivisions within or adjacent to the Inzone. Council may also extend kerbside collection services where township growth or contractor efficiencies allow additional properties to be serviced without significant increases in operational costs, travel distances, or disruption to existing collection schedules:

When considering extensions to collection services, Council will assess the following factors:

- Whether roads are listed on Council's Register of Public Roads;
- The cost-benefit feasibility of the extension;
- Existing contractual arrangements;
- Physical suitability of roads, including pavement strength, width, alignment, gradients, bridge limits, and turning areas;
- The risk of damage to assets;
- The number and proximity of dwellings to be serviced;
- The impact on existing collection schedules; and
- The proximity to existing collection routes.

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		Next Review Date: + 4 years

Collection on private roads requires a formal access agreement between Council, the contractor, and the landowner.

4.5 Conditions of Service

The provision and use of Council-managed waste services are governed by the conditions outlined in this policy, in accordance with Council's Waste Collection and Management By-Law No. 2 of 2017 (and subsequent by-laws made), as well as the contractual arrangements with Council's waste collection contractor.

Waste contractors may use camera footage to support transparent, accurate, and timely reporting of collection activities, and can be used to identify instances of non-conformance.

4.5.1 Service User Conditions

The following conditions are consistent with Council's Waste Collection and Waste Management By-law No 2 of 17:

All bins remain the property of Council and must remain at the property to which they are allocated.

Bins are to be stored securely on the property when not placed out for collection.

Bins must be placed at the kerbside or an agreed collection point, the night before or by 6:00am on collection day, to ensure service.


Bins should be removed from the kerbside and into the property as soon as practical after collection.

Service users are responsible for keeping bins clean, hygienic, and properly maintained.

Where damage or loss occurs due to misuse or neglect, the cost of repair or replacement may be recovered from the occupant before the service is reinstated.

Once bins are placed at the collection point, Residents remain responsible until the bin is collected

It is the responsibility of the occupant or property owner to notify Council if a bin is missing or stolen. Council does not provide refunds for missed collections resulting from stolen or missing bins.

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File:	4/14/2	Approved Date:	TBC
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4.5.2. User non-Compliance / non-Conformance

Incorrectly Presented Bins

Bins must have the lid opening facing the street, without obstruction, with the lid closed and not be overfilled.

If incorrectly presented:

- First and second instances: Bin will be collected, with a notification via sticker or letter; Council will be advised.
- Third instance: Collection may be refused. The resident will be notified, and the Council informed with recommendations to resolve the issue, including alternative collection points if needed.

Contaminated Bins

Correct use of kerbside bins is essential to maximise recycling and avoid contamination, which can lead to loads being landfilled and higher service costs.

If contamination is identified, a stepped education and enforcement process consistent with regional practices will occur:

- Minor contamination: The bin will be collected, but a warning will be issued within two business days.
- First and second instances of gross contamination (over 10%): The bin will not be collected and stickered or a letter issued.
- Third instance: A formal warning letter will be sent, advising that the service will be suspended after any further incidents.
- Fourth instance: Council may suspend the service and remove the bin, with charges continuing until the occupant agrees in writing to correct their use.


Council aims to avoid service suspension by providing education and support to help residents reduce contamination.

Other Non-Conforming Bins

The contractor will not collect bins that:

- Contain hazardous, dangerous, or illegal materials
- Are non-approved bins
- Contain loose waste outside the bin
- Are excessively heavy

In these cases, a notice will be provided.

 BURNIE CITY COUNCIL	COUNCIL POLICY	
	Waste Collection and Management Services (DRAFT)	
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4.5.3. Replacement of Lost or Stolen Bins

All bins remain the property of Council and must stay with the property to which they are allocated. If a bin is missing when moving into a property, new occupants should first contact the property manager, previous owner, or conveyancer to arrange the return of the allocated bin(s). If the bin cannot be recovered, a replacement can be purchased by completing the Bin Replacement Form available from Council.

4.5.4. Contractor non-Conformance

The region operates a contact and education service for the community to ask questions or to report any issues with the contracted service.

Missed Bins

If a compliant bin is missed, customers must report it for rectification by the Contractor.

Partially Emptied Bins

If a compliant bin is not fully emptied, customers must report it for rectification. If items are loose and accessible, the contractor will rectify the issue. If items are wedged or stuck due to dampness or improper preparation, it is the responsibility of the user to resolve.

Damaged Bins


If a bin is damaged, customers must report it for assessment and rectification. Supporting information, such as photographs, may be requested to help determine whether the damage was caused by the contractor, normal wear and tear, or user misuse. Council's contractor will undertake repairs on damaged bins and replace any bins that are stolen or damaged whilst left out for collection.

4.6 Waste Management Centre

The Burnie Waste Management Centre provides waste disposal and resource recovery services for residents, businesses, and approved users within the municipality. It operates in line with Council's commitment to responsible waste management and equitable access for the community.

The Centre accepts general waste for landfill transfer, recyclables, green waste, and other approved recoverable materials, in accordance with Council's Waste Collection and Management By-Law No. 2 of 2017 (or subsequent by-law) and applicable fees and charges.

Council periodically reviews these services to ensure operational efficiency, environmental compliance, cost recovery, and fair access for all users.

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4.6.1. Fair Use Policy

To ensure fair access to the 100kg free waste allowance for residents and to help prevent misuse, the following Fair Use Policy applies:

Daily Visit Limits

A reasonable number of visits per day per vehicle applies to prevent excessive or commercial dumping under residential rates.

Waste Volume Limits

Residents may deposit up to 100kg per visit, with the expectation of one resident presenting their identification per vehicle load.

Lap Restrictions

The number of consecutive trips (laps) permitted within a single day may be limited to ensure fair access and prevent overuse of the facility by any one individual or household, with the expectation that there will be 60-minute intervals between loads if the resident is completing a reasonable clean out.

Refer to Council's By-law for further compliance details with use of the Waste Management Centre.

5 LEGISLATION


Local Government Act 1993
Environmental Management and Pollution Control Act 1994
Waste and Resources Recovery Act 2022.
Litter Act 2007
Environmental Management and Pollution Control (Waste Management) Regulations 2020
Waste and Resource Recovery Regulations 2022
Litter (Infringement Offences) Regulations 2020

6 RELATED COUNCIL DOCUMENTS

Kerbside Collection Zones Map
Waste-Collection-and-Waste-Management-By-law-No.-2-of-2017
Rates and Charges Policy
Rates Resolution

7 OTHER REFERENCES

National Waste Policy 2018, and Action Plan 2019
Tasmanian Waste and Resource Recovery Strategy 2023-2026
Cradle Coast WMG Strategic Plan 2023-2028

 BURNIE CITY COUNCIL	COUNCIL POLICY Waste Collection and Management Services (DRAFT) Approved By: Council Doc Controller: Executive Manager Corporate Governance File: 4/14/2 Document Code: CP-CCS-CG-076 Version: 1.0 Approved Date: TBC Next Review Date: + 4 years
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Policy Endorsement	
Responsibility:	It is the responsibility of the Director Operations to oversee the implementation of this policy. It is the responsibility of the Governance Unit to maintain this policy in the Corporate Document Framework.
Minute Reference:	TBC
Council Meeting Date:	TBC
Strategic Plan Reference:	Making a Better Burnie 2044 Goal 5: Democratic and engaged communities Key Direction 5.1: Leadership through communication, consultation, collaboration and facilitation
Previous Policies Replaced:	This is the first council policy relating to this matter.
Date of Commencement:	TBC
Publication of policy:	Members of the public may inspect this policy at the City Offices, or access it on Council's website: www.burnie.tas.gov.au

OFFICERS' REPORTS**AO187-25 ACTIVE TRANSPORT STRATEGY**

FILE NO: 4/21/8
PREVIOUS MIN:

1.0 RECOMMENDATION:

"THAT Council:

- 1) Endorse the Active Transport Strategy as attached to this report; and***
- 2) Approve construction of the proposed priority project (footpath widening) located at Burnie Park using the available allocated grant and budget funds."***

2.0 EXECUTIVE SUMMARY**Purpose and Background**

Burnie City Council engaged the Institute for Sensible Transport to develop an Active Transport Strategy

Key Issues

The Strategy aims to make walking, wheeling, and riding safer and more accessible throughout the municipality, through a connected network of inclusive infrastructure for all ages and abilities.

The Strategy has been informed by technical reports, site assessments, and stakeholder and community feedback. It outlines proposed infrastructure upgrades as well as behaviour change programs to encourage increased use of active transport.

Council has not spent all of the project funds completing the strategy. With the remaining budget, officers propose completing a priority project identified by the strategy at Burnie Park.

All project work must be completed by 30 December 2025.

3.0 SUPPORTING INFORMATION**Strategic Alignment**

This report aligns with Council's Strategic Plan Making a Better Burnie 2044:

- Goal 1: Making an active town centre
- Goal 2: Making liveable local communities

Legal

There are no direct legal implications or legislative requirements for Council arising from the recommendation in this report.

Finance

Burnie City Council has allocated \$250,000 in the 2026-27 capital program for the development and implementation of the Active Transport Strategy.

Council secured a \$200,000 grant from the Tasmanian State Government under the Better Active Transport Program, along with a \$50,000 Council contribution to total the budgeted amount.

After completion of the strategy document, approximately \$125,000 remains available for works, proposed to be used for upgrading the walking track at Burnie Park.

Relation to Council Policy

There are no Council policies relating to this report.

Discussion

The Strategy proposes upgrades to footpaths, shared paths, bike lanes, and pedestrian crossings, as well as programs for education, promotion, and grant opportunities. These initiatives aim to support safer, more convenient active transport and address community concerns.

The vision of the Burnie Active Transport Strategy is:

“By 2040, walking, wheeling, and riding in Burnie is safe, accessible, and convenient, supporting a connected and active community.”

To achieve this vision, the Strategy outlines guiding principles, strategic objectives, and a detailed action plan.

Following the completion of the strategy, Council has a significant portion of the budget remaining. Officers have received approval from the grantors to utilise these remaining funds on projects identified within the strategy. Any selected project must be achievable and completed by the end of December 2025.

After consultation with the Active Transport Strategy Working Group, officers recommend that the remaining budget be used to fund the widening of approximately 250–300 metres of footpath at Burnie Park, which will provide a more accessible pathway connection

between West Park and Park Grove. The proposed works are both achievable within the required timeframe and will fully utilise the remaining funds. The location is shown in Figure 1 below.



Figure 1

Health and Safety

There are no health and safety impacts resulting from this report.

Risk

Given the short timeframe for project delivery, if Council chooses not to proceed with the proposed project at Burnie Park, the remaining grant funding will need to be returned to the State Government.

Environmental Sustainability

There are no environmental sustainability impacts to be considered in this report.

Consultation

The Active Transport Strategy was developed in consultation with the Active Transport Strategy Working Group appointed by Council, and the broader community through a public consultation process.

Conflict Of Interest

No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

ATTACHMENTS

1 [!\[\]\(23d9fc146e83b5c3013cfa32c784f8d5_img.jpg\)](#). Active Transport Strategy



Active Transport Strategy

Burnie City Council

August 2025

Institute for
Sensible Transport



The Institute for Sensible Transport acknowledges the people of the Wurundjeri Woi Wurrung language group of the eastern Kulin Nation on whose unceded lands we work.

We respectfully acknowledge their Ancestors and Elders, past and present.

We also acknowledge the Traditional Custodians and their Ancestors of the lands and waters across Australia where we conduct our business.

Institute for Sensible Transport

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202/26 Rokeby Street, Collingwood
Melbourne, Australia VIC 3066
E: info@sensibletransport.org.au
www.sensibletransport.org.au

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Executive Summary

Burnie City Council has developed this Active Transport Strategy (ATS) to grow the contribution walking, wheeling and riding play in our community over the next 15 years. The Strategy lays out Council's commitment to creating more opportunities for people to choose active transport to get around Burnie.

What does the Burnie Active Transport Strategy cover?

The Burnie Active Transport Strategy details Council's plan to provide more sustainable transport options. It covers upgrades to infrastructure, such as footpaths, shared paths and bike lanes, as well as policy changes, education programs and promotional activities.

The Strategy is funded by the Tasmanian State Government, through the *Better Active Transport Program*. This is Council's first Active Transport Strategy and will guide our decision making over the next 15 years.

What is Active Transport?

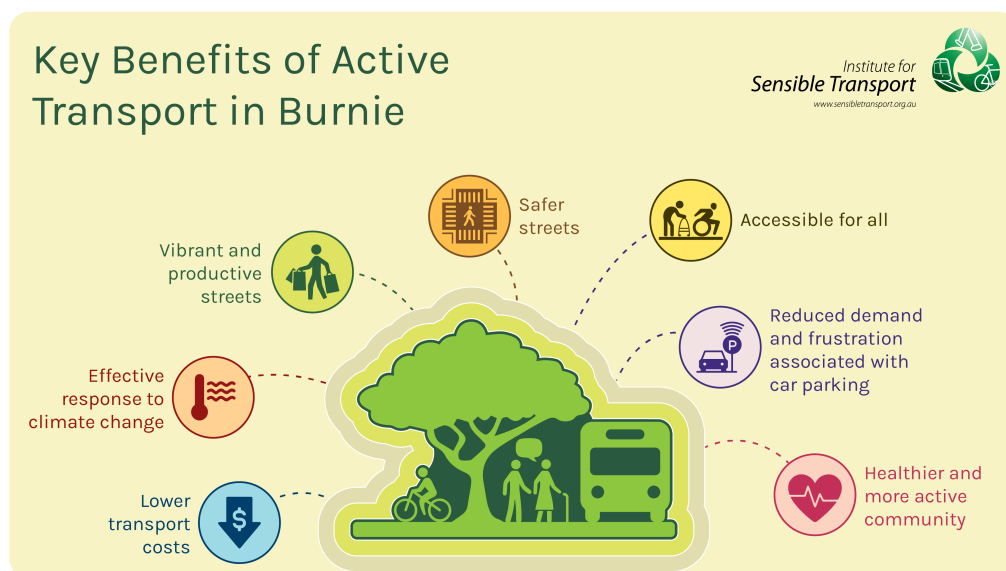
This Strategy defines active travel as any form of land transport that requires some form of human propulsion; principally walking, wheeling and riding. This also includes e-bikes, the use of wheelchairs, mobility scooters, walking aides, or other small personal mobility devices.

A glossary of some common active transport terms has been provided in Appendix A.1

What the Strategy will do

There are a growing number of reasons why Council is taking action to give more people the option to choose walking, wheeling and riding. Developing a strategic approach to increase walking and cycling will deliver broad and lasting benefits for the community, as outlined in Figure 2.

This Strategy outlines why the creation of a more walking, wheeling and riding friendly community aligns with our wider strategic ambition to become a more vibrant, people focused place to live, work and visit.



Benefits of active travel for Burnie

Developing the Strategy

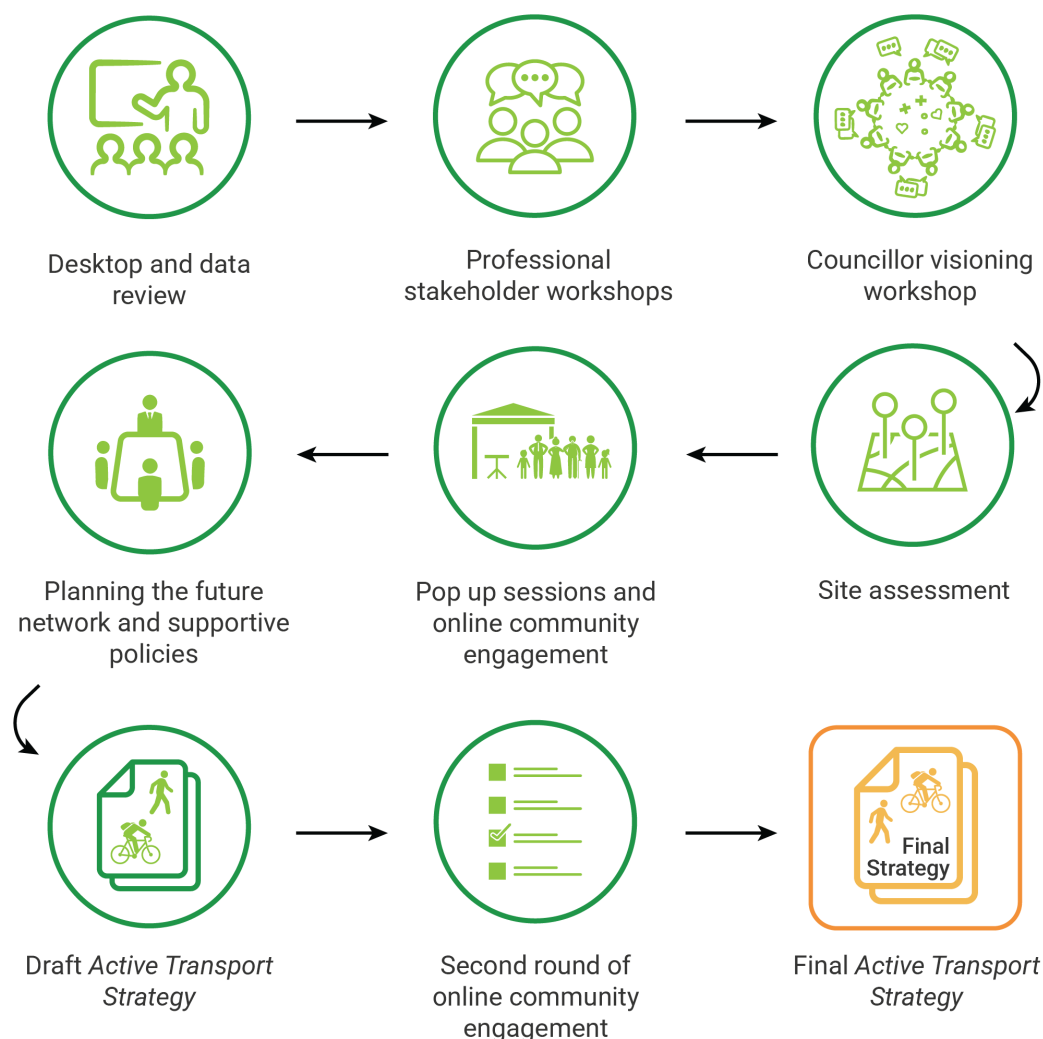
The Strategy has been developed in a multi-stage process that has involved engagement with Councillors, direct conversations with locals and visitors to Burnie, online community engagement and active transport experts.

The figure below provides a summary of the key stages in the process of developing the ATS.

What you told us

The consultation conducted as part of the development of the Strategy showed the community want to walk, wheel and ride more.

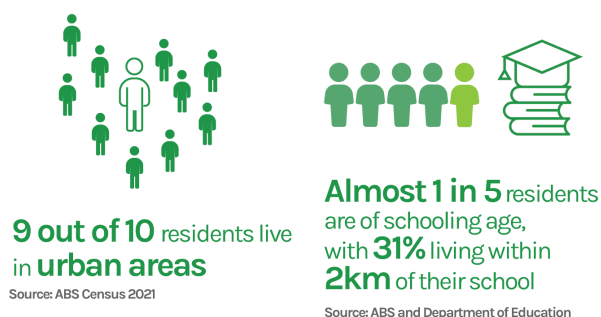
Overwhelmingly, safety related issues such as a lack of bike lanes or safe crossing points, and Burnie's hilly topography were cited as key reasons reducing people's participation in active travel.



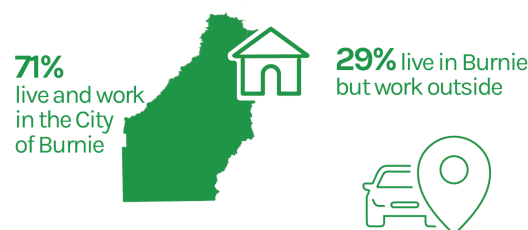
Developing the Strategy

How we currently live and move around Burnie

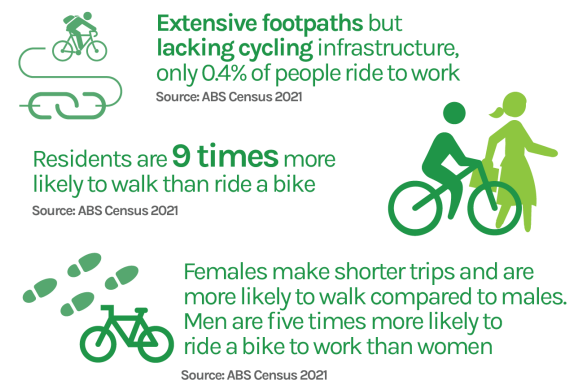
For journeys to work in Burnie, around 4% are currently done by foot and less than 1% of trips are by bicycle. However, this masks the widespread use of walking, wheeling and riding for many of the trips that take place for other purposes (e.g. going to the shops or visiting friends). The infographic below provides a snapshot of key demographic and movement data relevant to walking, wheeling and riding in Burnie.



Where residents work



Active Transport



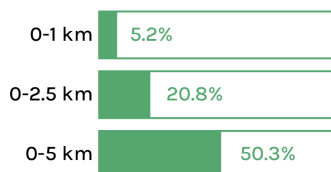
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Snapshot of travel and demographics in Burnie

Travelling to work

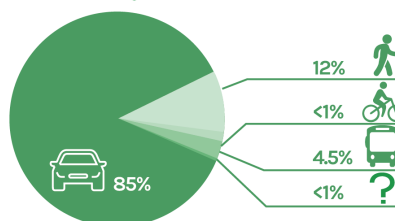
92% of residents travel to work **by car**

Source: ABS Census 2021



Travel in general

For trips of any purpose, **85%** are by car



*Based on comparable LGA from household travel survey

Road safety

50% of crashes involving active transport occurred in the CBD. **Mount Street** and the **Bass Highway** are crash hotspots

Source: Tasmanian Government

Vision

The vision for the Burnie Active Transport Strategy is that by 2040:

Walking, wheeling and riding in Burnie is safe, accessible, and convenient, supporting a connected and active community.

The riding network

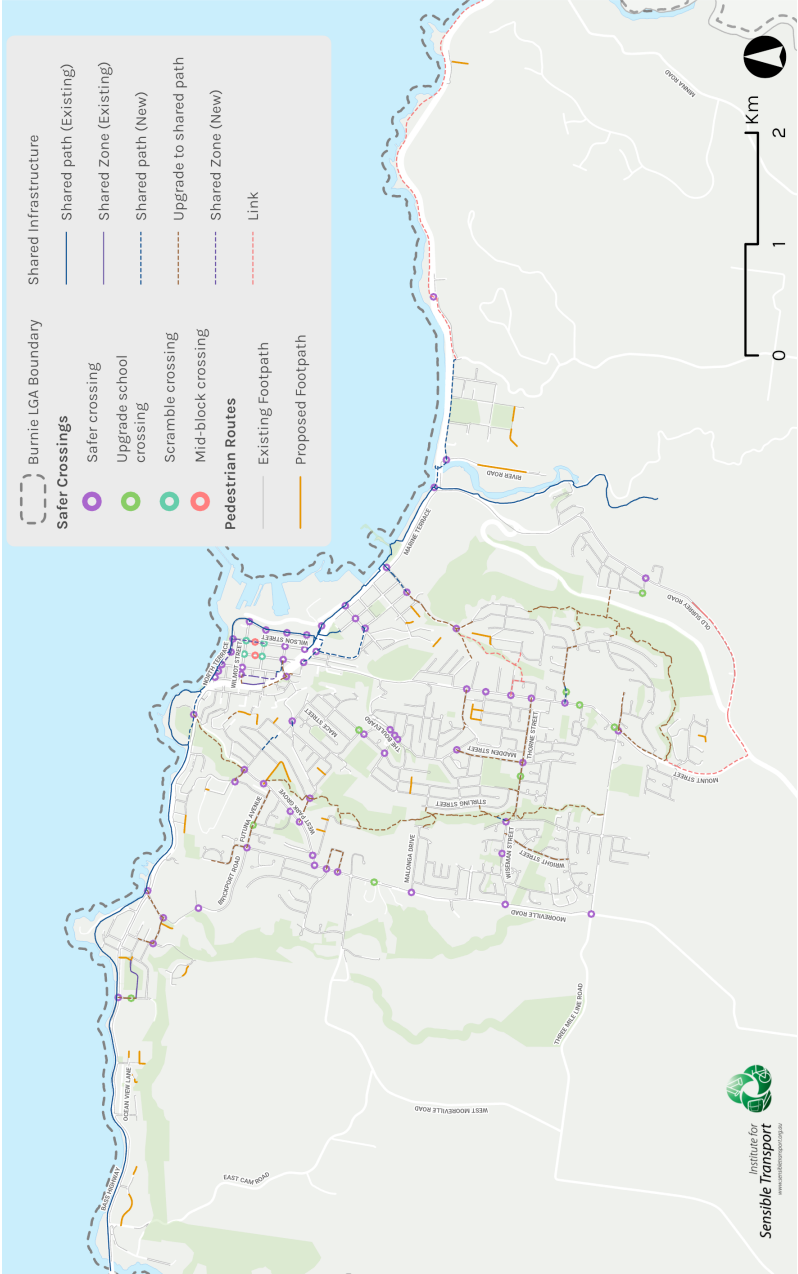
The proposed riding network is outlined in the figure below. The proposed infrastructure aims to create a safe, connected network for people of all ages and abilities. The network will support everyday riding by connecting residential areas to key destinations such as CBD, schools, hospitals and smaller activity centres. For detailed information on the proposed riding network, refer to Section 8 of this Strategy.



Proposed Burnie riding network

The walking network

The proposed walking network is shown in the figure below. It focuses on identifying streets without footpaths for upgrades to improve pedestrian safety and accessibility. Several intersections and crossing points have been identified for potential safety enhancements and to better prioritise pedestrian movement.



Proposed Burnie walking network



1. Overview

Burnie City Council (BCC) has developed this *Active Transport Strategy (ATS)* to grow the contribution walking, wheeling and riding play in our community over the next 15 years. The ATS lays out Council's commitment to creating more opportunities for people to choose active transport to get around Burnie.

Burnie. The ATS is jointly funded by Burnie City Council and the Tasmanian State Government, through the *Better Active Transport Program*.¹ This is Burnie's first ATS and will guide our decision making over the next 15 years.

This is Burnie's first Active Transport Strategy and will guide decision making for the next 15 years.

1.1 What is Council's ATS

The ATS details Council's plan to provide more sustainable transport options. It covers upgrades to infrastructure such as footpaths, shared paths, trails, bike lanes and pedestrian crossings. It also provides guidance on policy changes, education and promotion activities intended to give more people the freedom to walk, wheel and ride in

Figure 1 outlines Council's *Strategic Planning Framework*. The ATS sits within Council's broader framework of local strategic documents. It has been prepared to support the *Burnie 2044 Strategic Plan* and the *pataway/Burnie City Centre Urban Plan*.



Figure 1 Burnie City Council's Strategic Planning Framework

Source: Burnie City Council

¹https://www.infrastructure.tas.gov.au/network_management_and_freight/network_planning_and_performance/projects_and_programs

1.2 Why we need an ATS

There are a growing number of reasons why Council is taking action to provide an environment to give more people the option to choose active transport. Walking, wheeling and riding bring a wide range of interconnected social, health, environmental and economic benefits to communities.

We recognise that there is a need for a transport system that provides multiple options to get people where they want to go, beyond just the car. We don't currently have a strategy to guide our active transport decision making. By creating a strategic approach to grow opportunities for people to walk and ride, the community will benefit, as outlined in Figure 2.

There is a need for a transport system that provides multiple options to get people where they want to go, beyond just the car.

1.3 What the ATS will do

The ATS provides a blueprint to guide Council's investment to help ensure walking, wheeling and riding is accessible to all, and is safe and encouraged, across all Council activities. This Strategy seeks to remove barriers and create an environment where active transport is a viable choice for people making local trips.

We know the community values the freedom of simply walking, wheeling and riding, whether for transport or leisure. The ATS provides a detailed set of inter-linked, prioritised and costed actions to build the walking, wheeling and riding network. It will help provide the community with the support they need to live healthier, more sustainable and connected lives.

Finally, some of the big picture actions included in this ATS will require additional financial support, from outside of Council. This Strategy will provide the foundation for advocacy actions that enable increased State and Commonwealth investment to help more people enjoy walking, wheeling and riding.

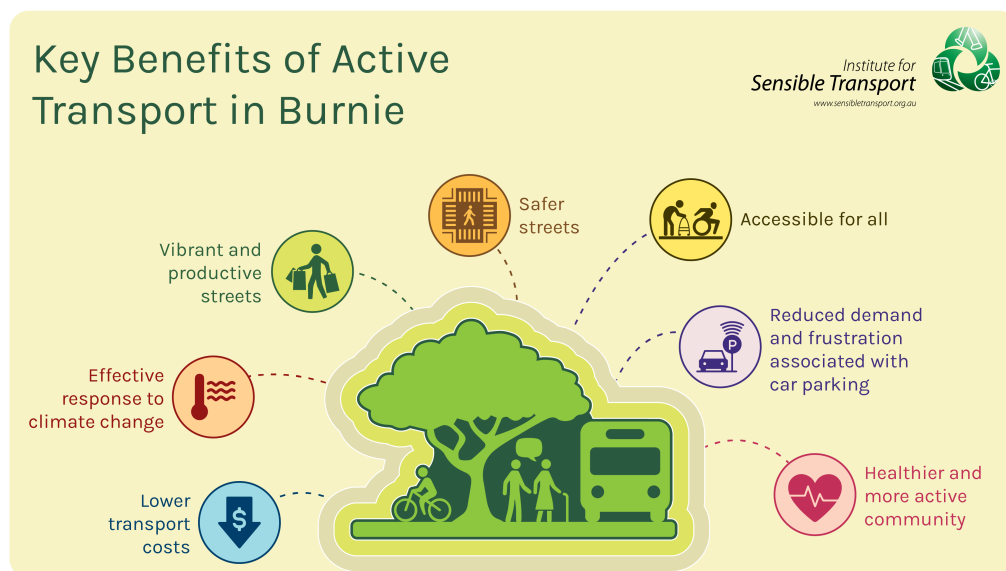


Figure 2 Benefits of active travel for Burnie

Source: Institute for Sensible Transport

2. Introduction



Burnie City Council is committed to making walking, wheeling and riding available to all members of the community. This ATS offers the blueprint for creating the conditions in which active travel is a safe, convenient and enjoyable mode of transport. This is especially useful for the many short to medium distance trips that take place every day in Burnie.

2.1 Current levels of active transport and opportunities for growth

Travel to work data is captured at every Census. It is the most complete dataset relating to transport

available in Australia. Figure 3 shows how the Burnie community travel to work in 2021. Only 4% of work trips were walked and less than 1% were by bike. Over 92% were made by car. However, this masks the widespread use of walking and bike riding for many of the trips that take place for other purposes (e.g. going to the beach, shops, visiting friends). This ATS seeks to capitalise on the strong community interest in walking, wheeling and riding and boost it even further.

This ATS seeks to capitalise on the strong community interest in walking, wheeling and riding and boost it even further.

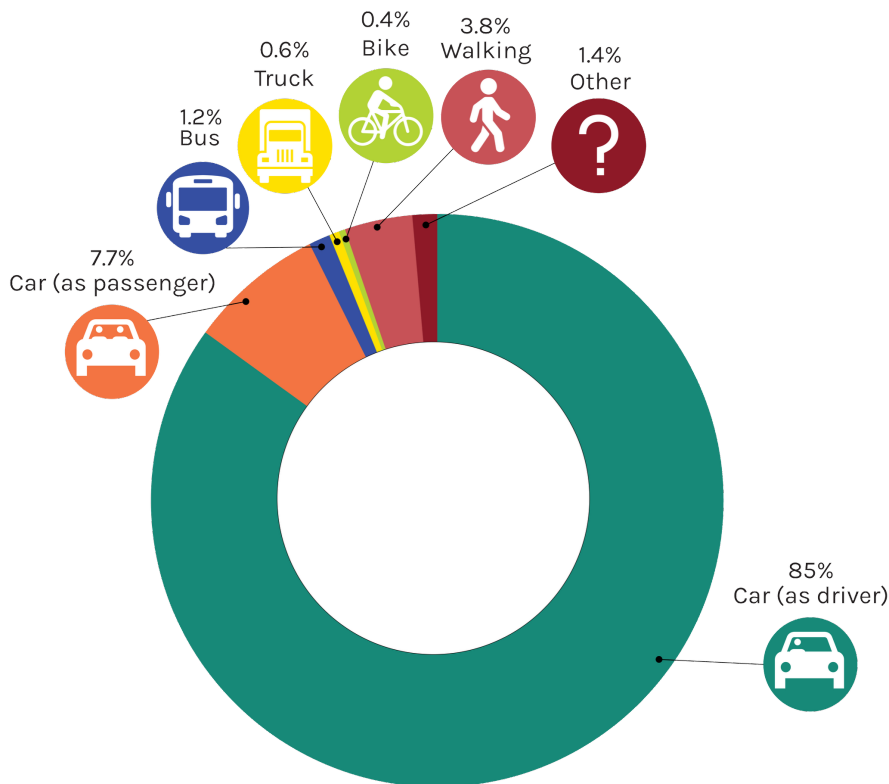


Figure 3 Journey to Work mode share, from Burnie
Source: ABS Census 2021

In 2021, 4% of work trips were walked and less than 1% were by bike. Over 92% were by car.

These localities make up just 9% of the total area but make up 90% of our population. Travel data also indicates that around half of journeys to work are less than 5km, a perfect distance for active transport.

While Burnie is a large regional municipality, most of our population is concentrated within the urban area. Figure 4 shows the most densely populated areas in Burnie.

Some 90% of the Burnie population live within 9% of the land.

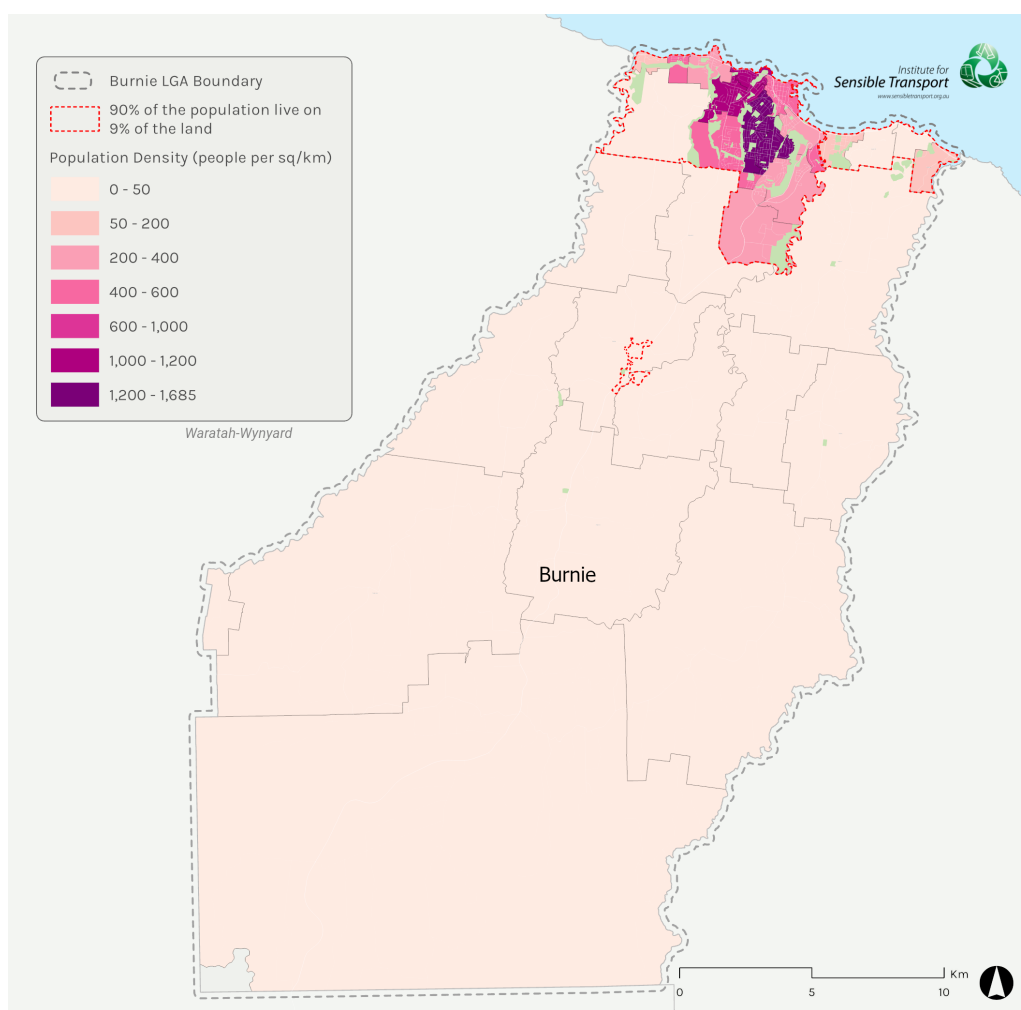


Figure 4 Population density

Source: Institute for Sensible Transport using ABS Census data

2.2 Safety

The community told us that more people would walk, wheel and ride if it was made safer. The Tasmanian State Government's *Towards Zero - Tasmanian Road Safety Strategy* has committed to working towards reducing the annual number of serious injuries and deaths on Tasmanian roads to fewer than 200 by 2026. The long-term vision is no one is seriously injured or killed on Tasmanian roads.²

In the last five years, there have been 1,369 crashes recorded across the Burnie region, with numbers increasing year on year since 2020. Of these crashes, 323 sustained an injury, 38 of which were considered serious. An additional six crashes resulted in fatalities.

Crashes involving people walking, wheeling or riding are mostly concentrated in and around the Burnie CBD. Around 45% of crashes involving active transport modes in Burnie occurred around the CBD area (see Figure 5). There have been four pedestrian fatalities in the last five years in the Burnie CBD.

A core objective of the ATS is to reduce risk and enable more people to safely enjoy the freedom of being able to walk, wheel and ride.

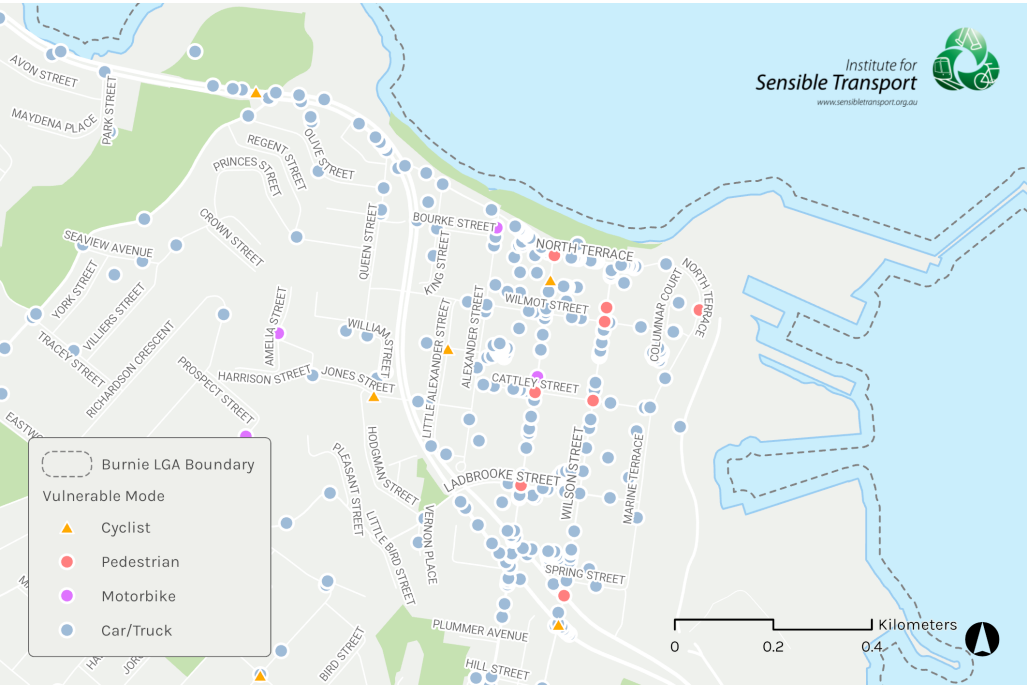


Figure 5 Crashes by mode of transport, Burnie CBD

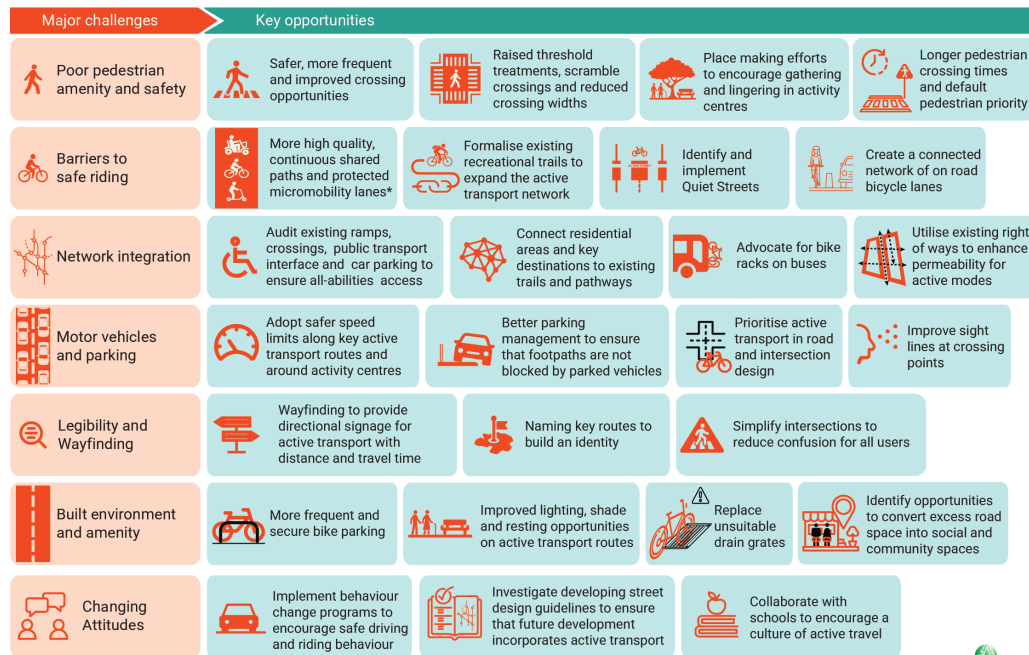
Source: Tasmanian government

² <https://rsac.tas.gov.au/about-us/tasmanian-road-safety-strategy-2017-2026/>

2.3 Summary of major challenges and key actions

A summary of the major challenges and key actions for a more active transport friendly Burnie is shown in Figure 6.

This synthesis was developed via the broad stakeholder engagement conducted as part of the development of this ATS, as well as the analysis of transport data and site assessment.



* Micromobility includes small footprint, low speed transport devices, such as bicycles, e-bikes, and e-scooters.

Institute for
Sensible Transport
www.instituteforsensibletransport.org.au

Figure 6 Challenges and actions

Source: Institute for Sensible Transport

2.4 How the ATS has been developed

The ATS has been developed in a multi-stage process that has involved engagement with Councillors, direct conversations with locals and visitors to Burnie, online community engagement and active transport experts. Figure 7 provides a summary of the key stages in the process of developing the ATS.

support the planning and delivery of active transport infrastructure. This ATS draws on these resources, such as the *Tasmanian Walk, Wheel, Ride* guidance, to inform the development of a network that meets the needs of a wide variety of users. Designing an inclusive network means catering for people of all ages, abilities, genders and backgrounds, as well as accommodating all trip purposes and all bike types (AAA).

The specific tools and guidance provided by the Tasmanian Government, and how they have informed the ATS are explored in more detail in Section 7 of this Strategy.

2.4.1 Tasmanian Government guidance

The Tasmanian Government has developed a range of policies, strategies and guidance documents to

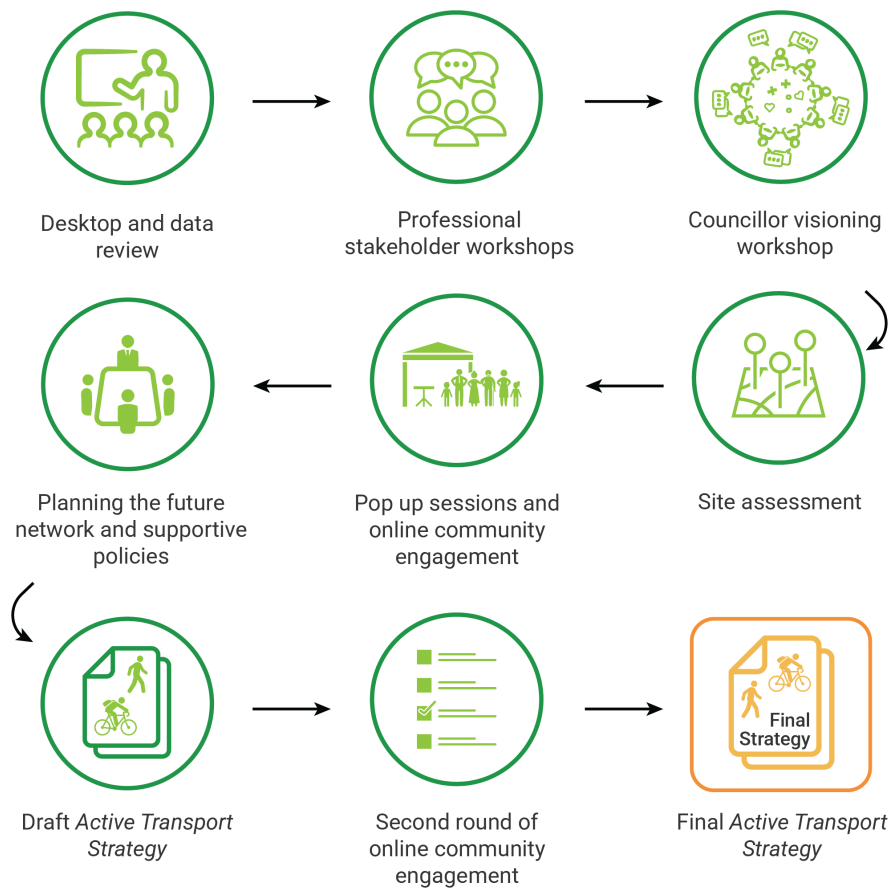


Figure 7 Multi-stage process for developing the ATS

Source: Institute for Sensible Transport

3. Vision, guiding principles and strategic objectives



The ATS is led by a vision, which is supported by a set of guiding principles and measurable strategic objectives.

Figure 8 captures the structure of how the vision, guiding principles, strategic objectives and actions fit together.

The vision is supported by the guiding principles. The guiding principles are underpinned by strategic objectives, which are designed to be measurable, time based and enable Council to measure progress towards achieving the vision of the ATS. The actions are the activities Council will pursue to create the change necessary to meet the future vision for active transport in Burnie, over the next fifteen years.

The remainder of this section identifies Council's vision for the Strategy, as well as the six guiding principles and nine strategic objectives.



Figure 8 Vision, principles, objectives and actions

3.1 Vision

The vision for the Burnie *Active Transport Strategy* is that by 2040:

Walking, wheeling and riding in Burnie is safe, accessible, and convenient, supporting a connected and active community.

Achieving this vision will not be easy and the ATS has a suite of guiding principles, strategic objectives and actions that aim to establish an accessible, convenient and connected Burnie.

3.2 Guiding principles

Figure 9 outlines the guiding principles developed to support BCC's vision for active transport.

-  Burnie is a vibrant people-focused place to live, work and visit.
-  Walking, wheeling and riding is the preferred choice for trips under 3km and streets provide safe, high-quality opportunities.
-  People of all ages and abilities have access to convenient and accessible transport choices.
-  Transport investment decisions are guided by their ability to support Council's commitment to being leaders in sustainability.
-  The walking, wheeling and riding network integrates seamlessly with public transport.
-  Safe Systems (Towards Zero) underpins the design of Burnie's transport network, consistent with Tasmanian State Government policy.

Figure 9 Guiding principles

3.3 Strategic objectives

The following set of strategic objectives provide Council with a series of measurable targets, to assist in evaluating progress towards more opportunities for active travel.

- 1. Increase the proportion of the population within 400m of a riding facility suitable for all ages and abilities (AAA), measured every five years, to 2040.
- 2. Increase the number of *Disability Discrimination Act 1992* (DDA) compliant pedestrian crossings and bus stops, measured every five years, to 2040.
- 3. Double the number of people who walk or cycle to work by 2040, consistent with Tasmanian government targets.
- 4. Double the proportion of short trips completed by walking, wheeling and riding to places of

education by 2040, consistent with Tasmanian government targets.

- 5. Double the proportion of the Burnie community residents that meet the national guidelines for physical activity through walking, wheeling and riding by 2040.
- 6. Lower transport emissions, consistent with 1.5 degrees of global temperature increase.³
- 7. Reduce the number of serious injuries and deaths on Tasmanian roads consistent with the Tasmania government Towards Zero – Road Safety Strategy 2017-2026 and the National Road Safety Strategy 2021-2030.
- 8. Increase the number of trips that integrate active and public transport.
- 9. Increase the proportion of bus stops which have adequate bike parking and increase the number of buses capable of carrying a standard size bicycle (see Figure 10).



Figure 10 Example of bike rack on bus

Source: CDC NSW

³ The 1.5-degree threshold is the target established in the 2015 *Paris Agreement*, a treaty in which 195 nations including Australia pledged to tackle climate change.

4. Transport mode hierarchy



This section presents a conceptual framework that can be used to inform the design of current and future streets, giving more people the option to walk, wheel or ride in Burnie.

4.1 Mode hierarchy

Road space is limited, and there is strong competition for this scarce resource. To assist in making consistent, transparent decisions that work to support our vision and principles, two transport mode hierarchies have been developed.

The first (Figure 11) is for the Burnie township. A separate mode hierarchy has been developed for our regional roads. This is in recognition of the different roles our urban areas, smaller townships and regional roads have in the transport network.

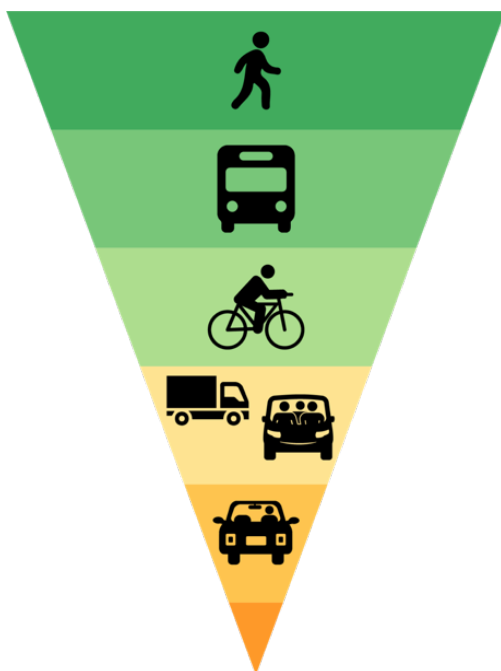


Figure 11 Mode hierarchy to towns and villages

Source: Institute for Sensible Transport

While the mode hierarchy within our towns prioritise sustainable mobility, our regional roads are better suited to supporting long distance travel and industry. Motorised transport will continue to

be the priority in road space allocation decisions on major regional roads.

The mode hierarchy for regional roads (Figure 12) indicates that where walking, wheeling and riding is accommodated, this will generally be on a separated piece of infrastructure parallel to the main corridor.

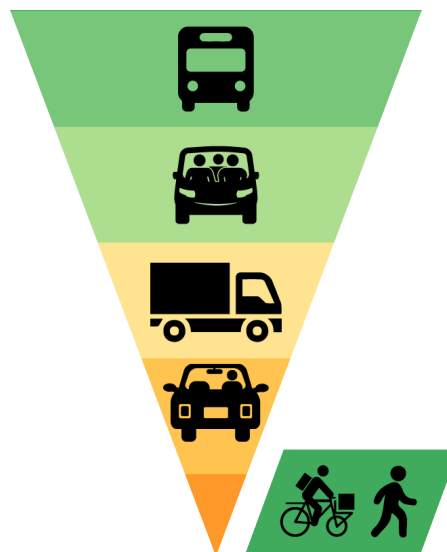


Figure 12 Mode hierarchy for regional roads

Source: Institute for Sensible Transport

In practical terms, these road hierarchies assist in determining how space is allocated between modes in situations where there is insufficient space to cater to every mode of transport.

The road hierarchies assist in determining how space is allocated between modes when there is insufficient capacity to accommodate all forms of transport.

5. What you told us



A range of stakeholders contributed to the development of the ATS. Engagement activities enabled a better understanding of walking, wheeling and riding behaviour. Participants were also asked their barriers to active travel and what would help to make walking, wheeling and riding a better choice. Face to face events took place at *SummerFest* and Council's *Food and Feedback* event. A series of online opportunities were also provided.

5.1 Who we engaged with

The creation of the ATS has been a team effort. Councillors, Council staff, professional stakeholders and the Burnie community have had the opportunity to tell us what would encourage more people to choose walking, wheeling and riding.

5.2 Key issues

The following three issues were frequently mentioned by those participating in engagement activities.

- **Lack of safe infrastructure:** Community members consistently identified a lack of dedicated bike lanes, separated paths and safe pedestrian crossings.
- **Challenging topography:** Burnie's hills were frequently identified as a challenge for active travel. This barrier is amplified for older adults, people with mobility challenges and families with young children.
- **Road safety:** Issues such as high vehicle speeds, poor lighting and an aggressive driving culture reduce people's feelings of safety and act as a barrier to people choosing active travel.

5.3 Key opportunities

Stakeholder feedback repeatedly identified the following opportunities for growing active travel participation in Burnie.

- **Capitalising on success:** Better connecting the Coastal Pathway with a wider active transport network will amplify the positive impact the Coastal Pathway has had on the community. The success of the Coastal Pathway is explored in Box 1.
- **Desire for more active travel:** Over 90% of survey respondents expressed a desire to walk, wheel and ride more often.
- **E-bikes and e-micromobility:** There is growing interest in e-bikes and other forms of e-micromobility, highlighting an opportunity to support their use through safe infrastructure and programs.

Celebrating our success – Coastal Pathway

Throughout the engagement, the community highlighted the Coastal Pathway as a major success story for Burnie. Many community members told us it was the sole reason they had started walking, wheeling and riding again. Since planning begun in 2009, the Coastal Pathway has grown to run from Burnie's western boundary at the Cam River to the Emu River in the east. Completing the pathway further east to Wivenhoe and beyond to Devonport remains a key priority for the community.

The success of the Coastal Pathway is a strong indicator that the community wants more safe and enjoyable active transport infrastructure. The ATS will build on the momentum by creating more opportunities for walking, wheeling and riding in Burnie.



Box 1 Celebrating our success

Photo: Institute for Sensible Transport

Over 90% of community respondents said they want improved walking, wheeling and riding infrastructure and supportive programs.

5.4 Barriers to walking and wheeling – what you told us

The barriers to walking and wheeling identified by the community in the online survey are shown in Figure 13. Topography was the most significant barrier, with almost 40% of respondents noting it is too hilly. Safety related issues also ranked highly, with 29% of respondents identifying a lack of safe crossings and around 25% concerned about safety and poor lighting at night respectively.

Additional barriers included distances being too far (21%), lack of shade (21%), needing to carry too many items (18%). Notably, responses in the *other* category were high (13%), with the majority relating to cars parked on footpaths (explored in Box 2).

Cars parked on footpaths

We heard your concerns about cars parked illegally on footpaths. Council recognises that this creates access and safety issues. We will work to improve enforcement of parking violations and collaborate with Tasmania Police to strengthen enforcement in areas outside of the CBD.



Box 2 Cars parked on footpaths
Photo: Institute for Sensible Transport

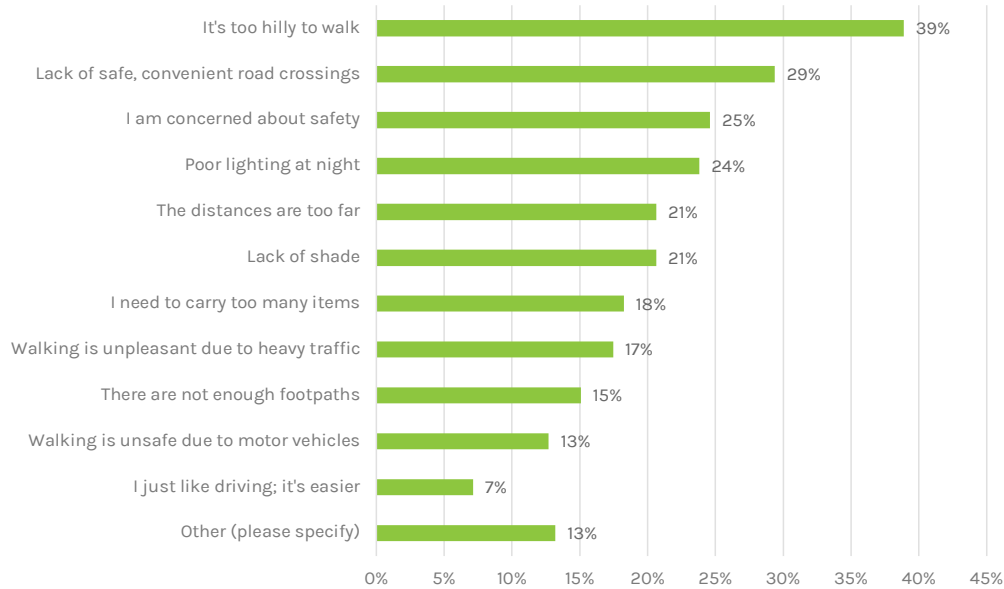


Figure 13 Barriers to walking
Source: Institute for Sensible Transport

5.5 Barriers to riding– what you told us

Barriers to riding in Burnie, as identified by the community, are shown in Figure 14. Three of the top four barriers relate to the lack of safe riding infrastructure and separation from other modes. Over half of respondents identified the lack of bike lanes and paths as a key issue (52%). Related to this, 41% said there was insufficient separation between bicycles and motor vehicles. Just over a quarter said there was a need for better separation between pedestrians and bicycles (27%). Burnie’s hills were also a major barrier, with half of respondents highlighting it as a challenge.

Three of the top four barriers relate to the lack of safe bicycle infrastructure.

Additional barriers include a lack of safe crossing points (20%), needing to carry too many items (15%), poor lighting (14%) and long distances (13%).

Collaborating with schools

You told us that for active travel to be successful in Burnie, we need to start with schools. This means making it safer for students to walk or ride to school by improving crossings and providing appropriate infrastructure.

We also recognise that infrastructure alone is not enough. Encouraging a culture of healthy, active lifestyles is equally important. This strategy includes behaviour change actions focused on collaborating with schools to ensuring they have appropriate end of trip facilities such as bicycle parking and secure storage areas.

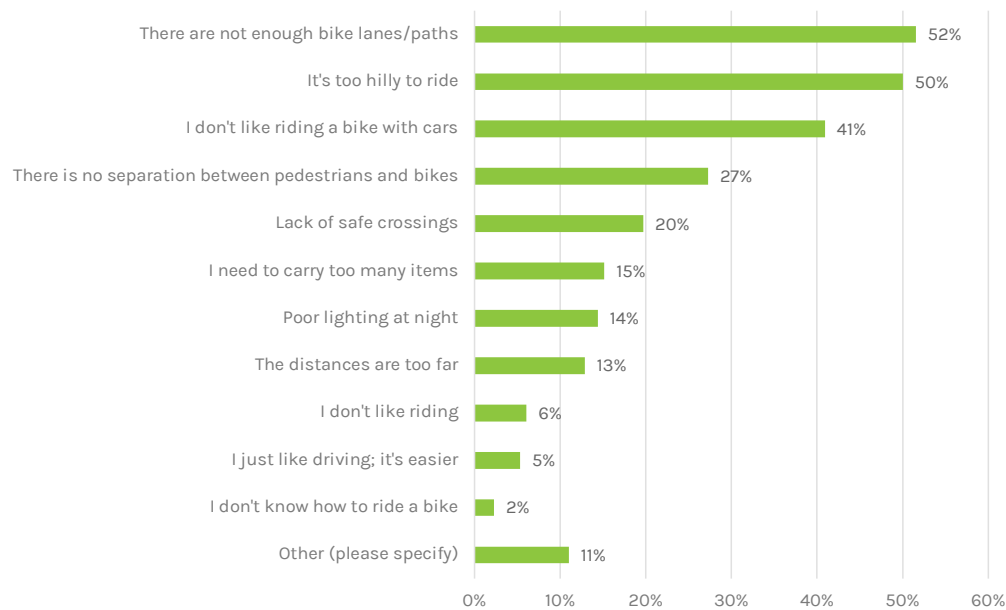


Figure 14 Barriers to riding
Source: Institute for Sensible Transport

6. Transport and demographics



Burnie is highly car dependent. By removing barriers to active travel, the ATS seeks to create an environment where active transport is an attractive choice for local trips.

A snapshot of the key transport and demographics in Burnie are captured in Figure 15 below.

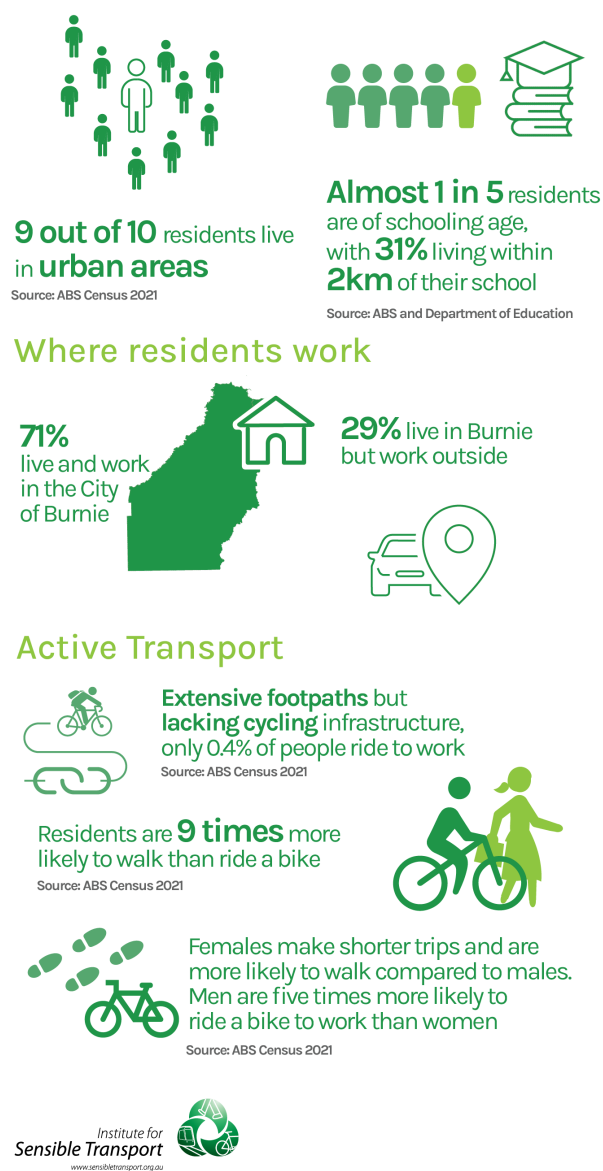


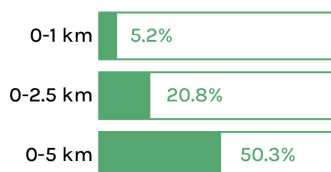
Figure 15 Transport and demographics in Burnie

Source: Institute for Sensible Transport

Travelling to work

92% of residents travel to work **by car**

Source: ABS Census 2021



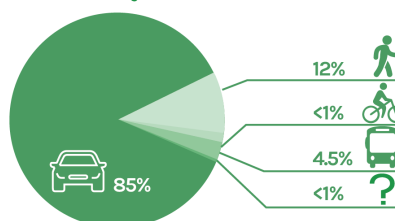
1 in 5 trips to work are less than **2.5km**

of trips to work, **50%** are **5km or less**

Source: ABS Census 2021

Travel in general

For trips of any purpose, **85%** are by car



*Based on comparable LGA from household travel survey

Road safety

50% of crashes involving active transport occurred in the CBD. **Mount Street** and the **Bass Highway** are crash hotspots

Source: Tasmanian Government



7. Active transport toolkit



This ATS provides a toolkit for planning and delivering walking, wheeling and riding infrastructure. The network design principles are based on the *Tasmanian Cycling Infrastructure Design Guide* and the *Austrroads Guide to Road Design*.

7.1 Cycling network planning

We will use the principles of the *Tasmania Cycling Design Guide* to inform our network planning. The following sections provide a summary of how we will use the guide to inform our decisions.

7.1.1 Cycling network principles

The *Tasmania Cycling Design Guide* identifies six principles to guide the development of AAA cycling networks (Figure 16). The considerations for designing a AAA network are outlined in Box 3 on the following page.

The network design principles were first identified by the *Dutch Design Manual for Bicycle Traffic*.⁴ They will be used to assist Council in integrating riding facilities in ways that balance a range of AAA considerations.



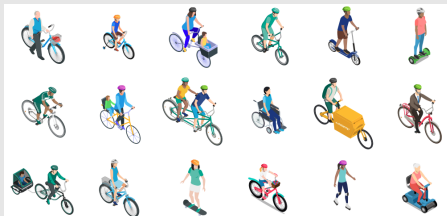
Figure 16 Cycling network design principles
Source: Tasmania Cycling Design Guide

⁴ Design Manual for Bicycle Traffic (CROW 2006)

Designing for all ages and abilities (AAA)

A well-designed riding network should cater to all users, ensuring that it is safe, and comfortable, for everyone. Designing for AAA means considering:

- **All people** – regardless of age, gender and background – including women, children, seniors, people living with disability, low-income households and people who are less confident riding.
- **All types of bicycles** – including standard and electric bicycles, kick scooters, electric scooters, cargo bikes, folding bicycles, tricycles, unicycles and specialist bicycles for people living with disabilities.
- **All types of trip purposes** – including commuting, school run, shopping and recreation.

**Box 3 Designing for all ages and abilities**

Source: Tasmania Cycling Design Guide

7.1.2 Treatment selection tool

Infrastructure must be designed to align with people's preferences, as well as real and perceived safety requirements. The type of facility provided will influence the range of people who will consider using it. As shown in Figure 17, three design options are identified as providing a comfortable facility for AAA. These include:

- Local street bikeways (calm streets, riding streets)
- Protected bicycle lanes
- Off-road paths.

Painted lanes and sharrows⁵ will generally provide a lower level of comfort and are unsuitable for a AAA facility. According to the State Government guidance, these treatments should only be considered where there is a clear justification for not providing AAA infrastructure.

Painted lanes and sharrows will generally not provide a AAA facility.

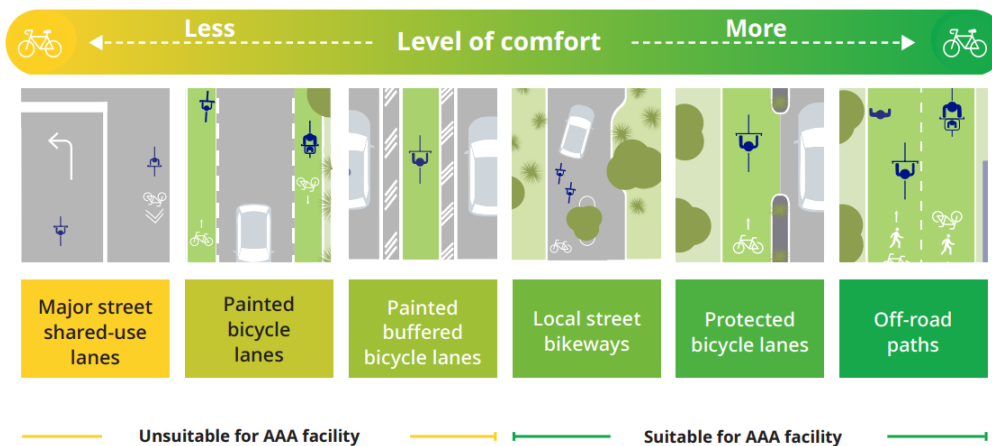


Figure 17 Cycling treatments and level of comfort

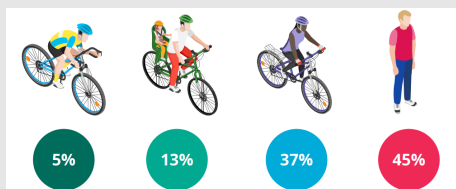
Source: Tasmania Cycling Infrastructure Design Guide

⁵ A painted surface treatment that signifies that it is safe for riders to use the shared traffic lane.

Designing for the *interested but concerned*

Most people are not confident riding in traffic but would consider riding if the environment felt safe and comfortable. Nearly 40% of Tasmanians are considered *interested but concerned*, meaning they are open to riding but only in places where they feel protected from traffic. This group represents the biggest opportunity to grow riding for everyday trips.

Unlike the small number of confident riders who are willing to share the road with vehicles, the *interested but concerned* need connected networks of separated infrastructure to consider riding. Designing for this group is essential to making riding inclusive and accessible for all ages and abilities.



Source: Tasmania Cycling Design Guide

Council will use the *treatment selection tool* to guide future decision-making regarding riding infrastructure in Burnie. The tool, provided in Figure 18, has been sourced from the *Tasmania Cycling Infrastructure Design Guide*. The key principles include:

- Riders should only share space with vehicles with low speeds (30km/h or lower) and low volumes (400 vehicles per hour or less).
- Riders should be physically separated from high speed (>30km/h) and high volume (>400 per hour) vehicles.
- Riders should be provided with off-road facilities when traffic speeds are 60km/h or greater.
- For off-road paths, people walking should be separated from fast riders with a separate

footpath or by providing a painted on-road bicycle lane for more confident, faster riders.

Additional factors to consider in selecting the most appropriate facility include:

- Presence of heavy vehicles, which increase the need for physical separation
- Kerbside activity – parking and loading, which increases the risk of ‘dooring’ collisions
- Interaction with public transport, including bus stops and other infrastructure
- Street context – considering the general location of the site and adjacent land uses.

Council will use the *Tasmanian treatment selection tool* to guide future decision-making regarding bike infrastructure in Burnie.

Street design guidelines

While this ATS focuses on key routes and crossings, Council also recognises the importance of making every street in Burnie safe and supportive of active transport and social connection. To achieve this, Council will investigate the development of Burnie-specific street design guidelines.

The guidelines would serve as a tool for planners, engineers, developers and the community when designing or upgrading streets. A similar approach has been taken by the City of Hobart, which recently developed the *Hobart Design Guidelines* to provide clear direction for buildings, streets and public spaces across the city.⁶

⁶ <https://yoursay.hobartcity.com.au/100360/widgets/463241/documents/308523>

Treatment selection tool

Design speed*	Two-way traffic volume** (busiest hour)	Painted buffered bicycle lane	Riding street	Calm street	Protected bicycle lane	Off-road paths
< 20 km/h	<400			AAA		
	≥ 400	AAA				
< 30 km/h	<400		AAA			
	≥ 400				AAA	
< 40 km/h	<400				AAA	
	≥ 400				AAA	
< 50 km/h	<400				AAA	
	≥ 400				AAA	
< 60 km/h	Any				AAA	
	Any				AAA	
≥ 60 km/h	Any				AAA	
	Any				AAA	

AAA	Likely the preferred AAA facility
	Provision should be suitable for most users
	Provision may not be suitable for all and may exclude some potential users
	Provision not recommended as it's unlikely to be suitable for a range of users
	Provision not considered suitable
	Provision may not be needed, consider local costs and benefits

Figure 4.2 Mid-block treatment selection matrix.
Adapted for Tasmania from the Ireland Cycle Design Manual (2023)

* motor vehicle design speed

**two-way volumes should be observed on the street where the bicycle facility is to be provided.

For traffic lane configurations of more or less than two lanes, assume a threshold of 200 cars per lane

Figure 18 Treatment selection tool

Source: Tasmania Cycling Infrastructure Design Guide

7.1.3 Cycling network route classifications

The *Tasmania Cycling Network Classifications* provides guidance on cycling routes based on the types of destinations they connect to, and the key reason for people to ride on that route. The future riding network for Burnie will comprise of three classifications, described in Table 1.

Each route type is assessed against two performance measures: *directness* and *capacity*. These are rated using a star system where:

- ★ indicates a basic level of performance
- ★★ indicates a moderate level
- ★★★ indicates a high-performing route suitable for greater demand or more direct travel.

This system helps guide infrastructure choices by aligning the form and function of each route with its expected use and performance needs.

The future riding network for Burnie will consist of three route classifications: primary, secondary, and neighbourhood routes.

Table 1 Cycling route classifications for Burnie

Route type	Function	Users	Typical Form*	Directness	Capacity
Primary Route	Connect activity centres. These trunk routes provide access to many destinations and are the backbone of the network for both current and future demand.	AAA	<ul style="list-style-type: none">• Off-road paths• Protected bicycle lanes	★★★	★★★
Secondary Route	Connect to primary routes and provide access to trip attractors such as schools, employment areas, shops, local services and facilities.	AAA	<ul style="list-style-type: none">• Protected bicycle lanes• Riding streets	★★☆	★★☆
Neighbourhood Route	Enable comfortable riding from homes and connect to at least one higher order route or a local destination.	AAA	<ul style="list-style-type: none">• Riding streets• Calm streets	★★☆	★★☆

Source: Tasmania Cycling Network Classifications

Note: *Typical form must be context responsive

7.2 Types of walking, wheeling and riding infrastructure

The different types of infrastructure we will use to support walking, wheeling and riding is shown in Figure 19.

The precise nature of the improvement will be determined following the outcome of planning and detailed design. Some of these upgrades may occur as part of our routine maintenance, but fully implemented routes may require external funding.

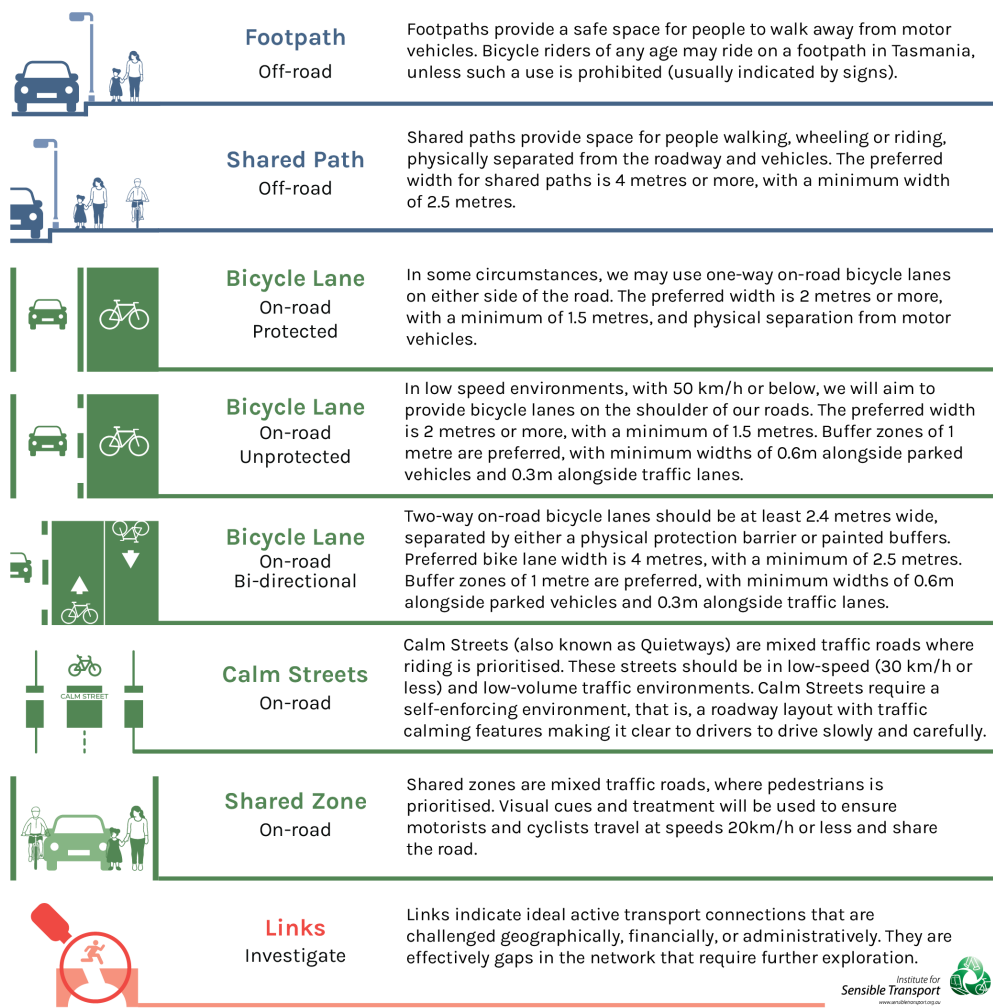


Figure 19 Types of walking and riding infrastructure
Source: Institute for Sensible Transport

7.2.1 Footpaths

Footpaths provide a safe space for people to walk. People with access to quality footpaths are twice as likely to walk compared to those without a connected network of footpaths.⁷

While many of our streets already have footpaths, there are some areas lacking connections. Where a need has been identified, we will install new footpaths, of at least 1.8 metres wide, unless physically not possible. In areas of high pedestrian activity, such as shopping centres and along main streets, we will provide wider footpaths so that groups of people can easily walk side-by-side, and support on-street dining and other uses. In new

developments, we will comply with DDA guidelines for pram ramps and driveways.

7.2.1.1 Pedestrian crossings

To build footpaths that provide safe and consistent walking experiences, we need appropriate crossing infrastructure at intersections in desirable locations. Suitable crossings will be applied according to need. The different crossings are illustrated in Figure 20.

More information on proposed footpaths and safer crossing points can be found in Section 8.2 of this ATS.

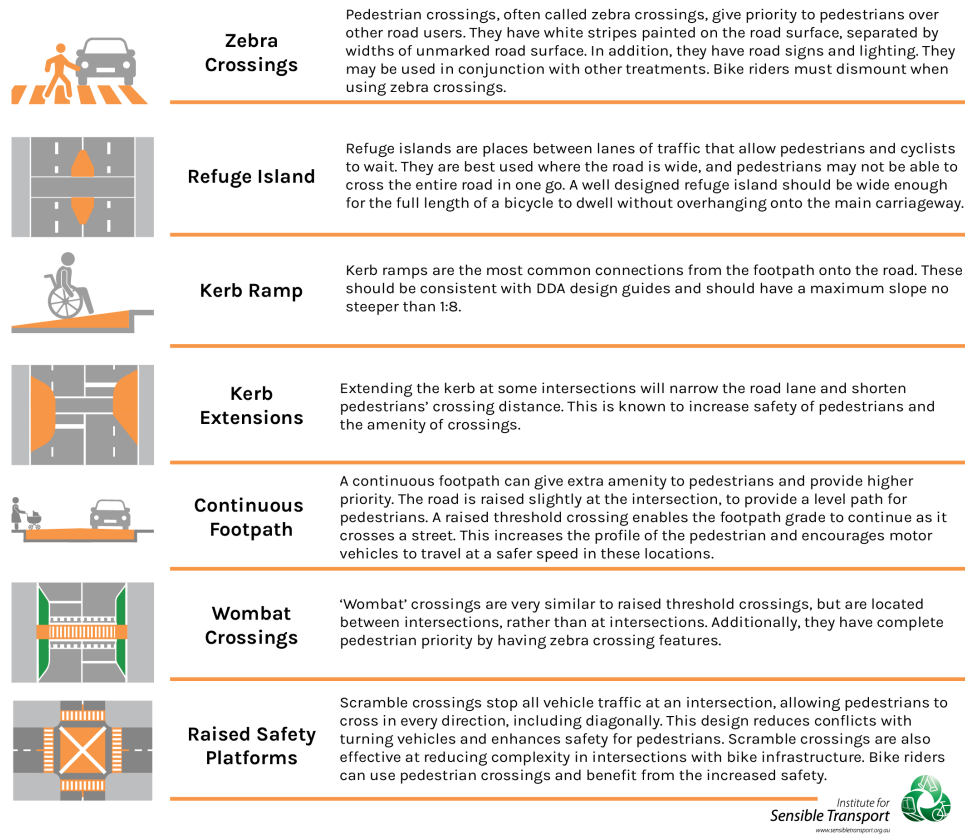


Figure 20 Crossing types

Source: Institute for Sensible Transport

⁷ <https://doi.org/10.1016/j.jypmed.2014.07.041>

7.2.2 Bike Parking

Having a safe place to store a bicycle when not in use is an essential element of active transport infrastructure. People require on-street parking for short term uses (e.g. visiting shops, community facilities) as well as off-street parking for longer stays (e.g. strata/multi-dwelling developments and workplaces).

Council will adopt a consistent and practical approach that exceeds the requirements set out in the *Tasmanian Planning Scheme* (TPS) provision *C2.0 Parking and Sustainable Transport Code*.⁸ The following sections outline the approach to both on-street and off-street bicycle parking. We will factor in the convenience of the user and the security of the bicycle in mind throughout all bicycle parking.

Council will require provision of bicycle parking above the *Tasmanian Planning Scheme* requirements.

7.2.2.1 On-street parking

Council will adopt a consistent approach to the provision of bike parking in public places. The bicycle parking hoop we will use in public places is shown in Figure 21. This design is consistent with Austroads guidelines and enables a wide selection of bicycle types to be locked, using the frame rather than the wheels. This helps to reduce theft, maximises user friendliness and a uniform approach increases cost effectiveness.

In order to achieve a consistent level of bike parking in areas likely to require it, Council will work towards providing:

- A minimum of two bike parking hoops every 100m of either side of the street in our existing high pedestrian activity areas (e.g. the CBD and smaller activity centres).

- A minimum of two bike parking hoops outside community facilities.
- A minimum of four bike parking hoops at tourist destinations.



Figure 21 On street bike hoop

Source: Institute for Sensible Transport

7.2.2.2 Off street parking

Ensuring people have a safe and convenient place to park a bike at a residential or work location is important if we are to achieve an increase in walking, wheeling and riding. The TPS currently sets no minimum bicycle parking requirements for a range of residential uses including:

- single dwellings
- multiple dwellings
- residential care facilities
- assisted housing
- visitor accommodation.

Other residential uses (in both the General Residential Zone and other zones) require 1 bicycle space per 5 bedrooms.

To address these gaps, Council will require the provision of off-street bicycle parking in new developments. Specifically, new developments will be required to provide:

- One bicycle parking hoop per dwelling in multi-unit developments, plus one visitor space per ten dwellings.⁹

⁸ <https://tpso.planning.tas.gov.au/tpso/external/planning-scheme-viewer/30/section/413?effectiveForDate=2024-10-07>

⁹ Visitors parking hoops would need to be in a publicly accessible area.

- One bicycle parking hoop per 150 m² of floor space and one visitor parking hoop per 400 m² for new commercial developments.
- One bicycle parking per two guest rooms in new visitor accommodation developments. (e.g. one hoop per two bedrooms).

7.3 Universal design

Creating an active transport network for all abilities ensures that everyone in the community can take part, no matter their level of ability or impairment.

Some of our established areas lack essential infrastructure which can limit transport choice.

We will undertake an audit of all footpaths, including ramps at crossing and intersections, to ensure that they are compliant to the *Disability Discrimination Act 1992* (DDA). We will develop a program to upgrade the non-compliant infrastructure, based on the audit.

Our designs will not exclude anyone based on their abilities or impairments. This is already part of many design guidelines, as shown in Figure 22, regarding footpath widths.

Everyone benefits from quality footpaths, with stepless kerbs to make roads easier to cross, and multiple forms of wayfinding making the network easier to navigate. Concepts of universal design are embedded in the walkability checklist (see Section 7.4).

Council will design its active transport network to ensure that regardless of ability or impairment, everyone has the opportunity to participate

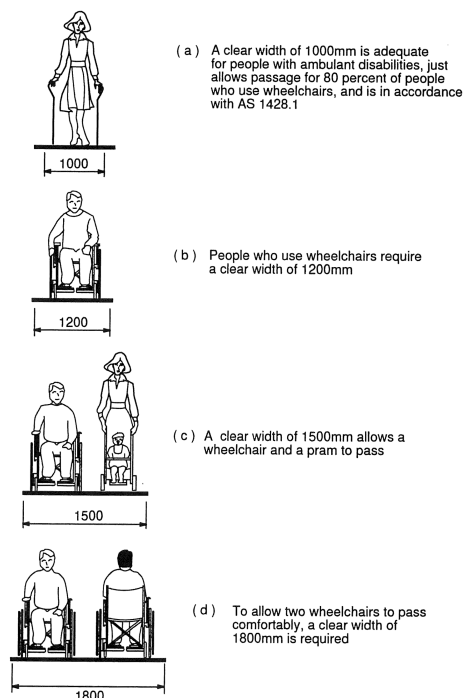


Figure 22 Pedestrian path width requirements for people with mobility impairment

Source: AS 1428.2:1992, quoted in Austroads Guide to Road Design Part 6A: Paths for Walking and Cycling

7.4 Walkability checklist

To help ensure Council consistently improves the walkability of Burnie, we will use a *walkability checklist*. Council will apply this checklist to ensure both new and existing developments hold the design characteristics that encourage walking, for both transport and recreation.

A walkability checklist will be used to ensure our design characteristics encourage walking.

The *walkability checklist* can be seen in Figure 23 below. More information on pedestrian design can be found in Section 7.2.1.1.

Walkability Checklist			Notes
	Does the street network provide high levels of permeability to reduce pedestrian travel distance?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Is there a footpath on one or both sides of the street?	One side <input type="checkbox"/> Both sides <input type="checkbox"/> None <input type="checkbox"/>	
	Is the footpath at least 1.8m wide in residential areas, and wider in shopping strips where higher pedestrian numbers is expected?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Is the footpath surface suitable for people using mobility aides (e.g. walking frames and scooters?)	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Are there kerb ramps at each crossing opportunity?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Are kerb ramps consistent with DDA requirements?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Are there tactile markings at intersections, in accordance with DDA requirements?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	In high activity areas, are there pedestrian priority crossings?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Are there crossing opportunities at mid-block locations within the core of town centres?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Is there a pedestrian refuge at the half way point of a crossing?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Is there appropriate separation between people walking from traffic?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Is there sufficient lighting?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Is there sufficient shade?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Is public seating appropriately located?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Are water fountains appropriately located?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	In new commercial areas, are there active street frontages facing the footpath?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Are there wayfinding options in high demand locations, with places of interest, distances and directions?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	
	Does the footpath connect to public transport stops?	Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	

Figure 23 Walkability checklist

Source: Institute for Sensible Transport

7.5 Integration with public transport

All infrastructure must be accessible for everyone regardless of ability or impairment. Providing supporting infrastructure to public transport such as shelters, seats and footpaths improves the integration and usability of the service.

Figure 24 provides a guide Council will follow when upgrading bus stops to ensure they are accessible to all members of the community. Council has been working in partnership with the Department of

State Growth as part of the *Bus stop upgrade program* to provide all-access, all-weather bus stops at priority locations.¹⁰ We received funding for a total of 62 bus stops upgrades across Burnie.

The community also identified a need for buses to be able to accommodate the carriage of full size bicycles. This helps to offer flexibility to the user when circumstances change (e.g. poor weather), as well as hills. To support this, Council will advocate to Metro Tasmania for the introduction of bike racks on buses. To ensure consistency to the user and operational simplicity, installing racks on all buses is preferred.



Figure 24 Public transport accessibility guide

Source: Institute for Sensible Transport

¹⁰ https://www.transport.tas.gov.au/public_transport/bus_stop_upgrade_program

8. Active transport network



This section provides a set of maps highlighting the proposed upgrades to the active transport network.

To create the proposed network plans, we applied the *Cycling Network Principles* from Section 7.1 to the Burnie area. Network planning was guided by spatial analysis, focusing on making riding an attractive option for 'Interested but Concerned' riders.

This ATS details the preferred infrastructure treatments for each route to create a network suitable for AAA. There needs to be flexibility in our planning to address the unique opportunities, challenges and constraints along each corridor. Each route will undergo detailed planning, design and consultation to ensure we use the appropriate treatment that responds to the context of each street/corridor.

In some instances, limited space will mean not all transport modes can be fully accommodated. Council will use the road hierarchies outlined in Section 4.1 to guide space allocation. Where traffic speeds and volumes are already low, shared road space may be suitable with minimal infrastructure. In other locations, a self-enforcing environment will be necessary. This means designing the street layout and introducing traffic-calming measures that clearly signal to drivers the need to travel slowly and carefully.

We will use the *Tasmanian Cycling Infrastructure Design Guide* to help design intersections, transitions and crossing treatments to improve safety for all road users. The intersection treatment will often be supported by other measures, such as bicycle boxes and advanced starts at traffic signals.

The Burnie CBD presents a unique opportunity to enhance transport choice and safety for the community. This is explored further in Box 4.

The Burnie CBD

Unlike many regional centres, the Burnie CBD does not experience high volumes of through traffic, due to the Bass Highway bypassing the city centre. This creates a unique opportunity to prioritise people over vehicles and shape a city centre that is safe, vibrant and accessible for all.

This ATS will support the implementation of the *Burnie City Centre Urban Plan* by ensuring infrastructure upgrades and public realm improvements create a safe environment for people walking, wheeling and riding.



Box 4 Burnie CBD

Photo: Institute for Sensible Transport

The Burnie CBD presents a unique opportunity to enhance transport choice and safety for the community.

8.1 Riding network

Figure 25 presents the proposed riding network for Burnie. Continuous lines represent existing infrastructure that may require some maintenance, and broken lines show proposed infrastructure.

Circles highlight intersections where safer crossings or transitions between different types of bicycle paths and lanes are needed. Several access points to trails and reserves that require formalised entry treatments are also identified.

The riding network is structured around a hierarchy of 13 routes in line with Tasmania Government guidance (explored in Section 7.1.3). The hierarchy includes:

- Primary routes
- Secondary routes
- Neighbourhood routes.

The network hierarchy is illustrated in Figure 26. A detailed focus on the CBD riding network is provided in Section 8.3.1.



Figure 25 Proposed riding network

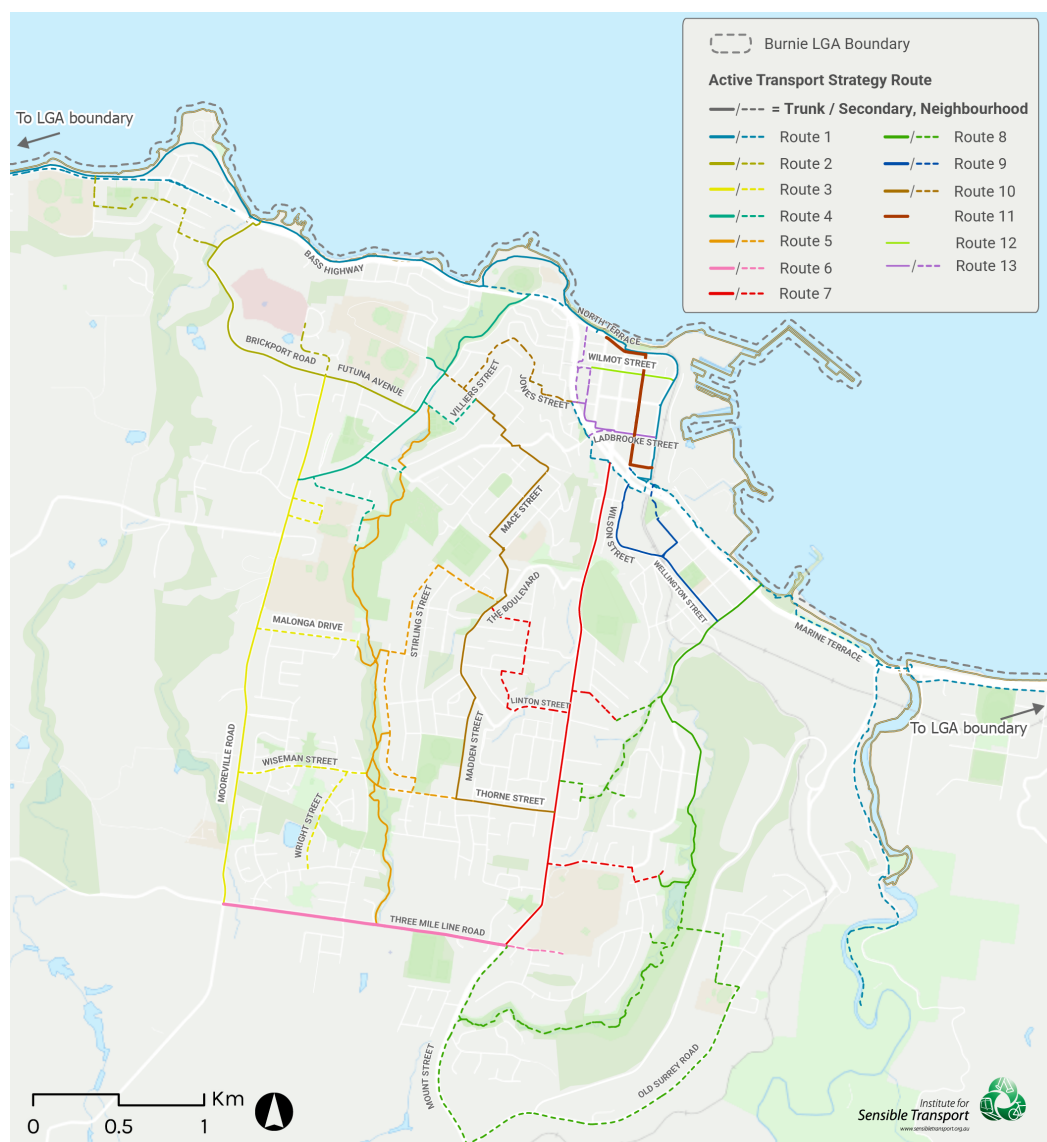


Figure 26 Riding network hierarchy

8.2 Burnie walking network

Figure 27 shows the proposed walking network for Burnie. Several streets with no footpaths have been identified for upgrades as well as the shared infrastructure from the riding network. Circles represent intersections that have been identified for safer crossing conditions. This includes locations for potential school crossing upgrades, new mid-block opportunities, as well as locations for scramble crossings in the CBD. The types of crossings we will use is explored in Figure 20. A detailed focus on the CBD walking network is provided in Section 8.3.2.

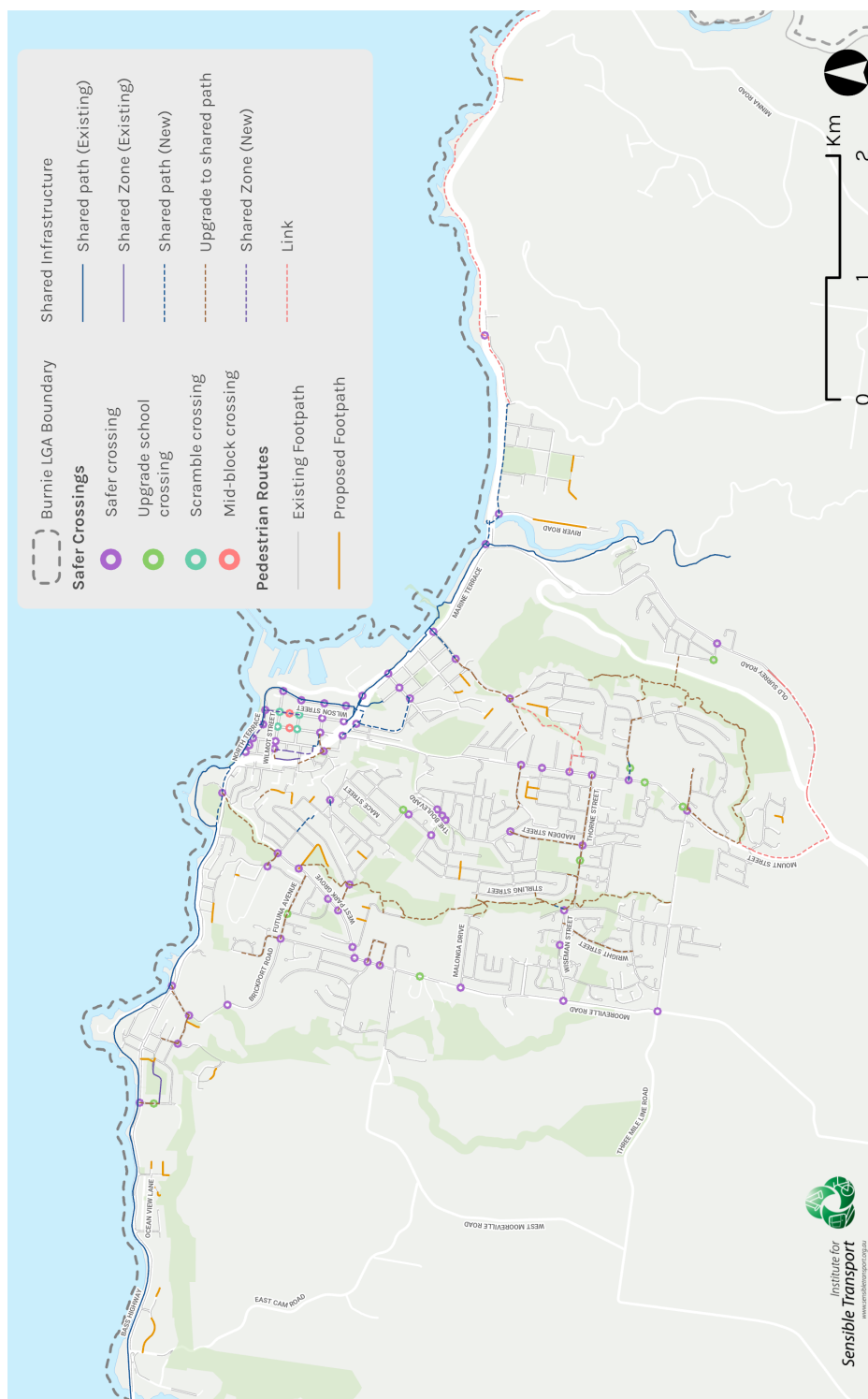


Figure 27 Proposed walking network

8.3 Burnie CBD

The Burnie CBD will undergo significant change over the next 15 years. The *City Centre Urban Plan* sets the foundation for creating a more vibrant, liveable city centre. To enable these outcomes, this ATS supports investigation into how street space and vehicle movement through the CBD can be managed to support improved walking, wheeling, and riding conditions.

A *circulation plan* is a planning tool used to guide how vehicles move through an area. It can help to balance the needs of people in motor vehicles while providing opportunities for people to safely travel by other modes. A *circulation plan* can also enable streets to be reconfigured in ways that support a more people-focused public realm.

Council will investigate opportunities to implement a *circulation plan* for the CBD aligned with the *City Centre Urban Plan*. For Burnie, this would not restrict vehicle access; rather, it would manage how vehicles move through the CBD.

Introducing a *circulation plan* early in the implementation process is important, as it will enable more space to be allocated to walking, wheeling, and riding, helping to achieve the vibrant, people-focused outcomes envisioned for the CBD.

Introducing a *circulation plan* will enable more space to be allocated to active transport, helping to achieve the vibrant, people-focused outcomes envisioned for the CBD.

8.3.1 CBD Riding network

Figure 28 shows the proposed riding network for the Burnie CBD. The network focuses on providing dedicated north-south and east-west corridors, supported by the Coastal Pathway along the northern and eastern periphery. Several crossing points and transitions have been identified that will require improved safety outcomes for people riding. The specific treatments will be determined during future planning and design.

While some streets in the CBD may not accommodate dedicated riding infrastructure, the priority is to ensure a safe and comfortable environment where people can confidently share space with vehicles. This approach should apply across the CBD.

We have provided two conceptual designs in Appendix A.2. to illustrate how the infrastructure outlined in Figure 28 could function in the CBD. A range of options and street configurations will be considered to ensure a safe and comfortable environment for people walking, wheeling and riding. Further detailed planning and community engagement will be undertaken alongside the implementation of the *City Centre Urban Plan*.

As part of the proposed network, Council will advocate to the Department of State Growth for scramble crossings at high pedestrian activity intersections. These are described in more detail in Box 5.

Scramble Crossings

A scramble crossing provides an exclusive pedestrian signal phase at an intersection. This allows people walking, wheeling or riding to cross in any direction, including diagonally, while all motor vehicle traffic is stopped. Trials have been conducted in the City of Hobart at several intersections.

This type of crossing is suitable where all corners of an intersection have high pedestrian volumes. Scramble crossings can also accommodate bike riders, providing a safe place to cross and transition between different types of infrastructure.



Box 5 Scramble crossings

Photo source: City of Hobart



Figure 28 Riding network, Burnie CBD

8.3.2 CBD Walking network

Figure 29 shows the proposed walking network for the Burnie CBD. The CBD is already well serviced, with most streets featuring wide paths on both sides of the street. Walking improvements will focus on the introduction of low-speed shared zones, shared paths and safer crossing opportunities.

Council will work to improve active transport priority at the crossings identified in Figure 29 by implementing safer mid-block opportunities, advocating for longer pedestrian crossing times and exploring opportunities for scramble crossings at key intersections. Improving pedestrian safety and access across the Bass Highway will be another focus of the ATS.



Figure 29 Walking network, Burnie CBD

9. Policy and behaviour change



This section provides a set of policy and behaviour change actions that complement the proposed infrastructure in this ATS. The non-infrastructure measures are essential to creating an environment where walking, wheeling, and riding are not only safe, but attractive and convenient choices.

Creating a successful ATS requires more than just infrastructure actions. It also depends on the policies and programs that encourage walking, wheeling and riding. Table 2 outlines a range of policy and behaviour change actions that will support the delivery of the ATS. These actions aim to build a culture that encourages walking, wheeling, and riding through improved governance, alignment with state policy, and initiatives that encourage and enable sustainable transport choices.

Table 2 Policy and behaviour change actions

Action	Name	Description
A1	Aligning our targets to Tasmanian Government policy	<div>Use Tasmanian Department of State Growth policies and strategies to help create an environment in which walking, wheeling and riding cycling is safer and more convenient. Relevant documents include:</div> <ul style="list-style-type: none">• Tasmanian Walk, Wheel, Ride Strategy 2024• Tasmania Cycling Infrastructure Design Guide 2024• Tasmanian Cycling Network Classification 2024• Towards Zero - Tasmanian Road Safety Strategy 2017-2026• Principal Urban Cycling Network Burnie & Devonport 2012• Cradle Coast Integrated Transport Strategy• Development Design Specification-Geometric Road Design.
A2.1	Integrate walking, wheeling and riding into Council policies and strategies	<div>Burnie City Council has a number of existing policies that contain explicit or implicit support for growing walking, wheeling and riding participation. A selection of relevant policies are identified below:</div> <ul style="list-style-type: none">• Making a Better Burnie 2044• Council Plan 2024-2027• City Centre Urban Plan 2025• Burnie Road Network Strategy.
A2.2		<div>Investigate developing street design standards to ensure low traffic streets, both existing and in new developments, are designed to support walking, wheeling and riding.</div>

Action Name	Description
A3.1 Providing better bicycle parking opportunities	<p>Increase the availability of on-street bicycle parking through Burnie. This includes installing bike parking at:</p> <ul style="list-style-type: none"> • A minimum of two bike parking hoops every 100m of either side of the street in existing high pedestrian activity areas (e.g. activity centres, medical facilities, shopping strips). • A minimum of two bike parking hoops outside community facilities. These will be located with the convenience of the user and the security of the bicycle in mind. • A minimum of four bike parking hoops at tourism destinations with the convenience of the user and the security of the bicycle in mind. • Deliver secure bicycle parking areas and end of trip amenities as proposed in the <i>City Centre Urban Plan</i>.
A3.2	<p>Increase the availability of off-street bicycle parking in new developments. This includes:</p> <ul style="list-style-type: none"> • Requiring one bicycle parking hoop per dwelling in multi-unit residential developments, plus one visitor space per ten dwellings. • Requiring one bicycle parking hoop per 150 m² of floor space and one visitor parking hoop per 400 m² in new commercial developments. • Requiring one bicycle parking space per two guest rooms in new visitor accommodation developments.
A3.3	Develop a secure bike parking program in the CBD that provides weather-protected and monitored facilities.
A4.1 Adopt safer speed limits	Apply safer speed limits on the proposed active transport network broadly in line with Tasmanian Government guidance (e.g. 30km/h on calm streets and 40km/h on streets with painted bike lanes). This includes street design changes consistent with safer speed limits (i.e. speed humps, modal filters, speed tables and other traffic calming measures).
A4.2	Apply safer speed limits around activity centres and shopping strips. This includes street design changes consistent with safer speed limits (i.e. speed humps, modal filters, speed tables and other traffic calming measures).
A4.3	Initiate discussions with the Tasmanian Government about introducing safer speed limits on State roads where they pass through residential or commercial areas in Burnie (e.g. Mount Street, Bass Highway).
A5 Support transport choice	Apply the Road User Hierarchy (see Figure 11 and Figure 12) in road space allocation and transport investment decisions.

Action Name	Description
A6	<p>Building active travel into road renewal and our streetscapes</p> <p>Ensure that any changes to the road network results in improved walking, wheeling and riding outcomes. This means that whenever:</p> <ul style="list-style-type: none">• A road is resurfaced, bicycle infrastructure is included as part of new lane marking, unless impractical to do so from a safety or road width perspective.• A road treatment project is planned, walking, wheeling and riding infrastructure is integrated unless impractical to do so.
A7.1	<p>Maintaining our active travel network</p> <p>Whenever road works occur that involve the removal and replacement of road surface on a designated active transport route, any bicycle line marking or symbols that are disturbed must be replaced when the works are completed.</p>
A7.2	<p>Include active transport routes and paths in Council's operational maintenance schedule to ensure networks are regularly cleared of debris and kept in good condition.</p>
A8.1	<p>Adopt universal design</p> <p>Ensure that the active transport network provides equal access by:</p> <ul style="list-style-type: none">• Undertaking an audit of footpaths including ramps at crossing and intersections, to ensure that they are compliant to the Disability Discrimination Act (DDA).• Develop a program to upgrade non-compliant infrastructure based on the audit.• Ensure that all new and/or renewed footpaths adhere to DDA design guidelines.• Respond to reports from the community of non-compliant or missing kerb ramps.
A8.2	<p>Ensure that all drain grates in Burnie are safe for people riding and wheeling. This includes:</p> <ul style="list-style-type: none">• Undertaking an audit of existing drain grates to identify where unsafe drain grates exist.• Upgrading non-compliant drain grates based on the audit.• Ensure that all new drain grates are designed to minimise risk to all road users, especially those modes with small and/or narrow tyres (e.g. bicycles).
A8.3	<p>Develop a program to identify and prioritise upgrades to bus stops that are not DDA compliant or lack supporting infrastructure. Figure 24 provides a checklist for public transport accessibility.</p>
A8.4	<p>Create a more pedestrian friendly environment by ensuring that street furniture and services are placed outside of the <i>clear path</i> and follow accessibility standards to not obstruct pedestrian movement.¹¹</p>

¹¹ The pedestrian clear path is the primary pathway that is free of doors, street furniture and services to ensure pedestrians have a safe and adequate place to walk.

Action	Name	Description
A9	Support walkability in all development	Adopt and apply a walkability checklist to ensure both new and existing developments hold the design characteristics that encourage high levels of walking, for both transport and recreation. This will help to ensure walking is safe, usable and attractive.
A10	Encourage good etiquette among users of our network	Encourage appropriate use on our shared path network. This includes: <ul style="list-style-type: none"> • Ensuring paths are a suitable width, based on the number of expected users. • Providing decals (pavement signage) to encourage people on bicycles to travel at a safe speed and use their bell when approaching pedestrians. • Remind people they must stay to the left and be aware of the needs of other users. • Encourage fast moving people on bicycles to avoid busy parts of the shared path network by providing suitable alternatives. • Use rumble strips and pavement murals to encourage everyone to slow down at key points in the network such as junctions and popular locations.
A11.1	Plan for micromobility	Consult with the community for a new shared e-micromobility service, and call for Expressions of Interest if there is strong community support.
A11.2		Create an ' <i>e-bike friendly businesses</i> ' program, in which businesses can elect to support people charging up their e-bike batteries. Cafes and libraries are examples of suitable locations.
A11.3		Investigate developing a hire and purchase program with local bicycle stores to support residents to trial and purchase e-bike's or micromobility devices.
A12.1	Support riding to and from school	Establish a program to enhance school crossings. Council will conduct an analysis of all primary and secondary schools in Burnie and develop a program to enhance the walking, wheeling and riding infrastructure within the school catchment. This will include upgrades to: <ul style="list-style-type: none"> • Footpaths • Crossings • Shared paths and bike lanes.
A12.2		Develop a travel to school survey for parents and caregivers to understand current and preferred school travel modes.
A13.1	Become a community leader in active travel	Increasing the number of Council staff that regularly walk, wheel and ride. This will help BCC become a leader in active transport participation and thereby be better able to create a user orientated network for the wider community. Council will:

Action Name	Description
	<ul style="list-style-type: none">• Develop maps and make them available to staff, highlighting convenient routes between regular destinations for staff.• Run an annual workshop with staff who ride a bike to gather feedback on issues associated with riding for business journeys and plan to address these barriers. Many of the issues identified are likely to be important for the wider community.• Provide a range of bicycle types for staff to use for a variety of purposes (including cargo bikes and e-assist bicycles, where necessary). These will be provided at workplaces as an alternative to the motor vehicle fleet for short trips.¹²• Run bicycle skills and maintenance training classes for staff.• Integrate the promotion of new facilities with awareness raising of sustainable mobility options.• Develop a promotional pack to be included in the induction program for new staff, highlighting the benefits of walking, wheeling and riding, and how new staff can take advantage of the services offered by Council to encourage staff to use active modes.• Regularly promote walking, wheeling and riding facilities and activities through existing internal communications.
A13.2	Participate and promote events that encourage walking, wheeling and riding (e.g. Ride to Work, Ride/Walk to School Day and Tasmanian Bike Week).
A13.3	Support and encourage workplace travel behaviour initiatives that encourage walking, wheeling and riding.
A14.1	Improve active transport priority <ul style="list-style-type: none">• Initiate discussions with the Tasmanian Government to improve the priority for active modes at signalised intersections by:<ul style="list-style-type: none">• Applying automatic 'green man' signals during the appropriate light cycle, without requiring pedestrians to activate the crossing.• Extending 'green man' signal times at crossings with high pedestrian activity to allow safe and comfortable crossing for all users.• Investigate opportunities for scramble crossings in the Burnie CBD.
A14.2	Apply default priority to people walking, wheeling and riding at minor road crossings on key active transport routes.
A14.3	Investigate opportunities to implement a <i>circulation plan</i> for the Burnie CBD aligned with the <i>City Centre Urban Plan</i> .
A15.1	Monitoring and evaluation <ul style="list-style-type: none">• Review this ATS every five years to monitor implementation progress and emerging trends.
A15.2	Advocate for Burnie to be included in future Tasmanian State Government Household Travel Surveys.

¹² The development of e-bike fleet would only occur once there is a demonstrated need and desire based on staff feedback.

Action	Name	Description
A16.1	Remove barriers to active travel	Remove barriers to active transport identified in the <i>City Centre Urban Plan</i> including: <ul style="list-style-type: none">• Removing barrier fencing to the foreshore to provide easier access opportunities between the CBD and beach.• Covering decommissioned railway tracks to improve access to the Coastal Pathway.
A16.2		Formalise access to the trail and shared path network where informal cut-throughs and desire lines exist. This includes providing appropriate paths depending on context (footpaths or shared paths) and installing modal filters ¹³ that maintain access for bikes, prams and mobility devices.
A16.3		Work with Tasmania Police to improve enforcement of parking violations that obstruct footpaths and compromise access and safety for people walking, wheeling and riding.
A16.4		Advocate to Metro Tasmania for bike racks on buses to support better integration between public and active transport.
A16.5		Investigate and promote crowdsourced safety reporting tools (e.g. BikeSpot) to identify unsafe locations on the active transport network.
A16.6		Investigate opportunities to provide resting areas along key active transport routes, particularly in areas with steep topography, including seats and shaded areas to support people of all ages and abilities.
A17	Wayfinding	Develop a <i>Wayfinding Strategy</i> for Burnie.
A18	External funding	Maximise opportunities for gaining external funding for the projects included within this ATS. In anticipation of future funding opportunities, Council will prepare the detailed designs, costings and benefits for key infrastructure projects in this Strategy.

¹³ A modal filter, sometimes called a point closure, is a road design that restricts the passage of certain types of vehicles to provide filtered permeability.

10. Prioritisation and implementation of infrastructure actions



The infrastructure actions have been prioritised to ensure we implement the projects that offer the most value to the community first. This section explains the variables included in the process, as well as the results of the prioritisation.

10.1 Prioritisation of infrastructure actions

This ATS applies two prioritisation frameworks to identify which connections should be delivered first based on *impact* and *feasibility*. The prioritisation frameworks help guide the staging of actions (Section 10.2) by assessing both the need

for a connection (*desirability*) and the ease with which it can be delivered (*deliverability*).

Each of the prioritisation frameworks includes a tailored set of variables relevant to riding or walking, as shown in Figure 30 and Figure 31 respectively. Variables were assessed using a five-point scale (1 = lowest priority, 5 = highest priority).

Scores were grouped into two categories, *desirability* and *deliverability*. A composite prioritisation score was then calculated using the squared mean of the *desirability* and *deliverability* scores.

Table 3 provides a summary of the types of walking, wheeling and riding infrastructure included in this ATS, as well as their length and high level estimated cost.

Table 3 Summary of walking, wheeling and riding infrastructure actions, by typology and cost

Typology	Current (m)	Upgraded (m)	Proposed (m)	Cost
Footpaths	202,385	0	4,070	\$1,442,000
Shared Paths	13,060	16,800	3,740	\$3,362,000
Bicycle Lanes	3,570		14,160	\$3,624,000
Calm Streets	0	0	9,330	\$1,399,000
Shared Zones	580	0	910	\$1,682,000
Links	0	0	8,230	N/A

Note: Costing estimates are based on linear distance of the infrastructure being installed, and do not account for additional engineering works which may be required, including rebuilding of kerb and channel, bridge works, etc. These numbers should be read as a broad guide, but final costing must be undertaken during the planning process.

Desirability - Riding**Potential to grow riding participation**

The bike and micromobility use propensity index is used alongside population data to identify routes that run from, to, or through areas of high latent demand for riding, and prioritise these routes higher.

**Ability to address safety issues**

Areas that have had fatalities, serious injuries, or high numbers of crashes over the past five years, roads with high traffic volumes, or locations identified as dangerous through community consultation are prioritised.

**Opportunity to enhance tourism**

Routes that connect to key tourist features, such as the foreshore, trails, and significant landmarks, are given higher priority.

**Ability to support local shops and services**

Routes that provide access to activity centres, healthcare facilities, schools, and essential services are prioritised more highly.

**Contribution to a continuous network**

Parts of the network which have the most influence in forming key connections and extending the network are identified and prioritised.

Deliverability - Riding**Ease of implementation**

The ease of implementing infrastructure is assessed by looking at what is proposed and the street's configuration. Street features such as bus stops, high demand car parking, bridges, and traffic signals make implementation more difficult.

**Cost**

Routes which have lower costs to be completed are prioritised above routes which have higher costs.

Figure 30 Variables in riding prioritisation framework

Desirability - Walking**Potential to grow walking participation**

Population data is used to identify footpaths that run from, to, or through areas of high population and prioritise these footpaths higher.

**Ability to address safety issues**

Areas with conditions that are known to create an unsafe environment for pedestrians including traffic volumes and speed limits are prioritised higher.

**Ability to support local education and open space**

Footpaths that connect to schools and open space are given higher priority.

**Ability to support local shops and services**

Footpaths that provide access to activity centres, healthcare facilities and community services are prioritised more highly.

**Contribution to a continuous network**

Parts of the footpath network which have the most influence in forming key connections and extending the network are identified and prioritised.

Deliverability - Walking**Street context**

The local street context is considered, including whether a path already exists or if the road is a quiet, low-traffic street where sharing space is feasible (e.g., cul-de-sacs).

**Cost**

Footpaths which are lower cost to be completed are prioritised above those which have higher cost.

Figure 31 Variables in walking prioritisation framework

10.2 Staging of actions

Using the prioritisation score, each action was allocated to one of four delivery stages. Each stage is approximately equal in terms of estimated construction costs, a significant portion of the infrastructure will require funding from external sources. This is discussed further in Section 11 of this ATS. A summary of each stage is shown in Table 4.

Table 4 Summary of delivery stages

Stage	Timeframe	Riding infrastructure		Walking infrastructure	
		Length of infrastructure delivered (m)	Costing estimate	Length of infrastructure delivered (m)	Costing estimate
1	Next 5 years	18,940	\$3,421,000	1,340	\$480,000
2	5 to 10 years	15,500	\$2,769,000	1,080	\$384,000
3	10 to 15 years	10,790	\$2,608,000	1,650	\$578,000
Opportunistic	N/A	8,230	N/A	N/A	N/A

Note: Costing estimates are based on linear distance of the infrastructure being installed, and do not account for additional engineering works which may be required, including rebuilding of kerb and channel, bridge works, etc. These numbers should be read as a broad guide, but final costing must be undertaken during the planning process.

10.3 Infrastructure actions

10.3.1 Riding actions

Table 5 outlines the riding actions, including the route number that each segment corresponds to, the street name, proposed treatment, prioritisation score, implementation stage, length and a high-level cost estimate. Where a street includes multiple infrastructure types (e.g., a bi-directional bike lane in one section and a shared path in another), separate actions are provided for each typology.

As the riding network is still in its infancy and existing infrastructure is limited, some segments have been manually adjusted (either brought forward or delayed), based on their strategic importance within the broader network. This ensures early works contribute meaningfully to a connected network rather than delivering isolated segments with limited standalone value. Some streets may have planned renewals, which will enable infrastructure projects to be brought forward.

A map of the network staging and detailed maps of the 13 riding routes, including the action numbers and proposed treatments are provided in Appendix A.3

Table 5 Riding actions

Action No.	Route No.	Name	Prioritisation score	Stage	Length (m)	Costing estimate
C1	Route 5a	Payne Street shared path	3.81	1	90	\$9,000
C2	Route 10	Tattersall Street calm street	3.52	1	120	\$18,000
C3	Route 5a	Hawkins Street calm street	3.39	1	90	\$14,000
C4	Route 4b	Grandview Avenue calm street	3.34	1	40	\$4,000
C5	Route 7a	Kingsley Avenue calm street	3.31	1	180	\$28,000
C6	Route 13a	Wilmot Street shared path	3.27	1	80	\$8,000

Action No.	Route No.	Name	Prioritisation score	Stage	Length (m)	Costing estimate
C7	Route 13c	View Road overpass shared path	3.26	1	160	\$16,000
C8	Route 4	Burnie Park shared path	3.26	1	720	\$72,000
C9	Route 7b	Cabot Street shared path	3.24	1	50	\$5,000
C10	Route 10a	Villiers Street calm street	3.23	1	250	\$38,000
C11	Route 7b	Grenville Street calm street	3.21	1	120	\$18,000
C12	Route 7b	Linton Street shared path	3.20	1	330	\$33,000
C13	Route 1f	Coronation Park shared path	3.18	1	290	\$29,000
C14	Route 5a	Ogden Street calm street	3.16	1	860	\$129,000
C15	Route 4b	View Road shared path	3.16	1	280	\$44,000
C16	Route 5a	Stirling Street calm street	3.15	1	570	\$86,000
C17	Route 5	Shorewell Creek north shared path	3.13	1	3,490	\$383,000
C18	Route 10	Bird Street calm street	3.12	1	40	\$7,000
C19	Route 11	Spring Street bike lanes	3.12	1	120	\$30,000
C20	Route 10	Joyce Street calm street	3.10	1	250	\$38,000
C21	Route 4c	Richardson Crescent calm street	3.10	1	200	\$30,000
C22	Route 7b	Conrad Street calm street	3.09	1	220	\$34,000
C23	Route 7a	Blackwood Parade shared path	3.07	1	90	\$9,000
C24	Route 4c	Eastwood Drive calm street	3.06	1	220	\$33,000
C25	Route 7c	Federal Street calm street	3.05	1	330	\$49,000
C26	Route 10	Mace Street calm street	3.04	1	530	\$79,000
C27	Route 1f	Fleming Court calm street	3.02	1	180	\$26,000
C28	Route 13	Ladbrooke Street shared path	3.00	1	160	\$73,000
C29	Route 10	Tracey Street calm street	2.98	1	240	\$35,000
C30	Route 10	John Street calm street	2.96	1	90	\$14,000
C31	Route 7b	Verelle Street calm street	2.96	1	260	\$38,000
C32	Route 10a	Amy Street calm street	2.95	1	150	\$23,000
C33	Route 10a	Jones Street calm street	2.92	1	310	\$47,000
C34	Route 5a	Terrylands Street bike lanes	2.91	1	410	\$186,000
C35	Route 10	Elizabeth Street calm street	2.90	1	100	\$15,000
C36	Route 7b	Mark Street calm street	2.89	1	310	\$46,000
C37	Route 8	Brooklyn Road shared path	2.87	1	920	\$92,000
C38	Route 7a	Roslyn Avenue shared path	2.87	1	500	\$105,000
C39	Route 8	Romaine Creek north shared path	2.83	1	740	\$74,000
C40	Route 11	North Terrace shared zone	2.82	1	260	\$117,000

Action No.	Route No.	Name	Prioritisation score	Stage	Length (m)	Costing estimate
C41	Route 13a	Little Alexander Street shared zone	2.80	1	370	\$165,000
C42	Route 10	Grandview Avenue shared path	2.78	1	110	\$48,000
C43	Route 11	Wilson Street shared zone	2.75	1	280	\$126,000
C44	Route 10	Morse Street calm street	2.75	1	220	\$33,000
C45	Route 10	The Boulevard bike lanes	2.74	1	210	\$53,000
C46	Route 10a	Seaview Avenue shared path	2.73	1	110	\$11,000
C47	Route 3c	Wisemen Street bike lanes	2.72	1	780	\$194,000
C48	Route 10	Elizabeth Street shared path	2.71	1	100	\$46,000
C49	Route 10a	Amelia Street calm street	2.68	1	110	\$16,000
C50	Route 1f	Railway underpass shared path	2.68	1	390	\$174,000
C51	Route 8	Swanston Street calm street	2.64	1	590	\$88,000
C52	Route 10a	Seaview Avenue calm street	2.63	1	260	\$39,000
C53	Route 13	Ladbrooke Street bike lanes	2.59	1	240	\$61,000
C54	Route 11	Wilson Street bike lanes	2.57	1	370	\$92,000
C55	Route 8	Reeves Street shared path	2.50	1	450	\$141,000
C56	Route 4d	Oldaker Street shared path	3.42	2	160	\$16,000
C57	Route 3a	Van Diemens Crescent shared path	3.34	2	470	\$47,000
C58	Route 5	Shorewell Creek south shared path	3.32	2	860	\$86,000
C59	Route 13b	King Street shared zone	3.29	2	260	\$9,000
C60	Route 10	Madden Street shared path	3.23	2	610	\$61,000
C61	Route 10	Thorne Street shared path	3.12	2	1,020	\$102,000
C62	Route 3	Brickport Road south bike lanes	3.08	2	630	\$157,000
C63	Route 2b	Uplands Place shared path	3.04	2	440	\$44,000
C64	Route 3b	Blake Boulevard calm street	2.94	2	30	\$5,000
C65	Route 4a	Curraghmore Avenue calm street	2.93	2	690	\$104,000
C66	Route 13b	King Street quiet street	2.78	2	200	\$30,000
C67	Route 3b	Malonga Drive calm street	2.77	2	600	\$90,000
C68	Route 10	Belton Street bike lanes	2.73	2	510	\$129,000
C69	Route 8b	Romaine Creek south shared path	2.71	2	2,270	\$227,000
C70	Route 9	Strahan Street calm street	2.71	2	300	\$45,000
C71	Route 4	West Park Grove bike lanes	2.70	2	1,170	\$281,000
C72	Route 9	Wilson Street shared path	2.65	2	670	\$301,000
C73	Route 8e	Mount Street shared path	2.63	2	570	\$57,000
C74	Route 12	Wilmot Street bike lanes	2.49	2	480	\$120,000

Action No.	Route No.	Name	Prioritisation score	Stage	Length (m)	Costing estimate
C75	Route 9	Wellington Street bike lanes	2.45	2	490	\$123,000
C76	Route 8c	Old Surrey Road bike lanes	2.42	2	480	\$119,000
C77	Route 1a	West Park Oval shared path	2.42	2	510	\$228,000
C78	Route 2	Brickport Road bike lanes	2.32	2	1,200	\$301,000
C79	Route 2	Futuna Avenue shared path	2.25	2	570	\$57,000
C80	Route 2	Brickport Road shared path	2.21	2	310	\$30,000
C81	Route 3d	Wright Street shared path	3.33	3	630	\$63,000
C82	Route 6a	Atkins Drive shared path	3.14	3	330	\$33,000
C83	Route 2a	Poke Street calm street	3.07	3	130	\$20,000
C84	Route 2a	Fidler Street shared path	3.05	3	170	\$17,000
C85	Route 8c	Romaine Creek to O'Grady Street shared path	3.04	3	270	\$27,000
C86	Route 2a	Durham Road shared path	2.96	3	140	\$14,000
C87	Route 2a	Les Clark Drive calm street	2.89	3	30	\$5,000
C88	Route 2a	Saundridge Road calm street	2.86	3	50	\$7,000
C89	Route 8c	O'Grady Street calm street	2.86	3	290	\$43,000
C90	Route 8c	Marriott Street calm street	2.82	3	140	\$21,000
C91	Route 2a	Thirkell Street shared path	2.67	3	250	\$25,000
C92	Route 7	Mount Street bike lanes	2.57	3	2,940	\$735,000
C93	Route 3	Mooreville Road bike lanes	2.31	3	2,540	\$635,000
C94	Route 6	Three Mile Line Road bike lanes	2.30	3	1,660	\$416,000
C95	Route 1c	Wivenhoe shared path	2.20	3	1,220	\$547,000

Note: Costing estimates are based on linear distance of the infrastructure being installed, and do not account for additional engineering works which may be required, including rebuilding of kerb and channel, bridge works, etc. These numbers should be read as a broad guide, but final costing must be undertaken during the planning process.

10.3.2 Walking actions

Table 6 outlines the walking actions, including the prioritisation score, implementation stage, segment length and high-level cost estimate. These actions focus on streets within the urban area that currently do not have a footpath on either side of the street. Each actions will be assessed individually to determine whether a footpath is required and whether it is feasible to deliver.

Maps of the proposed walking network staging and walking actions are provided in Appendix A.4.

Table 6 Walking actions

Action No.	Name	Prioritisation score	Stage	Length (m)	Costing estimate
W1	Maluna Crescent footpath	3.9	1	20	\$8,000
W2	Duke Street footpath	3.8	1	90	\$33,000
W3	Arthur Street footpath	3.8	1	50	\$17,000
W4	Frederick Street footpath	3.8	1	40	\$15,000
W5	Harrison Street footpath	3.8	1	90	\$31,000
W6	Elizabeth Street footpath	3.7	1	60	\$20,000
W7	Hopkinson Street footpath	3.7	1	60	\$22,000
W8	Hellyer Avenue footpath	3.5	1	70	\$26,000
W9	Cambria Street footpath	3.5	1	110	\$40,000
W10	Romaine Street footpath	3.4	1	120	\$42,000
W11	Bryan Street footpath	3.3	1	90	\$32,000
W12	Amelia Street footpath	3.2	1	110	\$38,000
W13	Alma Place footpath	3.2	1	40	\$16,000
W14	Bell Street footpath	3.2	1	140	\$51,000
W15	Ann Place footpath	3.2	1	50	\$18,000
W16	Chalk Avenue footpath	3.2	1	60	\$20,000
W17	Oval Street footpath	3.1	1	140	\$51,000
W18	Bay Street footpath	3.1	2	150	\$54,000
W19	Wembley Street footpath	3.0	2	70	\$26,000
W20	Young Street footpath	2.9	2	100	\$36,000
W21	Boston Avenue footpath	2.8	2	100	\$36,000
W22	Mercedes Place footpath	2.8	2	100	\$36,000
W23	Claude Place footpath	2.8	2	110	\$38,000
W24	Eastwood Drive footpath	2.7	2	450	\$158,000
W25	George Street footpath	2.6	3	140	\$49,000
W26	Ormsby Street footpath	2.6	3	300	\$106,000
W27	Best Street footpath	2.4	3	220	\$78,000
W28	River Road footpath	2.3	3	440	\$152,000
W29	Wembley Street Lane footpath	2.2	3	160	\$57,000
W30	Besser Crescent footpath	2.0	3	390	\$136,000

Note: Costing estimates are based on linear distance of the infrastructure being installed, and do not account for additional engineering works which may be required, including rebuilding of kerb and channel, bridge works, etc. These numbers should be read as a broad guide, but final costing must be undertaken during the planning process.

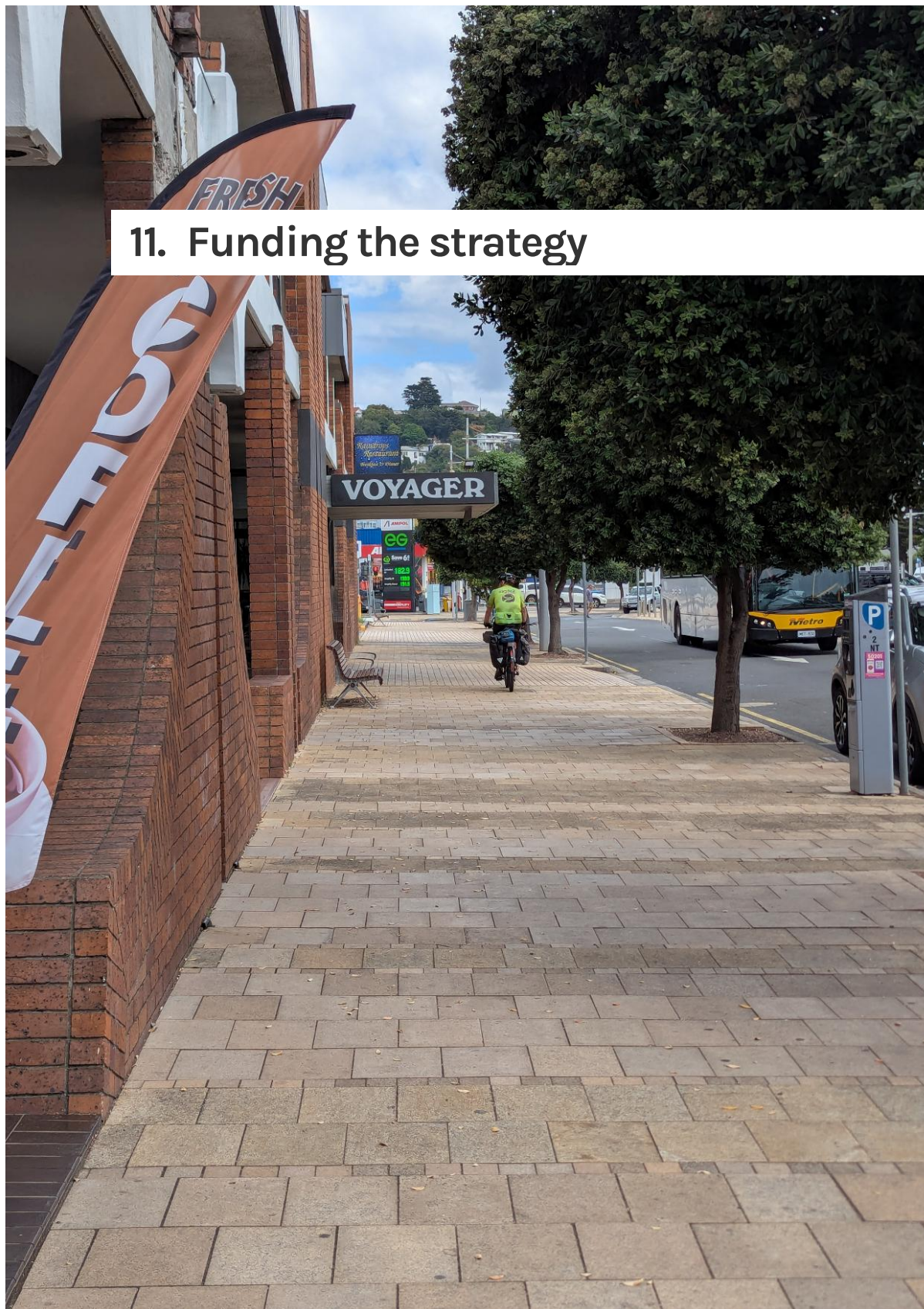
10.3.3 Opportunistic actions

The opportunistic actions outlined in Table 7 include connections that have not yet been fully defined or confirmed as feasible. Due to their undefined status, infrastructure typologies and alignments have not yet been determined and therefore cost estimates have not been developed.

The timing and delivery of these actions remain uncertain and may depend on factors such as coordination with other infrastructure projects (e.g. street upgrades), technical or engineering feasibility, land ownership or access to external funding.

Table 7 Opportunistic actions

Action No.	Route No.	Name	Length (m)
C96	Route 8f	Bathurst Street link	100
C97	Route 8f	Argyle Street link	140
C98	Route 8f	Brooklyn Road reserve link	890
C99	Route 1d	Coastal pathway east link	4,750
C100	Route 8b	Mount Street link	900
C101	Route 8d	Old Surrey Road future link	1,450



11. Funding the strategy

This section provides information on some of the options available to fund the actions included in this ATS. It also provides a brief overview of the existing legislative and policy context in Tasmania that allows for contributions towards infrastructure funding.

Council invests in walking, wheeling and riding infrastructure through rate revenue as well as state and federal grants. We will continue to invest in delivering infrastructure which achieves development of the networks laid out in this ATS. We will also continue to fund the ongoing maintenance of existing infrastructure. Delivering the full network will require significant external funding to supplement Council's commitments to improving active transport in Burnie.

11.1 State Government funding

11.1.1 Better Active Transport Grant Programs

Since 2015, the Tasmanian Government has committed a total of \$17.5 million to improving walking, wheeling, and riding safety and infrastructure across the state. *The Better Active Transport Grant Programs* were established to allocate this funding to local councils for the planning and delivery of active transport initiatives.

The Burnie ATS was co-funded through Round 2 of the program, reflecting the State Government's support for Councils efforts to encourage active travel. The funding attained through the program will also support some of the detailed design work required for delivering key infrastructure projects outlined in this ATS.

11.1.2 Future funding

The Tasmanian Government is committed to encouraging active transport through its *Walk, Wheel, Ride Strategy*. It acknowledges that local councils will require ongoing State Government support to deliver walking, wheeling, and riding infrastructure.

A key focus is to make future funding more accessible and to advocate for the introduction of additional grant programs to support councils in delivering local projects. Council will continue to seek State Government funding as new programs and grant opportunities become available.

Collaboration with the State Government

Many of the infrastructure actions outlined in this ATS will require external funding and support to become reality. This ATS was funded by the Department of State Growth, which will also support some of the detailed design work required. Council will work with the Tasmanian Government to advocate for investment in projects that improve walking, wheeling and riding conditions across Burnie.

Partnerships with the Department of State Growth and the Department for Education are particularly important to ensure that infrastructure upgrades around schools are aligned with safety, accessibility and behaviour change objectives. Council will also ensure that its designs align with relevant guidelines to strengthen funding applications and improve the likelihood of success.

Box 6 Collaborating with the State Government

11.2 Commonwealth Government funding

11.2.1 Active Transport Fund

The *Active Transport Fund* is the Commonwealth Governments initiative to encourage an increase in active transport. It was created as part of the Government's commitment, under the *National Road Safety Strategy 2021-30*, to improve road safety outcomes.¹⁴ Through the fund, \$100 million in funding will be available from 2024-25 to 2028-2029.

Applications were open to all local councils and applications were required to directly address at least one of the program's focus areas. Five councils in Tasmania received funding, ranging between \$500,000 to \$2.2 million per project. When

¹⁴ <https://investment.infrastructure.gov.au/about/local-initiatives/active-transport-fund>

new rounds of funding become available, or if funding is not fully allocated and additional tranches are announced, Council will apply to fund projects in this ATS.

11.2.2 Local Roads and Community Infrastructure Program

The *Local Roads and Community Infrastructure* (LRCI) Program provided by the Department of Infrastructure, Transport, Regional Development, Communications and the Arts provides grant funding for projects that benefit local communities. Projects must be publicly accessible and represent additional commitments beyond Council's baseline funding.

The program is currently in its fourth round, and Council has been successful in securing funding in previous rounds. We will continue to apply for future funding opportunities under the LRCI Program to support the development of Burnie's walking, wheeling, and riding network.

11.2.3 Black Spot Program

The *Black Spot Program*, also provided by the Department of Infrastructure, Transport, Regional Development, Communications and the Arts funds targeted road safety improvements at sites with a demonstrated history of crashes or a high risk of potential crashes. Council has recently secured funding through this program for the Futuna Avenue and West Park Grove roundabout upgrade.¹⁵

A number of intersections identified in this ATS require safety improvements. Council will undertake Road Safety Audits at these locations to identify sites that meet the program's eligibility criteria. This will enable us to proactively seek Black Spot funding and incorporate walking, wheeling, and riding improvements into intersection upgrade designs.

11.3 Local funding

Tasmania currently lacks a consistent, statewide framework for developer contributions. In other states such as NSW, Victoria and Queensland, contribution schemes are embedded in state planning legislation and supported by clear guidance and dispute resolution mechanisms. This enables councils to apply general infrastructure charges to support delivery of essential infrastructure.

Tasmania's legislative powers are spread across several Acts, each with limited and specific applications.¹⁶ This makes it difficult for local governments to rely on contributions for consistent funding. The available mechanisms for developer contributions in Tasmania include:¹⁷

- Council-specific infrastructure policies and charges enforced through permit conditions (under Section 51 of the Land Use Planning and Approvals Act 1993)
- Part 5 Agreements, which may be used to deliver infrastructure in specific cases (under Section 71 of the Land Use Planning and Approvals Act 1993)
- Open space contributions for new subdivisions (under Sections 116 and 117 of the Local Government [Building and Miscellaneous Provisions] Act 1993)
- Servicing infrastructure and headworks charges levied by TasWater, and in some cases, by local councils.

Each mechanism is enforced via a planning permit.

Council recognises the need for a more coordinated and reliable funding approach to support local infrastructure delivery. While current mechanisms are limited, Council will continue to monitor legislative developments. This may include advocating for improved state-level guidance.

¹⁵ <https://www.burnie.tas.gov.au/Council/Projects/2025/Futuna-Ave-and-West-Park-Grove-Roundabout-Works>

¹⁶ https://www.lgat.tas.gov.au/_data/assets/pdf_file/0030/1139691/LGAT-Infrastructure-Contributions-Discussion-Paper-11-April-2022.pdf

¹⁷ <https://yoursay.hobartcity.com.au/53448/widgets/277518/documents/217983>

12. Monitoring and evaluation



Monitoring and evaluation are critical components of any strategy. It allows us to track how well we are meeting our goals and objectives and adjust our approach. Our monitoring and evaluation framework is below.

The framework is designed to monitor and evaluate the success of the ATS as a whole, and individual key links and actions within the strategy.

12.1 Goals and objectives

The first step is to set goals and objectives. These will be measured against to evaluate the performance of the strategy. Our goals and objectives, discussed in Section 3.3 are:

- Increase the proportion of the population within 400m of a AAA riding facility, measured every five years, to 2040.
- Increase the proportion of the Burnie community that meet the national guidelines for physical activity of 20 minutes per day (first year of data collection to set baseline, then measured every five years, to 2040)
- Increase satisfaction with our walking, wheeling and riding infrastructure
- Reduce the number of serious injuries and deaths on Tasmanian roads consistent with the Tasmania government Towards Zero – Road Safety Strategy 2017-2026, and the National Road Safety Strategy 2021-2030.
- Increase the number of DDA compliant pedestrian crossings and bus stops, measured every five years, to 2040.
- Double the number of people who walk or cycle to work by 2040, consistent with Tasmanian government targets.
- Double the proportion of short trips completed by walking and cycling to places of education by 2040, consistent with Tasmanian government targets.
- Double the proportion of the Burnie community residents that meet the national guidelines for physical activity through walking and cycling by 2040.

12.2 Data collection

Data must be collected in order to measure success in meeting goals and objectives. Data should come from multiple sources, including through Council collected data and from other organisations. Below offers a list of data sources and types we will use to measure our success.

12.2.1 Data collected by Burnie City Council

Council will introduce a new Walking, Wheeling and Riding participation survey. This will include survey questions on:

- Demographics:
 - Age
 - Gender
 - Residential location within Burnie City Council.
- Walking, wheeling and riding activity
 - Number and type of bikes/micromobility devices owned
 - Riding in previous year (daily; a few times a week; a few times a month; a few times in six months; at least once a year; never)
 - Change from last year (less; the same; more)
 - Walking/wheeling in previous year (daily; a few times a week; a few times a month; a few times in six months; at least once a year; never)
 - Change from last year (less; the same; more).
- Riding activity in last week
 - Number of riding trips
 - Purpose of trip(s)
 - Origin/destination of trip(s)
 - Time of trip(s)
 - Other people on trip(s), e.g., friends, partners, children (if applicable).
- Walking/wheeling activity in last week:
 - Number of walking/wheeling trips
 - Purpose of trip(s)
 - Origin/destination of trip(s)
 - Time of trip(s)
 - Other people on trip(s), e.g., friends, partners, children (if applicable).

- Satisfaction with riding:
 - Satisfaction with route choice (Likert)
 - Satisfaction with infrastructure typology (Likert)
 - Feelings of safety while riding (Likert)
 - Feelings of comfort while riding (Likert).
- Satisfaction with walking/wheeling:
 - Satisfaction with route choice (Likert)
 - Satisfaction with infrastructure typology (Likert)
 - Feelings of safety while riding (Likert)
 - Feelings of comfort while riding (Likert).
- Non-infrastructure actions:
 - Awareness of non-infrastructure actions
 - Satisfaction with non-infrastructure actions (Likert).

12.2.1.1 Counters

- Riding Counts (new):
 - Permanent counters installed along key routes
 - Complete Bicycle Network Super Tuesday counts.
- Temporary counters:
 - Between one and six months before installation of major infrastructure (total cost over \$500,000) – for at least one week
 - In the same month the following year after completion – for at least one week.

12.2.2 Data collected by other organisations:

- Australian Bureau of Statistics – Census
 - Number of journeys to work walked and by bicycle (residents in Burnie City Council)
 - Percentage of journeys to work walked and by bicycle (residents in Burnie City Council)
 - Number of journeys to work walked and by bicycle (people working in Burnie City Council)
 - Percentage of journeys to work walked and by bicycle (people working in Burnie City Council)
- Tasmanian State Government – Household Travel Survey. It is important to note that only Greater Hobart is included in recent surveys, meaning

Burnie does not have data collection. We have included an action as part of this ATS to advocate to the State Government to be included in future surveys. We will use this data to measure:

- Number of trips (all purpose) by walking, wheeling and riding.
- Percentage of trips (all purpose) by walking, wheeling and riding.
- Tasmania Government Road Crash Data – Crash Statistics
 - Number of crashes involving pedestrians and people riding in Burnie
 - Number of pedestrians and people riding injuries in Burnie
 - Number of pedestrians and people riding fatalities in Burnie
 - Location of all crashes involving pedestrians and people riding in Burnie (including severity).

12.3 Output evaluation

The outputs should be measured to ensure that the strategy is delivering. This should include assessment on an annual basis of:

- Expenditure on walking, wheeling and riding:
 - Total spending
- Infrastructure delivered
- Non-infrastructure items delivered

This reporting will show how the ATS is performing over time in delivering its infrastructure and non-infrastructure actions.

12.4 Outcome evaluation

The success in meeting outcomes should be measured to ensure the ATS is succeeding in its objectives. This should include an assessment on walking, wheeling and riding participation and satisfaction, including assessment on a periodic basis of:

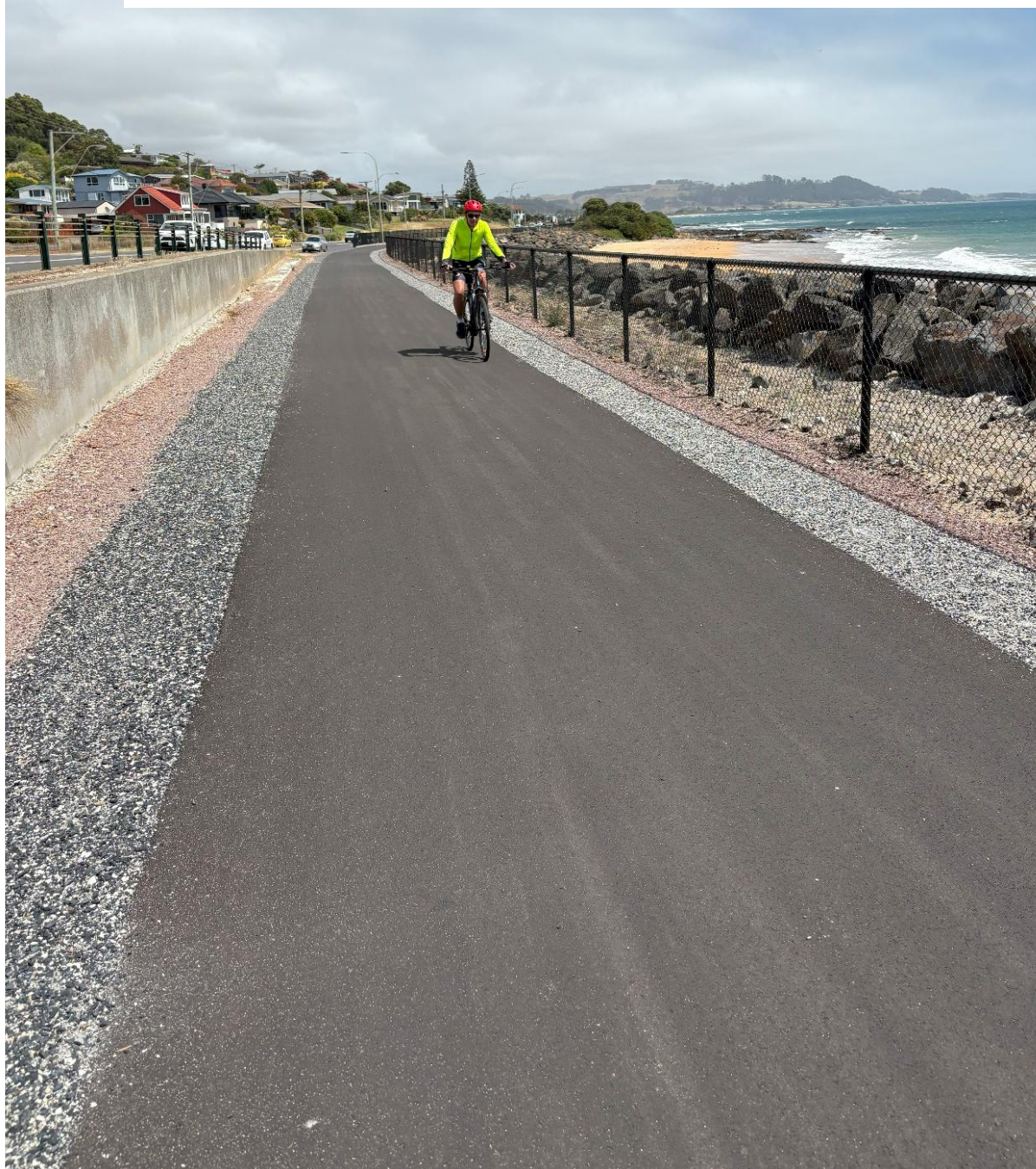
- Percentage of journeys to work made by walking, wheeling or riding
- Percentage of journeys to education made by walking, wheeling or riding
- Participation in walking, wheeling and riding for all residents of Burnie, including:

- Participation in walking, wheeling and riding by age and gender
- Trips per person per week
- Trips by purpose
- Km by mode (walked, wheeled, rode)
- Time spent walking, wheeling or riding.
- Crashes – total and by exposure (i.e., crashes per 100,000 km travelled by people walking, wheeling or riding)
- Satisfaction with walking, wheeling and riding in Burnie (route choice, typology, things of safety, feelings of comfort).

Data will be evaluated regularly and compared with targets. Data will also be evaluated across time to see how travel patterns, walking, wheeling and riding participation, and satisfaction changes over time.

Data will be evaluated regularly to see how travel patterns and satisfaction changes over time.

Appendices



A.1 Glossary

The below glossary terms have been sourced from the Tasmania Government *Cycling Infrastructure Design Guide*.¹⁸

Active transport: Any form of land transport that requires some form of human propulsion; principally walking, wheeling and riding.

All ages and abilities: The users that cycling infrastructure needs to serve including all people, all types of bicycles and other devices and types of trip purposes.

Bicycle: A small vehicle with one or more wheels that is built to be propelled by human power through a belt, chain or gears, which can include unicycles and tricycles/trikes.

Bicycle facility: A type of cycling infrastructure that may comprise of several integrated treatments.

Bicycle lane: A dedicated lane for bicycles, which can either be separated or not separated from vehicle traffic.

Commuting: Refers to the journey to or from work.

Cycling: Moving from one place to another on a bike. This term is not often used as riding captures a greater diversity of users and trips.

Cycling infrastructure: A range of treatments and facility types that are connected to form a safe and comfortable network for riding.

E-bike: A bike with an electric-powered motor, which primarily relies on human pedalling power, with the electric motor providing supplementary assistance.

Car dooring: A type of crash where a person in a vehicle opens a door into the path of a rider.

E-scooter: A scooter with an electric-powered motor that aids in movement.

Filtered permeability: A variety of treatments that prioritise selected transport modes.

Footpath: A path to walk on, usually next to a road.

Kerb buildouts: An extension of the footpath and kerb to reduce the crossing distance for people walking.

Micromobility: Refers to small, lightweight devices that are either human or electric powered, such as bicycles, scooters, skateboards, mobility scooters

Modal filter: A treatment, sometimes called a point closure to provide filtered permeability.

Pedestrian: A person moving from place to place, either by foot or using an assistive mobility device.

Policy: Agreed principles by which decision makers are guided, often in the form of plans or actions.

Riding: The act of using a bicycle or micromobility device such as an e-bike, scooter, cargo bike or trike to move from one place to another.

Shared path: A path that people either walking or riding bikes or scooters can use, which is typically wider than a normal footpath.

Sharrow: A painted surface treatment that signifies that it is safe for riders to use the shared traffic lane.

Traffic calming: Road design techniques that improve safety for people cycling, micromobility and pedestrians by reducing traffic volumes and/or encouraging slower motor vehicle speeds.

Treatment: An individual component that forms part of a bicycle facility.

Walking: Moving from one place to another on foot or using a mobility aid like a walking frame.

Wayfinding: Signage and information that assists users to navigate to their destination. Can be both fixed infrastructure or digital.

Wheeling: Moving from one place to another using a wheelchair, a scooter, skateboard, roller blades or pushing a pram.

¹⁸https://www.infrastructure.tas.gov.au/_data/assets/pdf_file/0006/534768/Tasmanian_Cycling_Infrastructure_Design_Guide.pdf

A.2 Conceptual CBD design



Figure 32 Wilson and Wilmot Street intersection concept design 1



Figure 33 Wilson and Wilmot Street intersection concept design 2

A.3 Riding network maps

A.3.1 Network staging

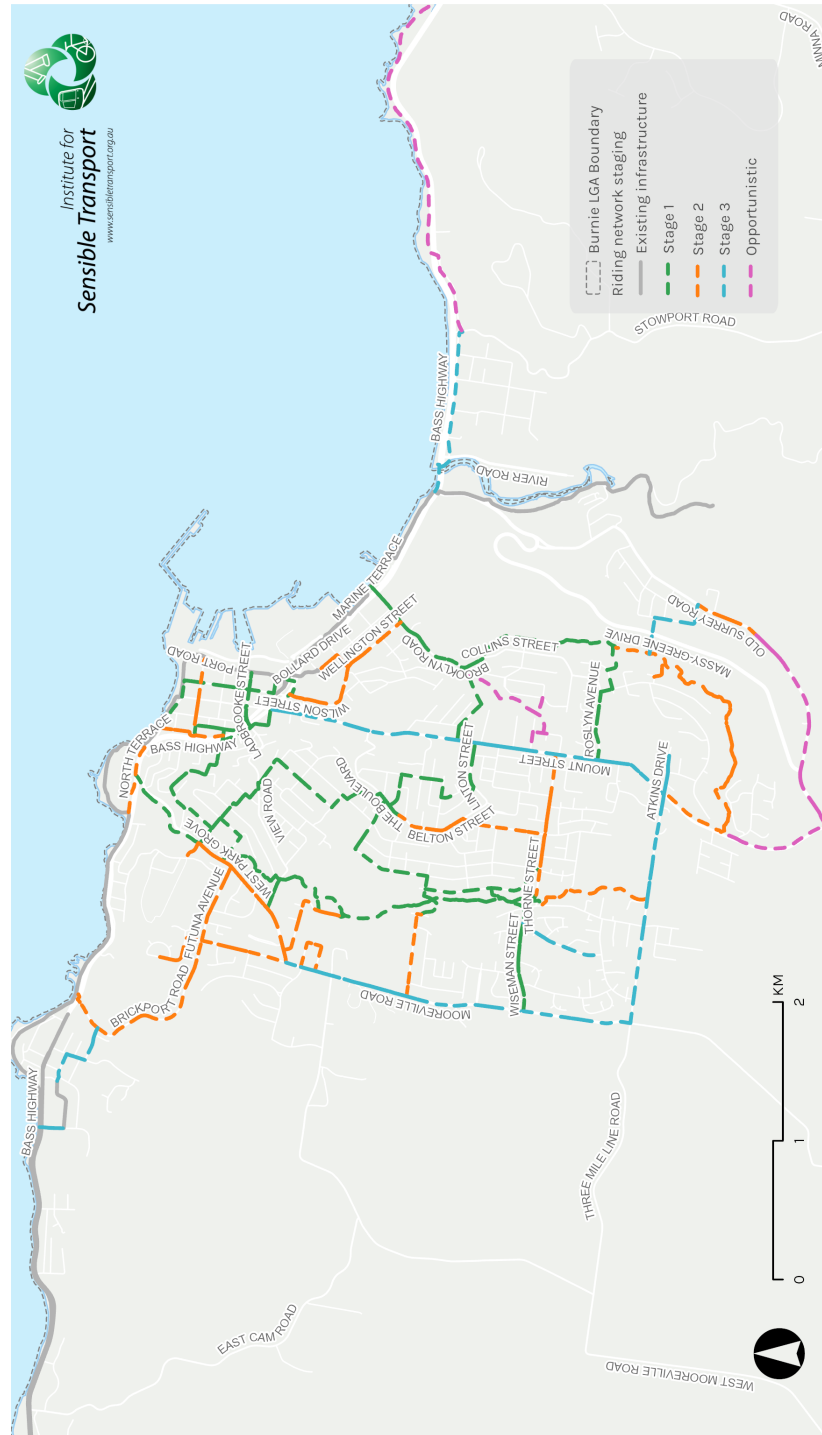


Figure 34 Riding network staging

A.3.2 Route 1



Figure 35 Route 1 map

Note: Existing infrastructure is shown with a route number only and does not have an associated action number.

A.3.3 Route 2



Figure 36 Route 2 map

A.3.4 Route 3

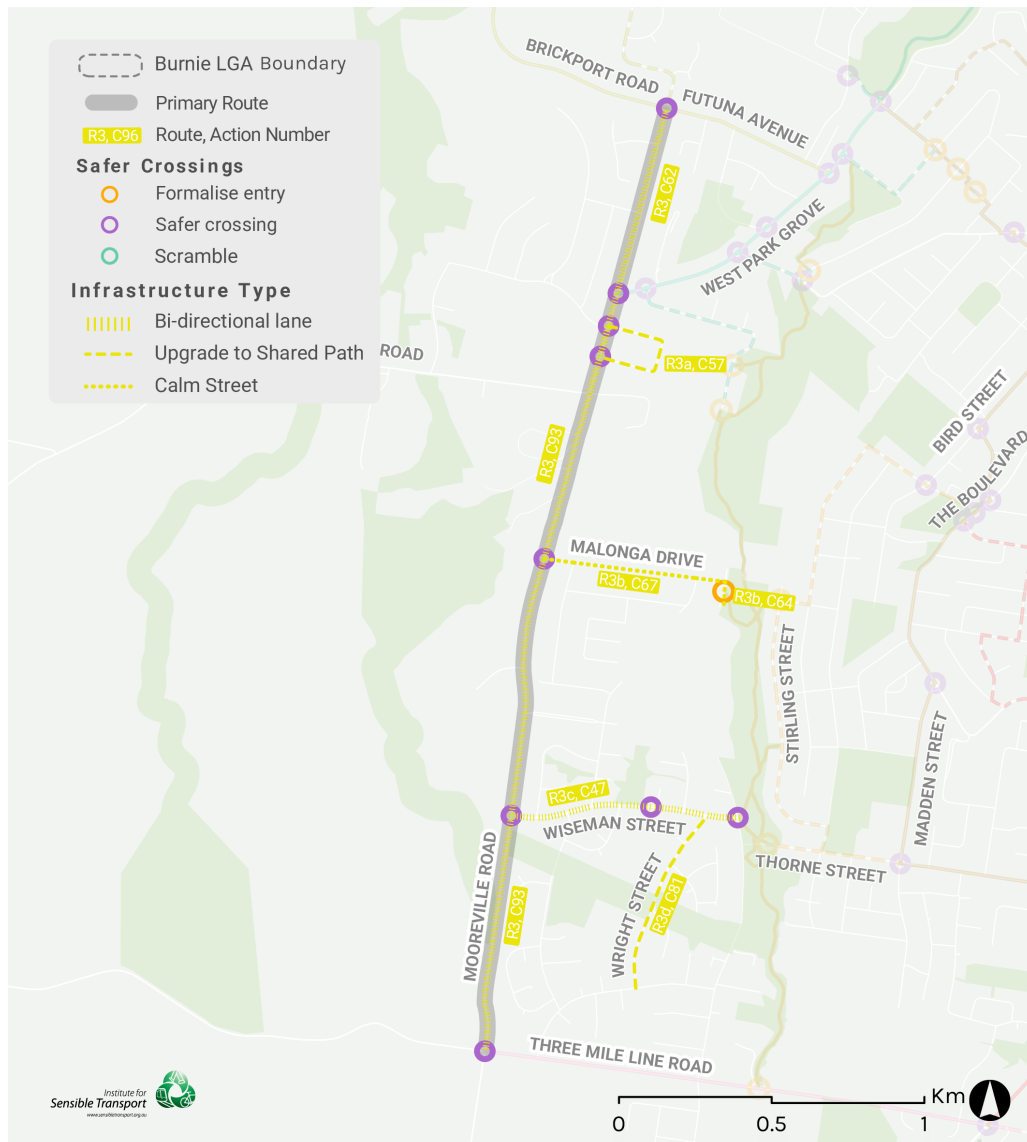


Figure 37 Route 3 map

A.3.5 Route 4

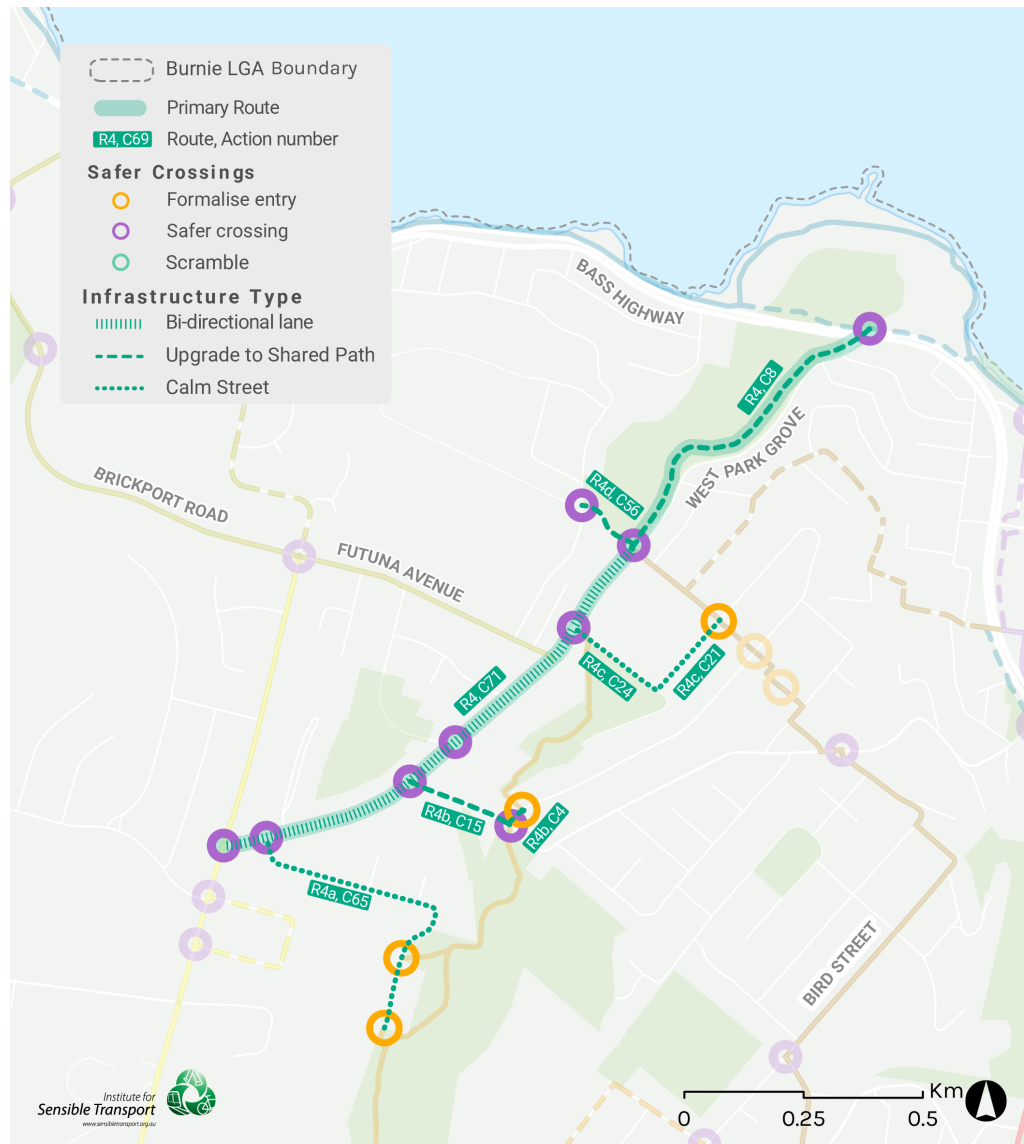


Figure 38 Route 4 map

A.3.6 Route 5

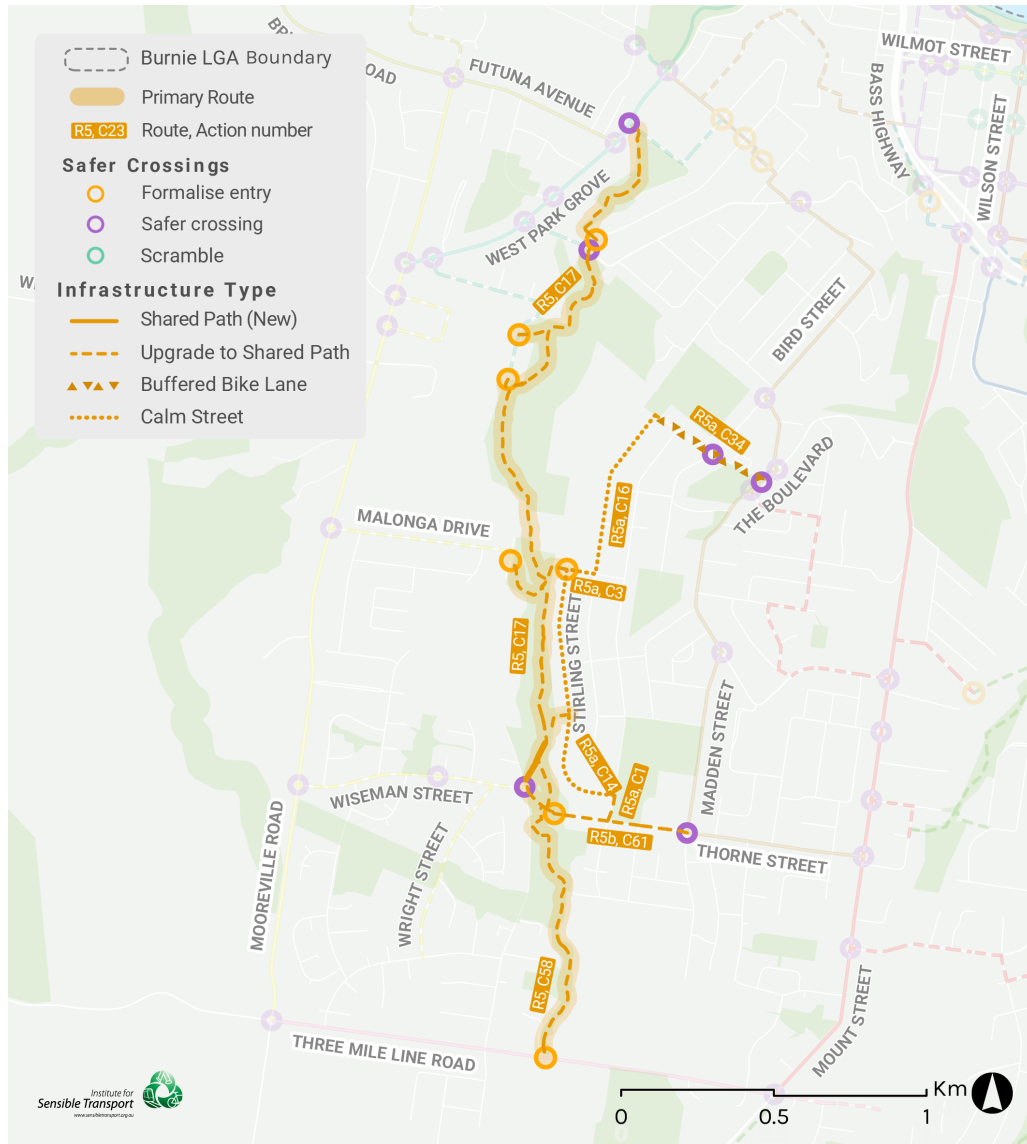


Figure 39 Route 5 map

A.3.7 Route 6

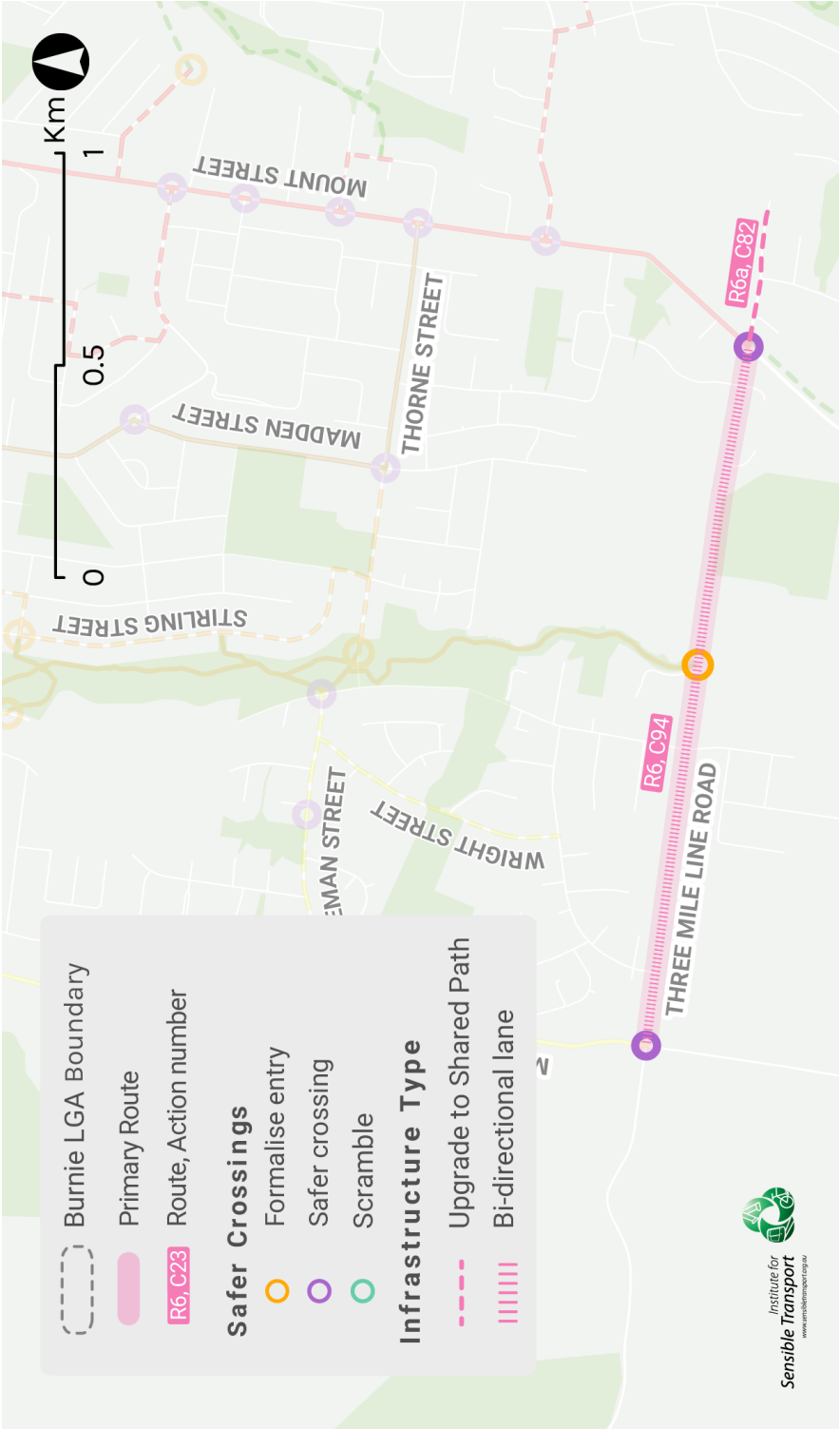


Figure 40 Route 6 map

A.3.8 Route 7

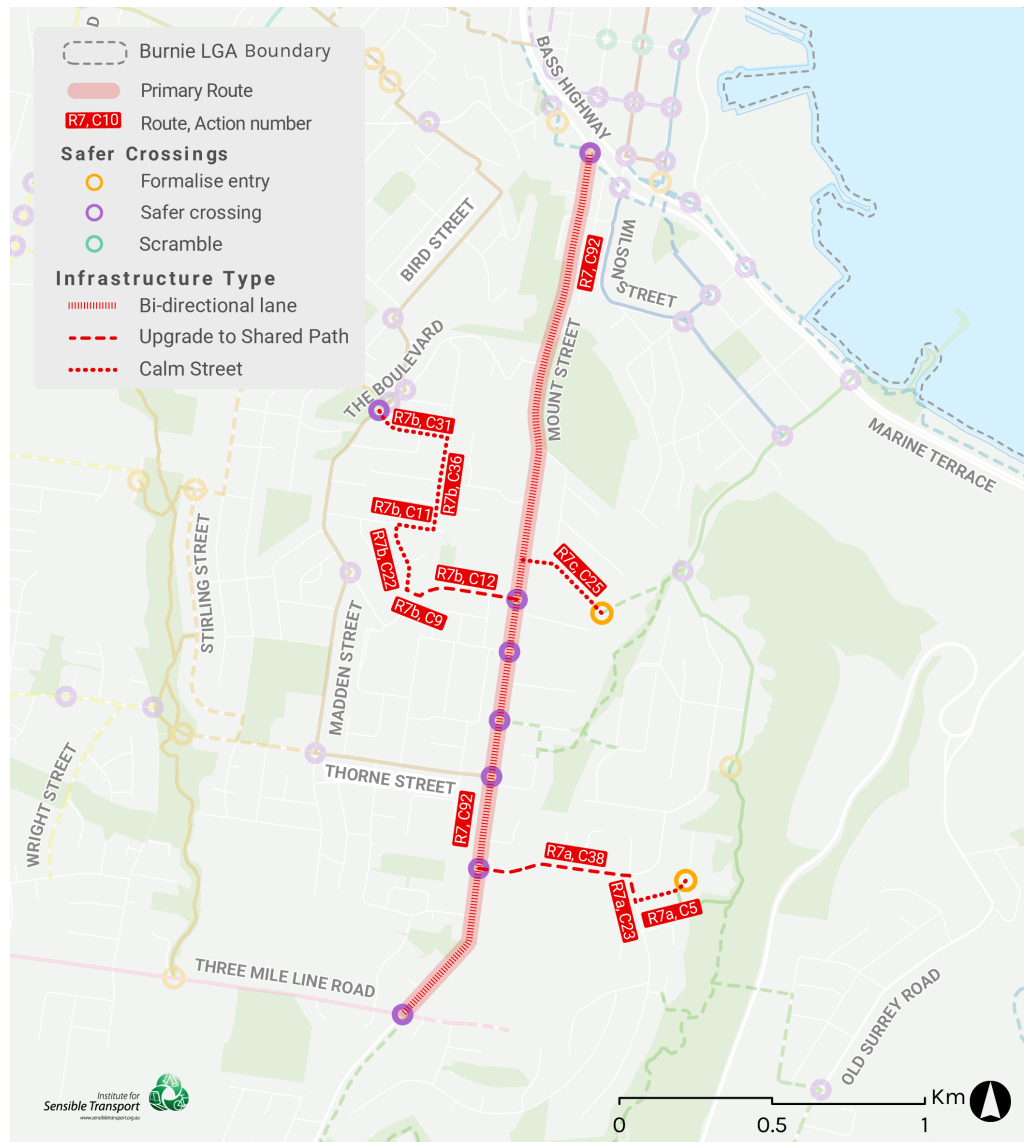


Figure 41 Route 7 map

A.3.9 Route 8

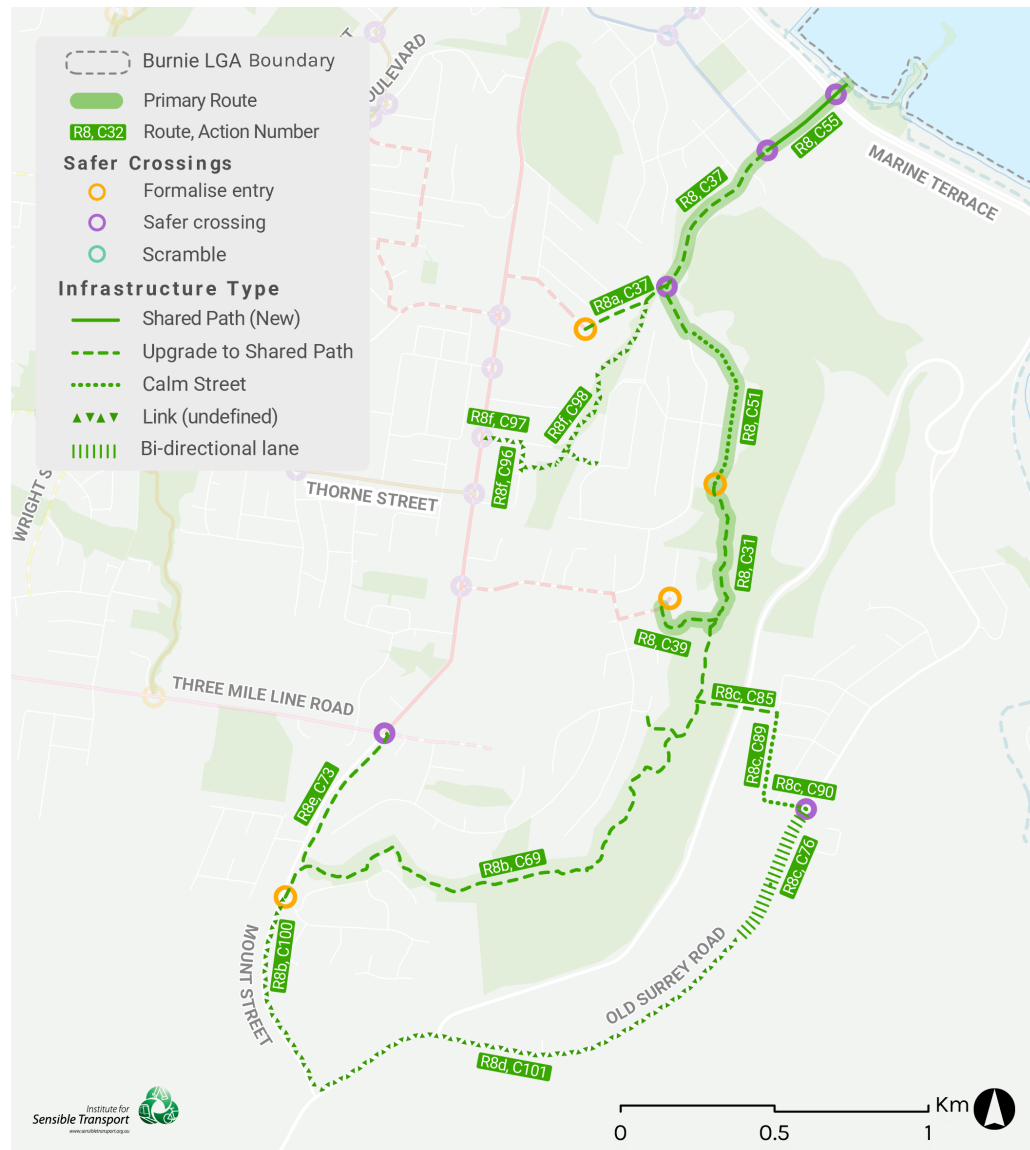


Figure 42 Route 8 map

A.3.10 Route 9

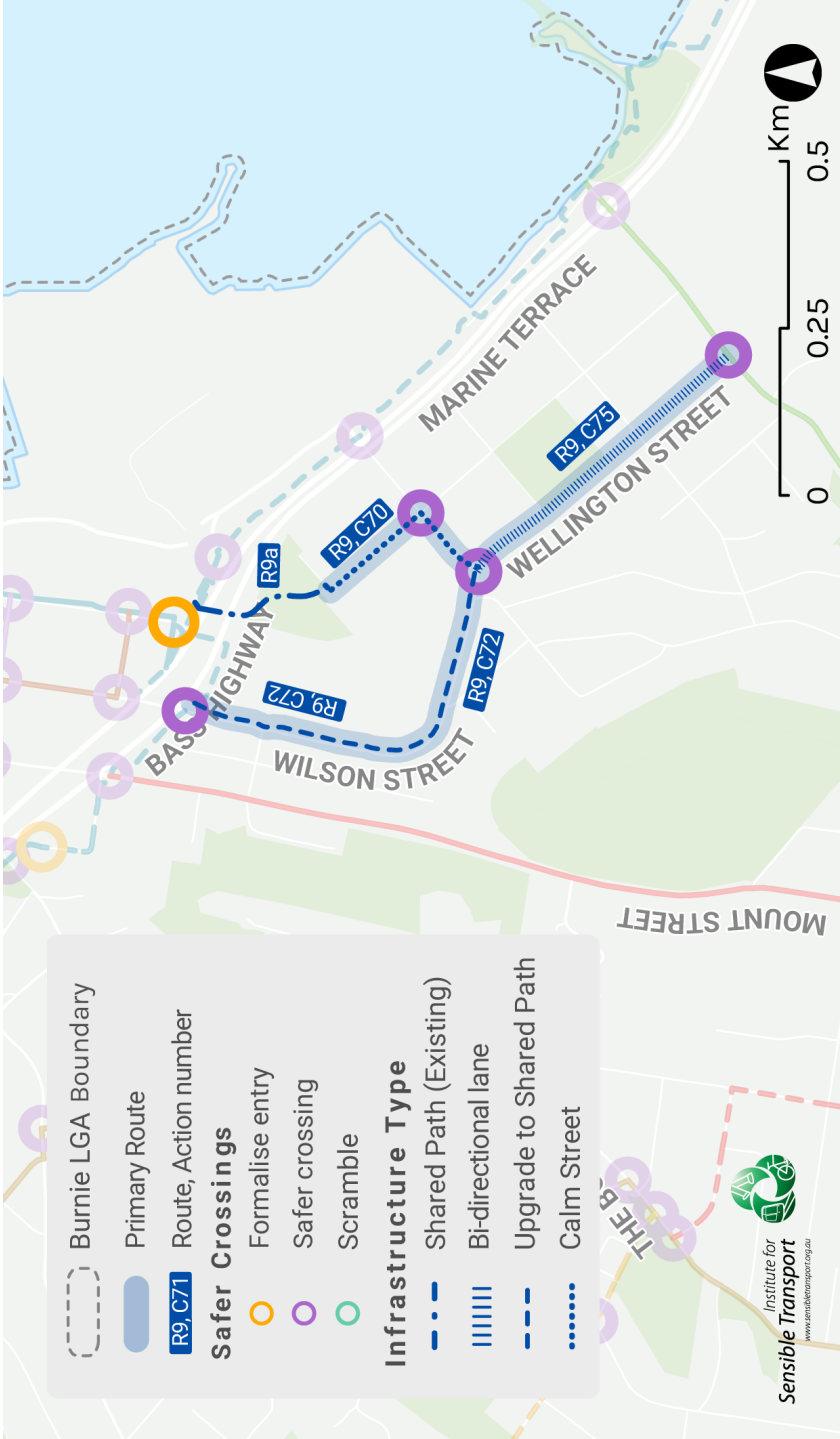


Figure 43 Route 9 map

Note: Existing infrastructure is shown with a route number only and does not have an associated action number.

A.3.11 Route 10

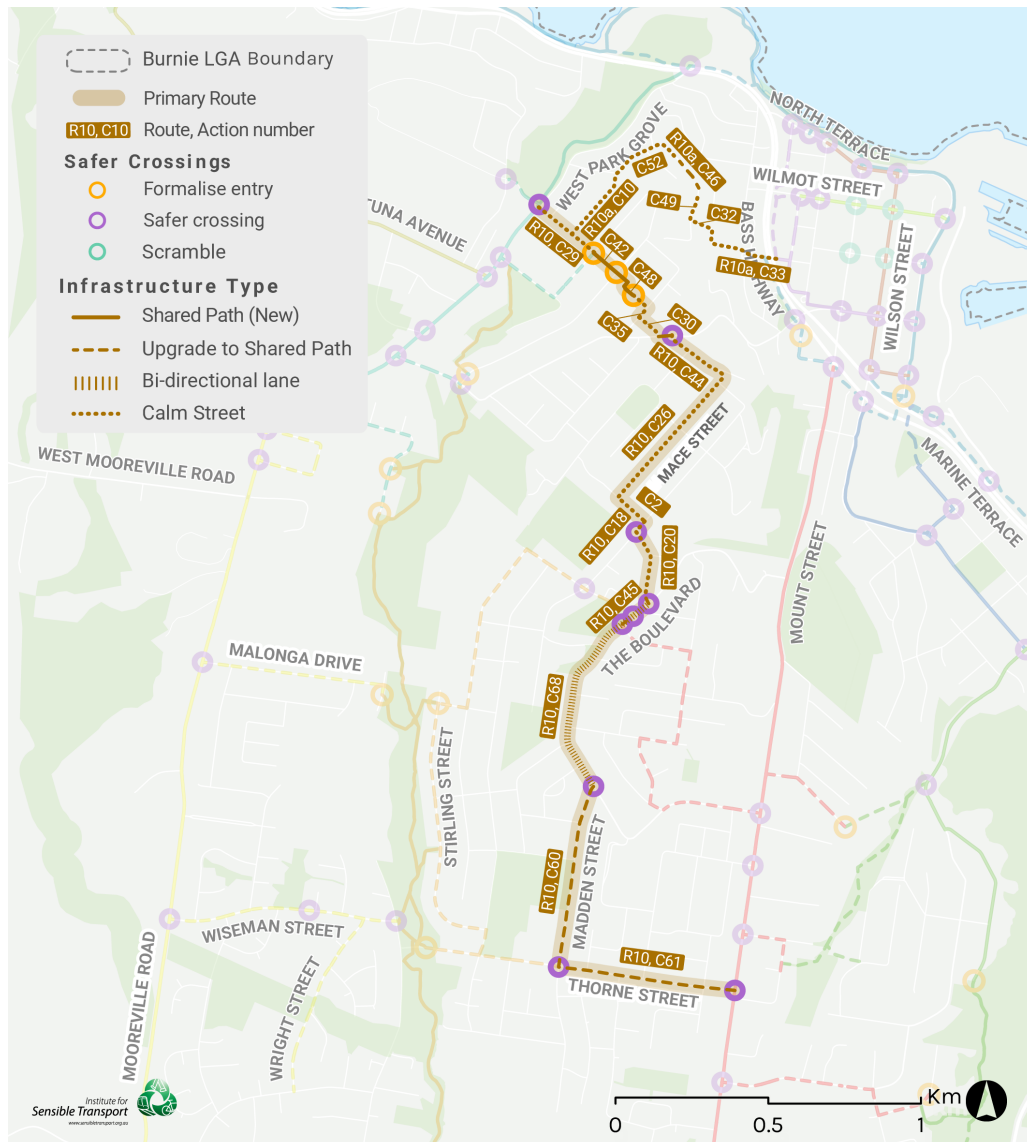


Figure 44 Route 10 map

A.3.12 Route 11

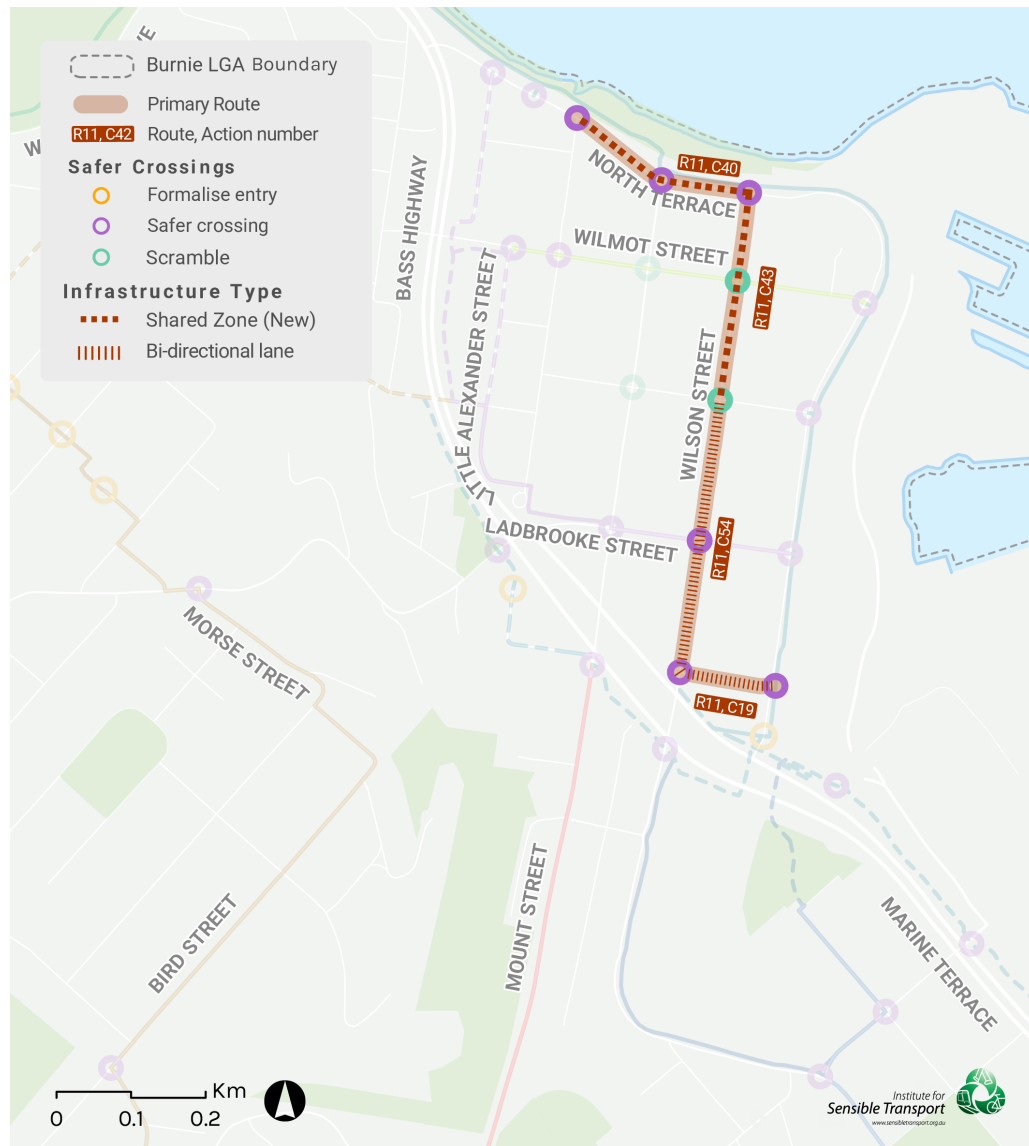


Figure 45 Route 11 map

A.3.13 Route 12

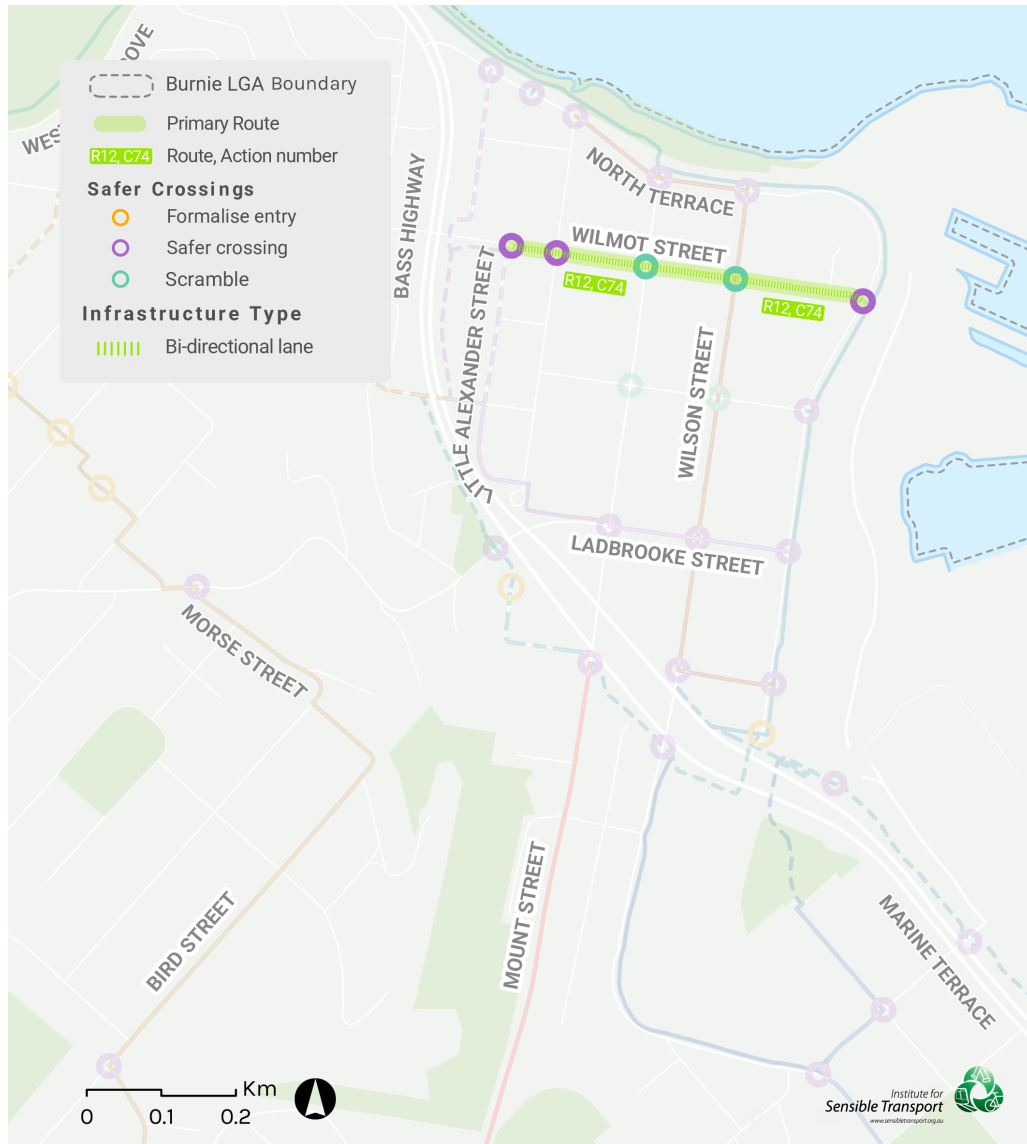


Figure 46 Route 12 map

A.3.14 Route 13

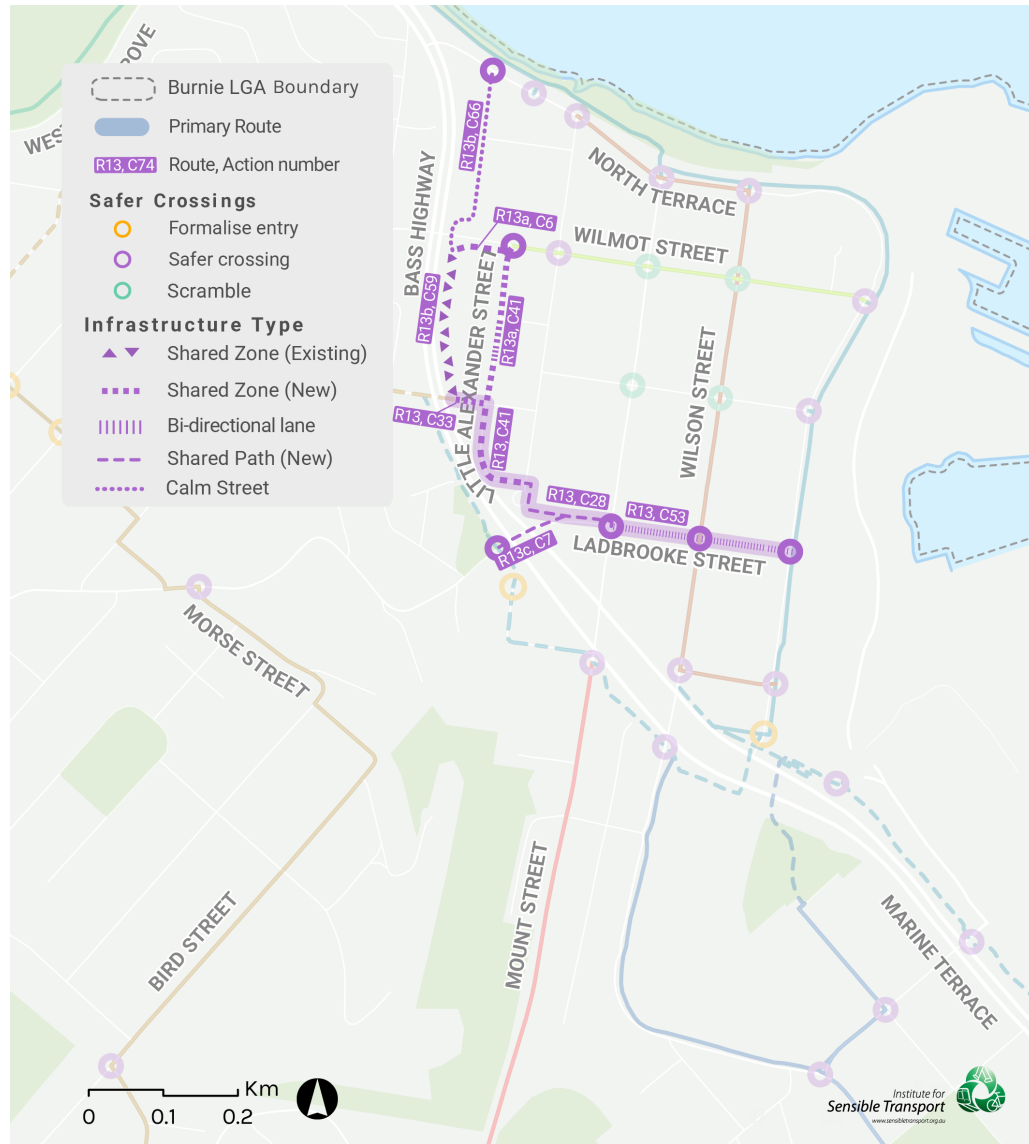


Figure 47 Route 13 map

A.4 Walking network maps
A.4.1 Footpath network staging – West Burnie



Figure 48 Proposed footpath network staging – West Burnie

A.4.2 Footpath network staging – Central Burnie

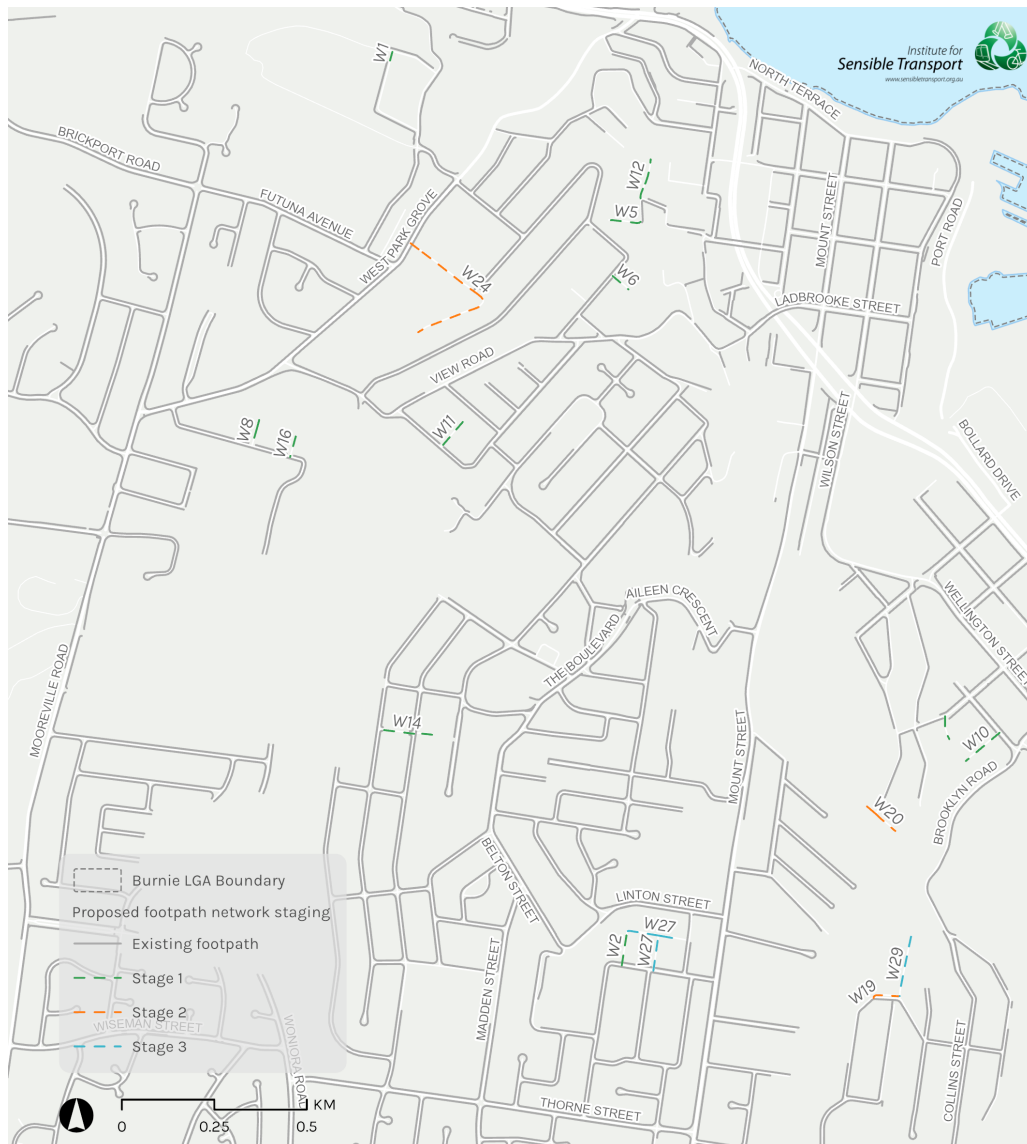


Figure 49 Proposed footpath network staging – Central Burnie

A.4.3 Footpath network staging – East Burnie

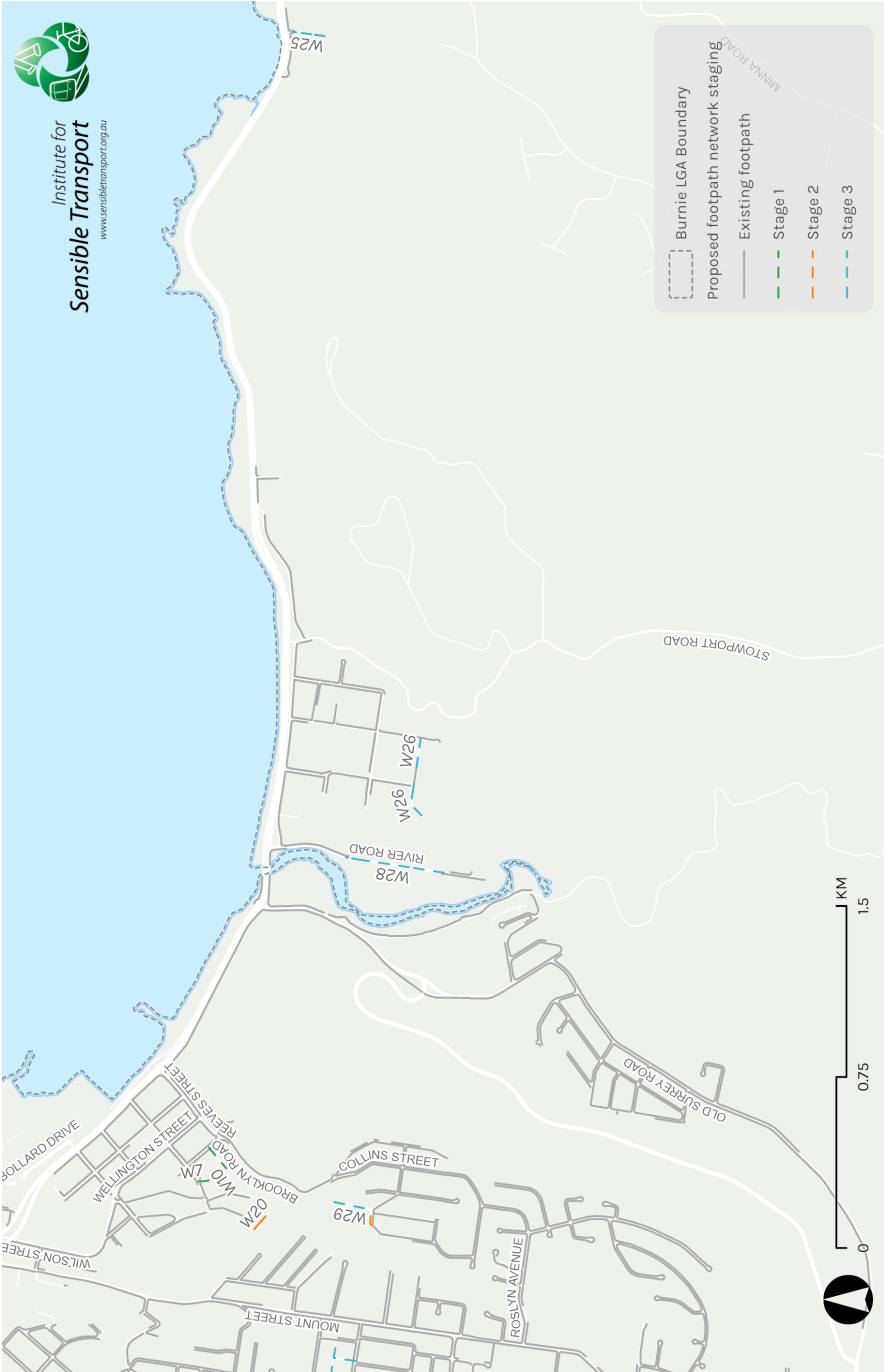


Figure 50 Proposed footpath network staging – East Burnie

MINUTES AND REPORTS OF COMMITTEES**AO188-25 BURNIE CITY YOUTH COUNCIL - UNCONFIRMED MINUTES OF MEETING HELD ON 25 SEPTEMBER 2025**FILE NO: 2/5/34

RECOMMENDATION:

“THAT the Unconfirmed Minutes of the Meeting of the Burnie City Youth Council held on 25 September 2025, be received and noted.”

SUMMARY

Key topics included participation in the ABC Takeover Burnie initiative, where members agreed to support local youth events and promote the Council’s priorities of Mental Health and Active Transport.

The group confirmed registration for the YNOT Youth Forum at UTAS Cradle Coast Campus on 31 October and proposed inviting mental health experts to co-develop a youth-led strategy.

A copy of the meeting minutes is attached.

ATTACHMENTS

1 [↓](#). Minutes - Burnie Youth Council - 25 September 2025



**BURNiE
WORKS**

	Parker, Junior Deputy Mayor opened the meeting and provided a Welcome, Acknowledgement to Country.
1.	<p>Present</p> <ul style="list-style-type: none"> - Chloe - CJ C - Eden - Grace - Indiana - Isabelle - Isla - Jaylen - Joseph - Leo - Parker - Riley - Tom <p>Apologies:</p> <ul style="list-style-type: none"> - Ava, Bayleigh, Chia, Chelsea, CJ J, Harrison, Kelsey, Madix, Mia, Noah, Norah, Sanuli, Summer, Teresa, Tilly, Yuegi (Kiki). <p>Ex-officio Officers in attendance:</p> <p>Burnie City Council: Giovanna Simpson Burnie Works: Deanna Hutchinson, Alan Radford, Aaron Meldrum, Allison Daley</p>
2.	<p>Minutes</p> <p>Motion</p> <p><i>"That the Minutes of the meeting held on 28 August 2025 be accepted as a true and accurate record of the meeting."</i></p> <p>Moved: Leo Seconded: Eden Carried</p> <p>Correspondence</p> <ul style="list-style-type: none"> - ABC Takeover Burnie: <ul style="list-style-type: none"> o 30 young people in Burnie were selected to participate o How can BYC link up with these 30 young people? - Senator Anne Urquhart MP is seeking a meeting with YC - an informal conversation on governance. - Izzy correspondence.
3.	<p>Burnie Youth Council Requests</p> <ul style="list-style-type: none"> - THRIVE National Youth Summit – Darwin



BURNIE CITY YOUTH COUNCIL

4.	Power Conversation on FOGO YOYO Competition <ul style="list-style-type: none"> - Youth Councillors felt excluded from the design of the YOYO competition and decision process. - Youth Councillors explored ways to express their opinion and give constructive feedback on ideas that they disagree with.
5.	ABC Takeover <ul style="list-style-type: none"> - Two representatives promoted the ABC Takeover Burnie Youth Ideas Launch at Burnie Arts today. <ul style="list-style-type: none"> o A week of youth networking, skill-development and community events. Including a gala screening and an ideas pitch to the community. o The representatives requested a group photo with YC. <p>Motion <i>"That the Burnie Youth Council agree to have a photo taken with ABC Takeover"</i></p> <p style="text-align: right;">Carried Unanimously</p> <p>Reflection on ABC Takeover Discussion:</p> <ul style="list-style-type: none"> - A good conversation - Invitation for YC and friends from school to attend. - BW will attend Launch and will promote Youth Council's priorities: Mental Health and Active Transport.
6.	YNOT Youth Forum <ul style="list-style-type: none"> - The Burnie Youth Council has been registered as an interested group for the Tasmanian Youth Forum being held at UTAS Cradle Coast Campus on Friday 31 October. - YC discussed roles and ideas for the Forum.
7.	Agenda Items for September meeting <ul style="list-style-type: none"> - Invite Mental Health experts to talk about mental health and how YC can help. YC to form a panel with experts to develop a Mental Health strategy. - End of Year Excursion.
8.	Next Meeting Thursday 30 October, 9:15am -11:30am

Closed: 11.36am

AO189-25 NON AGENDA ITEMS

In accordance with the requirements of Regulation 10(6) of the *Local Government (Meeting Procedures) Regulations 2025* a matter may only be discussed at a meeting if it is specifically listed on the agenda of that meeting.

Council by absolute majority may decide at an ordinary meeting to deal with a matter that is not on the agenda if the General Manager has reported:

- (a) the reason it was not possible to include the matter on the agenda; and
- (b) the matter is urgent; and
- (c) that advice has been provided under section 65 of the *Act*.

There being no further business the Chairperson declared the meeting closed at ____pm.